



Latitude:34.03346, Longitude:-92.93577

Route:7 Section:06 Log:2.81

Arnold Road ID:10x7x6xA, Arnold Log mile:2.786

District 07, 19 - Clark County

Owner: 1 - State Highway Agency

Bridge Posting Information

41 - Structure Open/Posted/Closed: A - Open, no restriction

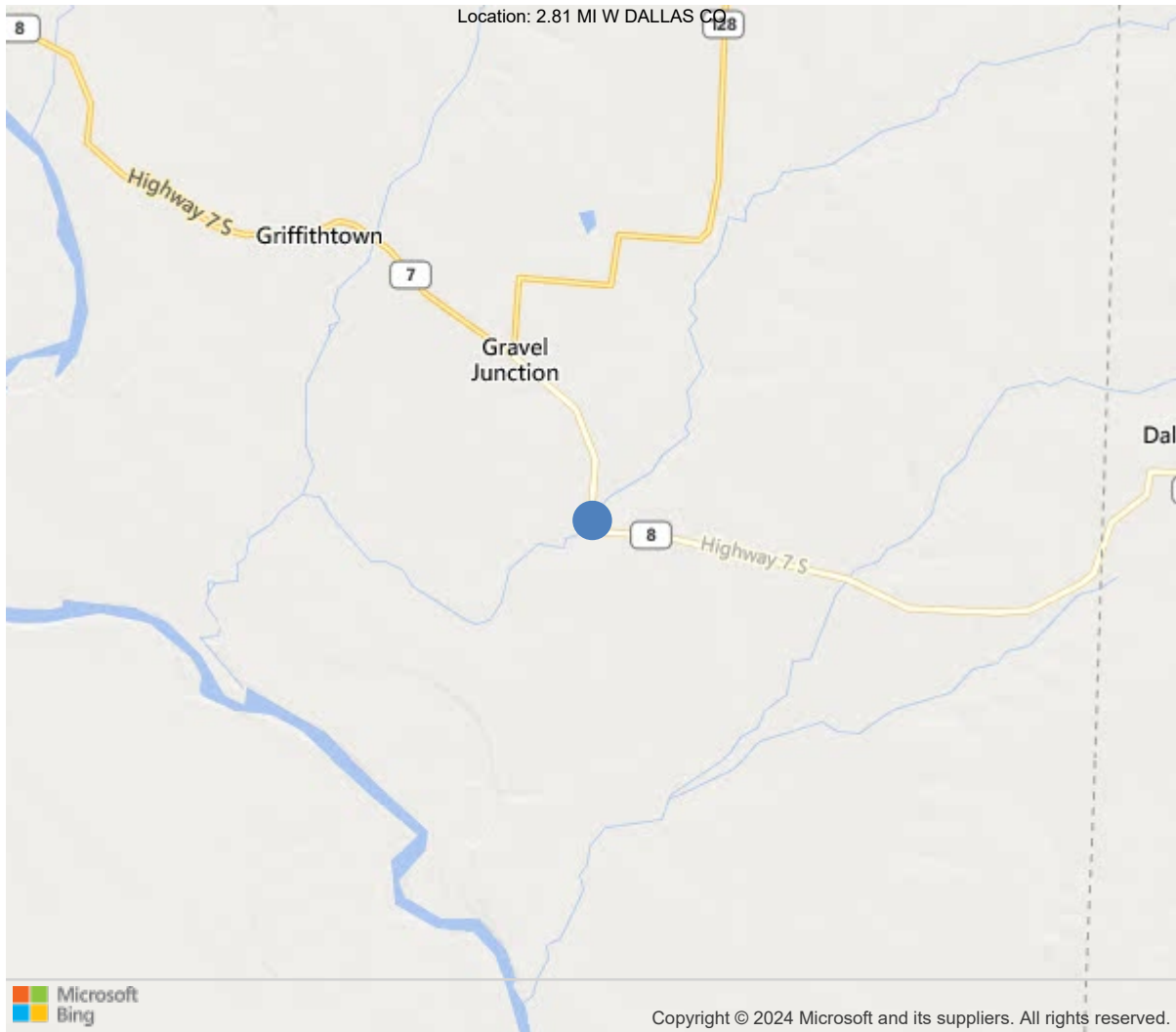
70 - Bridge Posting: 5 - Equal to or above legal loads

