

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	A40056	1	12
BOBBY HOPPER TUNNEL PAVEMENT FRICTION IMPVTS. (S)						

"A FULLY CONTROLLED ACCESS FACILITY"  
 ARKANSAS DEPARTMENT OF TRANSPORTATION  
 CONSTRUCTION PLANS FOR STATE HIGHWAY



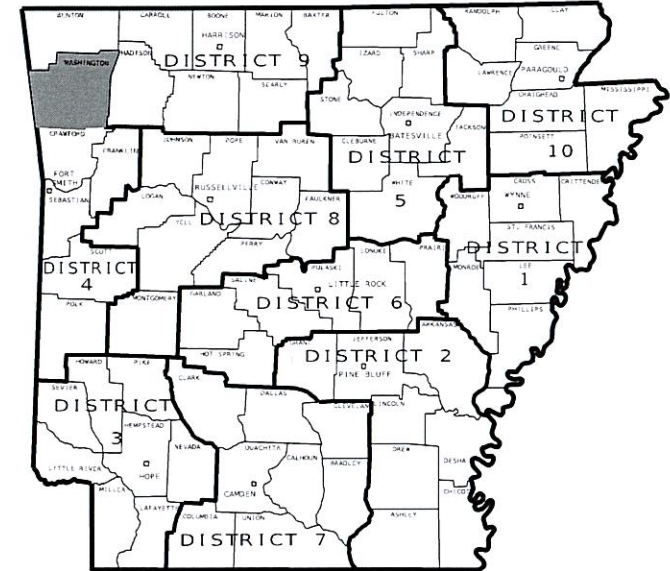
BOBBY HOPPER TUNNEL PAVEMENT FRICTION IMPVTS. (S)