Legal Load	Calculated Capacity	Beginning of Bridge Sign Current Value	End of Bridge Sign Current Value
Code 4 (22 Tons)	31		
Code 9 (31 Tons)	35		
Code 5 (40 Tons)	47		

If calculated Capacity is less than the Legal Load Listed, the Bridge Legally Requires Posting Signs to be installed by the Bridge Owner



30"x36" AR



34.03346, -92.93577



Asset #M0177(Routine, Underwater type 2)

SH 7 S-6 LM 2.81 over MILL CR.

Location: 2.81 MI W DALLAS CO

Team Lead: Lee Swan, Inspection Date: 05/11/2022

IDENTIFICATION	
(1) State Names	5 - Arkansas
(8) Structure Number	M0177
(5) Inventory Route	1
(2) Highway Agency District	07 - District 07
(3) County Code	19 - Clark County
(4) Place Code	0
(6) Features Intersected	MILL CR.
(7) Facility Carried	SH 7 S-6 LM 2.81
(9) Location	2.81 MI W DALLAS CO
(11) Mile Point	2.81 mi
(12) Base Highway Network	Yes
(13) LRS Inventory Rte & Subrte	0000007060
(16) Latitude	34.033463
(17) Longitude	-92.935768
(98) Border Bridge State Code	
(99) Border Bridge Structure No.	
STRUCTURE TYPE AND MATERIAL	
(43) Main Structure Type	122
Material	1 - Concrete
Type	22 - Channel beam
(44) Approach Structure Type	00
Material	0 - Other
Type	0 - Other
(45) No. of Spans in Main Unit	2
(46) No. of Approach Spans	0
(107) Deck Structure Type	2 - Concrete Precast Panels
(108) Wearing Surface/Protective System	
Type of Wearing Surface	6 - Bituminous
Type of Membrane	0 - None
Type of Deck Protection	0 - None
AGE AND SERVICE	
(27) Year Built	1955
(106) Year Reconstructed	0
(42) Type of Service	15
On	1 - Highway
Under	5 - Waterway
(28) Lane	
On	2
Under	0
(29) Average Daily Traffic	1400
(30) Year of ADT	2018
(109) Truck ADT	1 %
(19) Bypass, Detour Length	57 mi
GEOMETRIC DATA	
(48) Length of Maximum Span	19 ft
(49) Structure Length	38 ft
(50) Curb or Sidewalk Width	
Left	0 ft
Right	0 ft
(51) Bridge Roadway Width Curb to Curb	24 ft
(52) Deck Width Out to Out	25.3 ft
(32) Approach Roadway Width (W/Shoulders)	30.8 ft
(33) Bridge Median	0 - No median
(34) Skew	0 Deg
(35) Structure Flared	0 - No flare
(10) Inventory Route Min Vert Clear	99.99 ft
(47) Inventory Route Total Horiz Clear	25.6 ft
(53) Min Vert Clear Over Bridge Rdwy	99.99 ft
(54) Min Vert Underclear	0 ft
Ref:	
(55) Min Lat Underclear RT	99.9 ft
Ref:	
(56) Min Lat Underclear LT	0 ft
NAVIGATION DATA	
(38) Navigation Control	0 - No navigation control on w
(111) Pier Protection	1 - Navigation protection not
(39) Navigation Vertical Clearance	0 ft
(116) Vert-Lift Bridge Nav Min Vert Clear	0 ft
(40) Navigation Horizontal Clearance	0 ft