WASHINGTON COUNTY

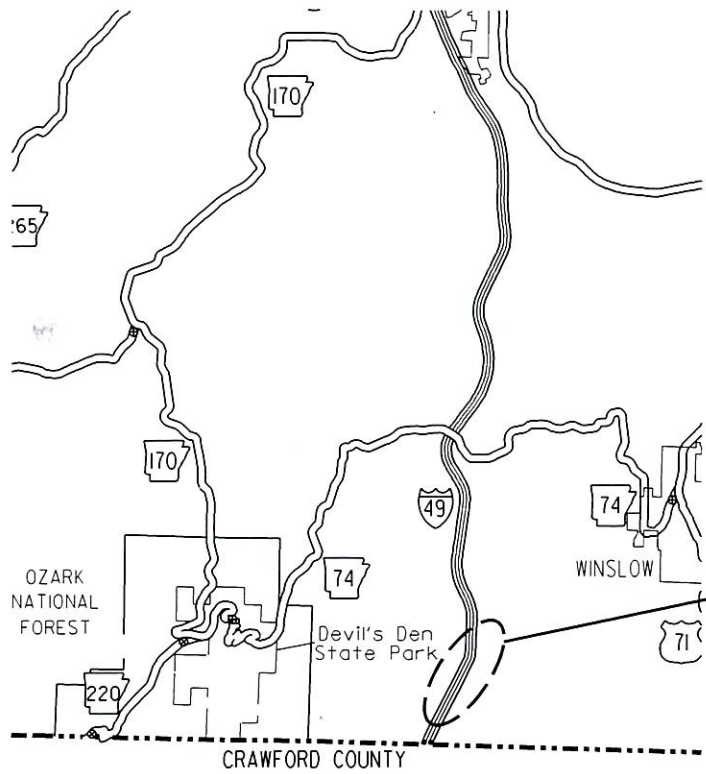
ROUTE I-49 SECTION 28

JOB A40056

FEDERAL AID PROJ. 9992



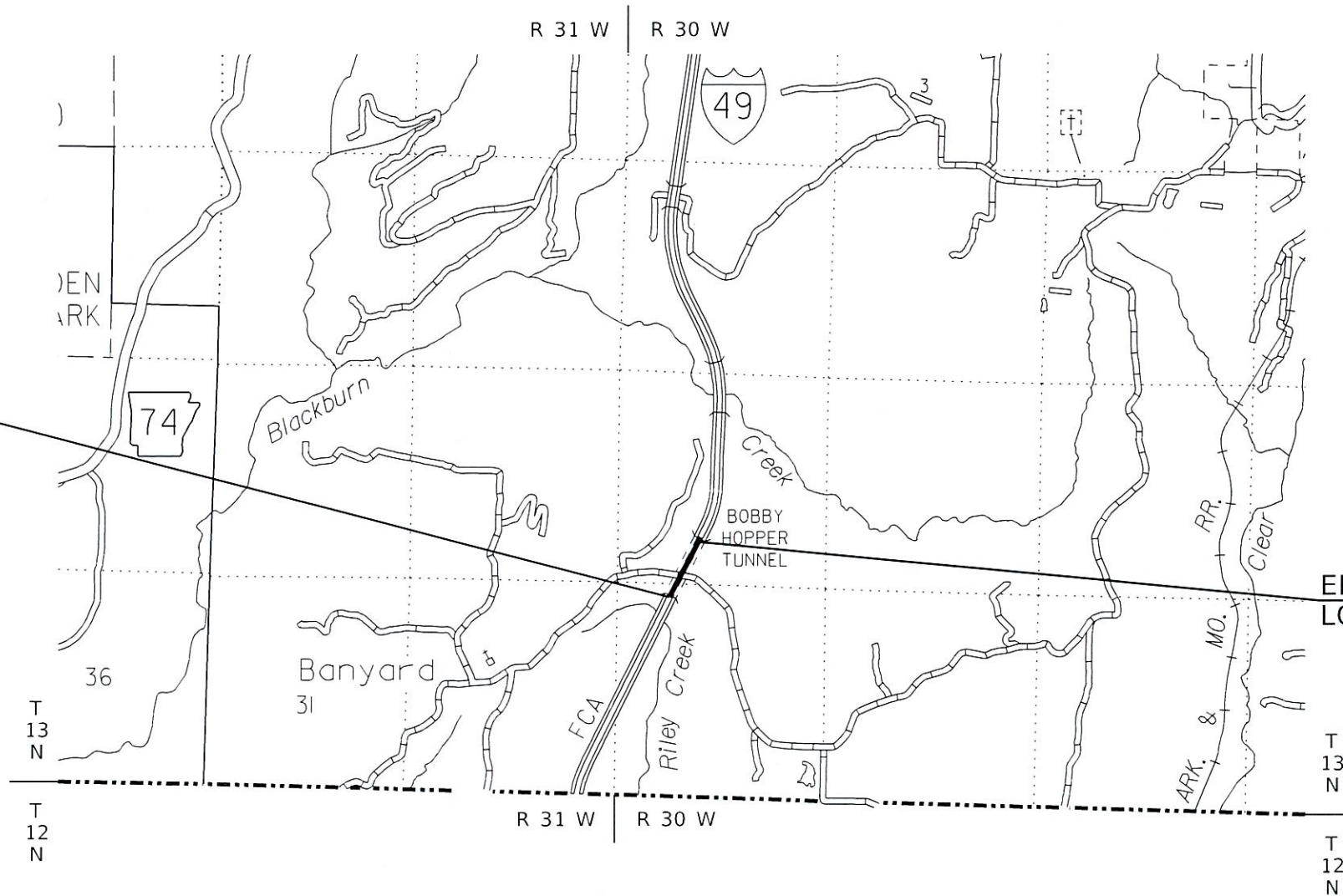
ARK. HWY. DIST. NO. 4



VICINITY MAP

NOT TO SCALE

BEGIN JOB A40056  
 LOG MILE 41.24



END JOB A40056  
 LOG MILE 41.54

APPROVED



*Kelvin Rex Vines* 5-14-26

CHIEF ENGINEER - OPERATIONS

	BEGIN PROJECT	MID-POINT OF PROJECT	END PROJECT
LATITUDE	N 35° 45' 58"	N 35° 46' 04"	N 34° 46' 12"
LONGITUDE	W 94° 11' 26"	W 94° 11' 21"	W 94° 11' 17"