CLASSIFICATION	
(112) NBIS Bridge Length	Y
(104) Highway System	0
(26) Functional Class	6 - Rural Minor Arterial
(100) Defense Highway	0 - The inventory route is not
(101) Parallel Structure	N - No parallel structure exists
(102) Direction of Traffic	2 - way traffic
(103) Temporary Structure	
(105) Federal Lands Highways	0 - N/A
(110) Designated National Network	0 - The inventory route is not
(20) Toll	3 - On free road. The structure
(21) Maintain	1 - State Highway Agency
(22) Owner	1 - State Highway Agency
(37) Historical Significance	5 - Bridge is not eligible for
CONDITION	
(58) Deck	7
(59) Superstructure	5
(60) Substructure	6
(61) Channel & Channel Protection	5
(62) Culverts	N
LOAD RATING AND POSTING	
(31) Design Load	2 - M 13.5 / H 15
(63) Operating Rating Method	1
(64) Operating Rating	
Type	1 - Load Factor(LF)
Rating	41
(65) Inventory Rating Method	1 - Load Factor(LF)
(66) Inventory Rating	
Type	
Rating	25
(70) Bridge Posting	5 - Equal to or above legal loads
(41) Structure Open/Posted/Closed	A - Open, no restriction
APPRAISAL	
(67) Structural Evaluation	
(68) Deck Geometry	4
(69) Clearances, Vertical/Horizontal	N
(71) Waterway Adequacy	5
(72) Approach Roadway Alignment	5
(36A) Bridge Railings	0 - Inspected feature does not meet
(36B) Transitions	0 - Inspected feature does not meet
(36C) Approach Guardrail	0 - Inspected feature does not meet
(36D) Approach Guardrail Ends	0 - Inspected feature does not meet
(113) Scour Critical Bridges	5 - Bridge foundations determined to
PROPOSED IMPROVEMENTS	
(75) Type of Work	31 - Replacement of bridge or
(76) Length of Structure Improvement	62 ft
(94) Bridge Improvement Cost	\$ 0
(95) Roadway Improvement Cost	\$ 156
(96) Total Project Cost	\$ 324
(97) Year of Improvement Cost Estimate	2002
(114) Future ADT	1448
(115) Year of Future ADT	2028

INSPECTIONS *			
(90) Inspection Date	05/11/2022		
(91) Frequency	24		
(92) Critical Feature Inspection	Done	Freq. (Mon)	Date
A: Fracture Critical Detail	No		
B: Underwater Inspection	No		
C: Other Special Inspection	No		
* The inspection date and frequency information in this box contains the current NBI date and frequency information. Please refer to the report header for the date this inspection was conducted.			



Asset #M0177(Routine, Underwater type 2)

District: 07, County: 19 - Clark County

Team Lead: Lee Swan, Inspection Date: 05/11/2022

General Observation

JPR 05-08-2018 This structure is logged South to North.
Boat needed.

2' curb 2.5 height cap x2

59 - Superstructure (5 - FAIR CONDITION - all primary structural elements are sound but may have minor section loss, cracking, spalling or scour.)

Most inside units have some horizontal cracking along stems. (noted as "bottom of channel" in a previous inspection. Span 2 Unit 2 has a 12' spalled area with exposed steel, minor section loss. Unit 6 span one has cracks that are beginning to spall. 2' .

60 - Substructure (6 - SATISFACTORY CONDITION - structural elements show some minor deterioration.)

All piles have normal wear

A-114 - Underwater Inspection General Observation

Underwater type 2 performed this date. Soundings located in Channel tab. Channel profile in files tab.



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SH 7 S-6 LM 2.81 over MILL CR.