LENGTH OF PROJECT CALCULATED ALONG C			
GROSS LENGTH OF PROJECT	1584.00 FEET OR	0.300 MILES	
NET " " ROADWAY	1584.00 " "	0.300 MILES	
NET " " BRIDGES	00.00 " "	0.000 MILES	
NET " " PROJECT	1584.00 " "	0.300 MILES	

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	A40056	2	12
INDEX OF SHEETS AND STANDARD DRAWINGS						



Digitally Signed 04/24/2026

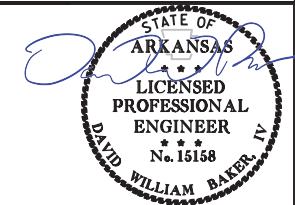
### INDEX OF SHEETS

SHEET NO.	TITLE
1	TITLE SHEET
2	INDEX OF SHEETS AND STANDARD DRAWINGS
3	GOVERNING SPECIFICATIONS AND GENERAL NOTES
4	TYPICAL SECTIONS OF IMPROVEMENT
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9	PERMANENT PAVEMENT MARKING DETAILS
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### ROADWAY STANDARD DRAWINGS

DRWG.NO.	TITLE	DATE
CPTJ-6A	TRANSVERSE & LONGITUDINAL JOINTS FOR CONCRETE PAVEMENT (NON-REINFORCED)	11-06-25
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PM-2	PERMANENT PAVEMENT MARKING ON ACCESS CONTROLLED ROADWAYS	05-14-20
TC-1	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	08-14-25
TC-1A	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	04-09-26
TC-2	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	08-14-25
TC-3	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	08-14-25
TC-6	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	04-09-26

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		6	ARK.	A40056	3	12
GOVERNING SPECIFICATIONS AND GENERAL NOTES						



Digitally signed by Baker, David W  
Date: 2026.05.12 15:20:51-05'00'

**GOVERNING SPECIFICATIONS**

ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2014, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS:

NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
100-3	CONTRACTOR'S LICENSE
100-4	DEPARTMENT NAME CHANGE
102-2	ISSUANCE OF PROPOSALS
102-3	PREQUALIFICATION OF BIDDERS
103-2	CONTACT INFORMATION FOR MOTORIST DAMAGE CLAIMS
105-4	MAINTENANCE DURING CONSTRUCTION
107-2	RESTRAINING CONDITIONS
108-1	LIQUIDATED DAMAGES
108-2	WORK ALLOWED PRIOR TO ISSUANCE OF WORK ORDER
108-3	WORK ORDER FOR FIXED COMPLETION DATE CONTRACTS
306-1	QUALITY CONTROL AND ACCEPTANCE
501-3	PORTLAND CEMENT CONCRETE PAVEMENT
603-1	LANE CLOSURE NOTIFICATION
604-1	RETROREFLECTIVE SHEETING FOR TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
604-3	TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES (MASH)
800-1	STRUCTURES
802-5	CONCRETE FOR STRUCTURES
JOB A40056	ASSESSMENT OF WORKING DAYS - MAINTENANCE OF TRAFFIC
JOB A40056	BIDDING REQUIREMENTS AND CONDITIONS
JOB A40056	DOCUMENTATION OF PAYMENTS MADE - PROMPT PAYMENT (SIGNET)
JOB A40056	ELECTRONIC TICKETING SYSTEMS (E-TICKETING)
JOB A40056	ENHANCED THERMOPLASTIC PAVEMENT MARKING
JOB A40056	LIQUIDATED DAMAGES PROCEDURE FOR BID LETTINGS
JOB A40056	LOCAL WORK ZONE LAW ENFORCEMENT
JOB A40056	MAINTENANCE OF TRAFFIC
JOB A40056	MANDATORY ELECTRONIC CONTRACT
JOB A40056	MANDATORY ELECTRONIC DOCUMENT SUBMITTAL
JOB A40056	MOBILE SPEED NOTIFICATION SYSTEM (SPECIAL)
JOB A40056	PRIME CONTRACTOR PERFORMANCE EVALUATION
JOB A40056	ROADWAY CONSTRUCTION CONTROL - PAVEMENT PRESERVATION
JOB A40056	SEQUENCE OF CONSTRUCTION
JOB A40056	SILANE PROTECTIVE SURFACE TREATMENT FOR CONCRETE PAVEMENT
JOB A40056	TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
JOB A40056	VENDOR REGISTRATION REQUIREMENT
JOB A40056	WORK ZONE LAW ENFORCEMENT