Location: 2.81 MI W DALLAS CO

Team Lead: Lee Swan, Inspection Date: 05/11/2022

ELEMENTS	DESCRIPTION	UNITS	TOTAL	CS1	CS2	CS3	CS4
16	Reinforced Concrete Top Flange	SF	961	961	0	0	0
510	Wearing Surfaces	SF	912	852	0	60	0
3220	Crack (Wearing Surface)	SF	60	0	0	60	0
(510-16) Large cracks at the joints.							
110	Reinforced Concrete Open Girder/Beam	LF	266	143	63	60	0
1080	Delamination/Spall/Patched Area	LF	9	0	8	1	0
1090	Exposed Rebar	LF	14	0	0	14	0
1130	Cracking (RC and Other)	LF	100	0	55	45	0
(110) Span 1, unit 2, exposed rebar Span 2 unit 2, exposed rebar Large cracks on the bottom of the unit legs. Scattered spalls and delamination.							
C							
216	Timber Abutment	LF	51	22	25	4	0
1140	Decay/Section Loss	LF	25	0	25	0	0
6000	Scour	LF	4	0	0	4	0
(216) Missing boards abutment 2. Left side wing walls washed out. Deep void behind the timber cribbing at abutment 2.							
228	Timber Pile	EA	15	14	1	0	0
1140	Decay/Section Loss	EA	1	0	1	0	0
(228) Bent 2 pile 1 minor decay							
234	Reinforced Concrete Pier Cap	LF	75	70	2	3	0
1080	Delamination/Spall/Patched Area	LF	1	0	0	1	0
1090	Exposed Rebar	LF	2	0	0	2	0
1130	Cracking (RC and Other)	LF	2	0	2	0	0
(234) Bent 1 has cracks and delamination. Bents 2 and 3 have exposed rebar.							
330	Metal Bridge Railing	LF	76	76	0	0	0
515	Steel Protective Coating	SF	243	243	0	0	0

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Location: 2.81 MI W DALLAS CO

Team Lead: Lee Swan, Inspection Date: 05/11/2022

Superstructure

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Comment: Most inside units have some horizontal cracking along stems. (noted as "bottom of channel" in a previous inspection. Span 2 Unit 2 has a 12' spalled area with exposed steel, minor section loss. Unit 6 span one has cracks that are beginning to spall. 2' .



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(234) Bent 1 has cracks and delamination. Bents 2 and 3 have exposed rebar.							

60 - Substructure (6 - SATISFACTORY CONDITION - structural elements show some minor deterioration.)

Comment: All piles have normal wear



Elevation



Elevation



Typical deck



Approach



Bent 3 left side, exposed rebar in the face of the cap.



Unit 3 span 2, large longitudinal cracks on the bottom of the unit leg.



Unit 4 span 2, large cracks on the bottom of the unit leg.



Bent 3, large void behind the timber cribbing.



Span 2 unit 2, exposed rebar in the channel leg.



Typical soffit



Span 1, unit 2, exposed rebar and large delaminated area in the unit leg.



Span 1 unit 6, large crack in the leg of the channel unit.



Bent 2, exposed rebar in the bottom of the cap.

Maintenance Needs

Date Reported: 05/06/2014

Priority: D- Routine

Type of Work: (Inactive) (Inactive) 9 - None

Status: Monitor

Component:

Deficiency Description

Abutment 2 Lt. top back board is missing.

Remarks



Bent 3



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Team Lead: Lee Swan, **Inspection Date:** 05/11/2022

Routine Maintenance

Check Box Maintenance Items

Type of Maintenance	Is recommended?
A-54 - Sealable Deck Cracks	
A-55 - Deck Washing Needed	
A-56 - Joint Cleaning/Flushing Needed	
A-57 - Beam End and Bearing Paint Needed	
A-58 - Cap Cleaning/Flushing Needed	
A-59 - Joint Repair Needed	
A-60 - Full Beam Painting Needed	
A-61 - Polymer Overlay Advised	
A-62 - Hydro and LMC Advised	
A-63 Missing/Incorrect Log Mile Signage	
A-64 - Vegetation Removal Requested	

A-54 - Sealable Deck Cracks

A-55 - Deck Washing Needed

A-56 - Joint Cleaning/Flushing Needed



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A-57 - Beam End and Bearing Painting Needed