**GENERAL NOTES**

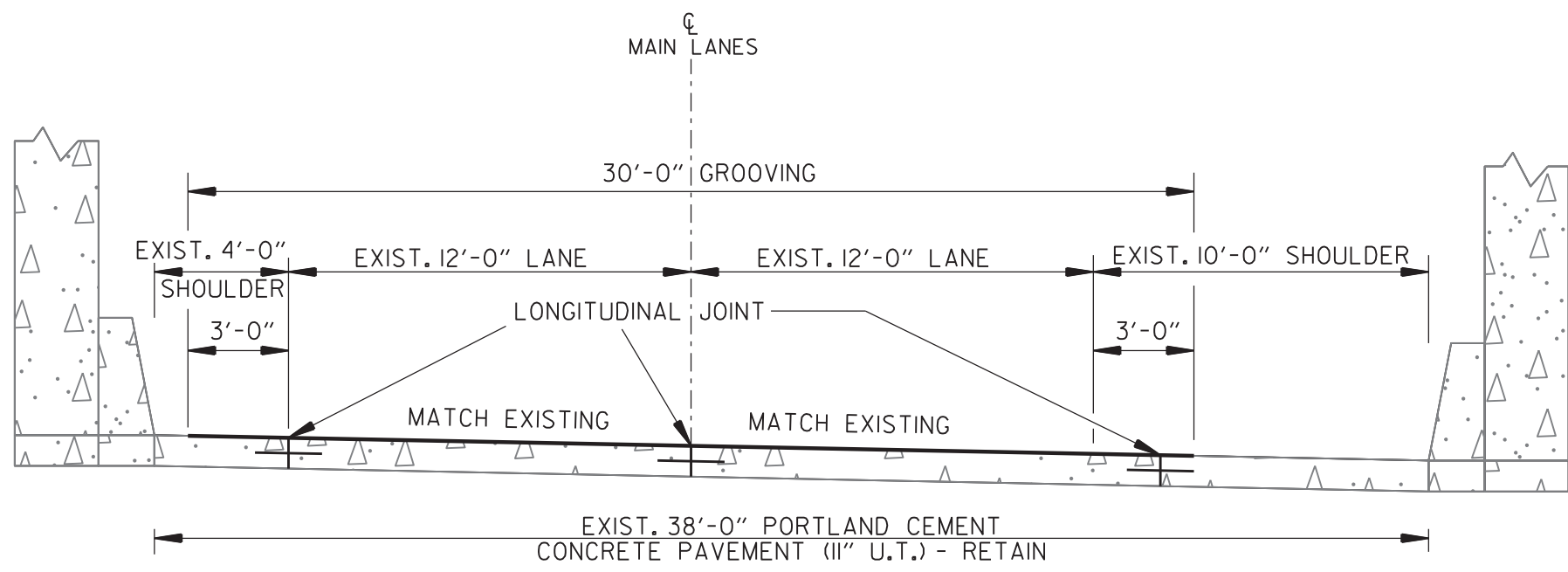
- ALL PIPE LINES, POWER, TELEPHONE, AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY THE RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH OWNERS.
- ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
- ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
- THE SEQUENCE AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS IS A GENERAL OUTLINE FOR THE CONSTRUCTION OF THIS PROJECT, AND IN NO WAY IS IT INTENDED TO COVER EVERY ITEM IN THE PROJECT. ITEMS NOT CRITICAL TO THE CONSTRUCTION SEQUENCE MAY BE CONSTRUCTED IN ANY STAGE AS APPROVED BY THE RESIDENT ENGINEER.
- THE CONTRACTOR SHALL FURNISH AND MAINTAIN STD. W8-1 "BUMP" SIGNS (30" X 30") WITH BLACK LEGEND ON ORANGE BACKGROUND AT ALL TRANSVERSE JOINTS EXPOSED TO TRAFFIC.