A-58 - Cap Cleaning/Flushing Needed

A-59 - Joint Repair Needed

A-60 - Full Beam Painting Needed

A-61 - Polymer Overlay Advised

A-62 - Hydro and LMC Advised

A-63 - Missing/Incorrect Log Mile Signage

A-64 - Vegetation Removal Requested



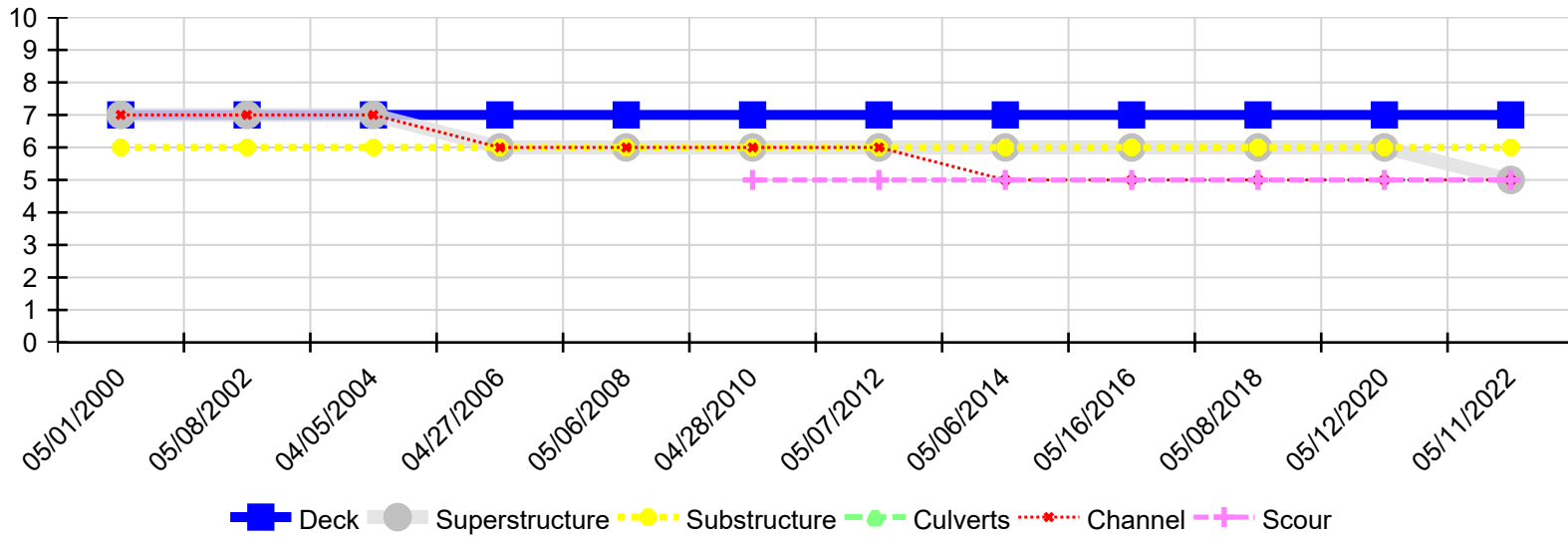
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Team Lead: Lee Swan, Inspection Date: 05/11/2022

Condition History



Inspection Date	Deck	Superstructure	Substructure	Culverts	Channel	Scour
05/11/2022	7	5	6	N	5	5
05/12/2020	7	6	6	N	5	5
05/08/2018	7	6	6	N	5	5
05/16/2016	7	6	6	N	5	5
05/06/2014	7	6	6	N	5	5
05/07/2012	7	6	6	N	6	5
04/28/2010	7	6	6	N	6	5
05/06/2008	7	6	6	N	6	N
04/27/2006	7	6	6	N	6	N
04/05/2004	7	7	6	N	7	N
05/08/2002	7	7	6	N	7	N
05/01/2000	7	7	6	N	7	N