RA40056\_INDEX.SPECS.SPECIAL.DETAILS.DGN CREATED: 1/6/2026 4:11/2026 SAVED: 4/1/2026

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	A40056	4	12
TYPICAL SECTIONS OF IMPROVEMENT						



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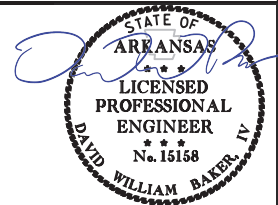


**NOTE:**

THE PORTLAND CEMENT CONCRETE PAVEMENT SURFACE SHALL BE GIVEN A GROOVED FINISH AS SPECIFIED FOR FINAL FINISHING IN SUBSECTION 802.19 FOR CLASS 7 GROOVED BRIDGE ROADWAY SURFACE FINISH.

I-49 SECTION 28  
L.M. 41.24 TO L.M. 41.54  
(SHOWN IN DIRECTION OF TRAFFIC)

RA40056\_TYPICAL SECTIONS.DGN    CREATED: 1/6/2026    SAVED: 4/1/2026

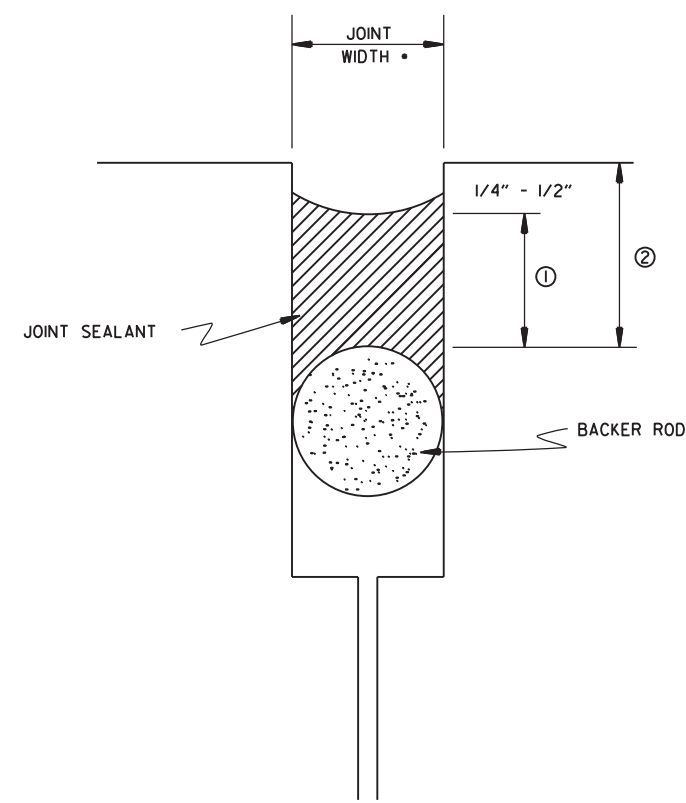


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JOINT CONFIGURATION FOR  
TYPE 3 & 4 JOINT SEALANT

JOINT WIDTH		BACKER ROD DIAMETER	
INCHES			
1/4	1/4	3/8	1/2
3/8	1/4	1/2	1/2
1/2	1/4	5/8	1/2
5/8	5/16	3/4	9/16
3/4	3/8	7/8	7/8
4/8	7/16	1	11/16
1	1/2	1 1/4	3/4
1 TO 1 1/2	1/2	1 1/4+	3/4

NOTE: JOINTS GREATER THAN 1 1/2" IN WIDTH SHALL BE SEALED WITH TYPE 5 JOINT SEALANT.

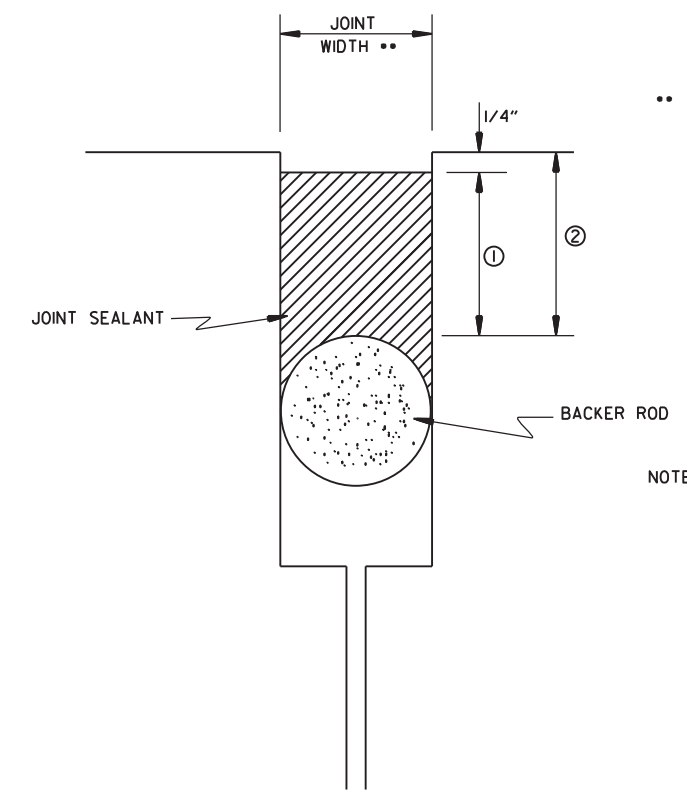


• CONTRACTION JOINTS SHALL BE SAWED TO MIN. WIDTH OF 3/8". WARPING & LONGITUDINAL JOINTS SHALL BE SAWED TO MIN. WIDTH OF EXISTING WIDTH + 1/8" (1/16" ON EACH SIDE).

DETAILS OF TYPE A OR TYPE B JOINT REHABILITATION

JOINT CONFIGURATION FOR  
TYPE 5 JOINT SEALANT

JOINT WIDTH	APPROX. WIDTH TO DEPTH RATIO	SEALANT THICKNESS ①	BACKER ROD DIAMETER	BACKER ROD PLACEMENT DEPTH ②
INCHES				
1/4	1:2	1/2	3/8	3/4
3/8		3/4	1/2	1
1/2		1	5/8	1 1/4
5/8	1:1.75	1 1/4	3/4	1 1/2
3/4		1 3/8	7/8	1 5/8
7/8		1 1/2	1	1 3/4
1	1:1.6	1 5/8	1 1/4	1 7/8
1 TO 3		1 5/8+	1 1/4+	1 7/8+



•• WARPING & LONGITUDINAL JOINTS SHALL BE SAWED TO MIN. WIDTH OF EXISTING WIDTH + 1/8" (1/16" ON EACH SIDE).

NOTE: FOR JOINTS WIDER THAN 1 1/2", THE CONTRACTOR SHALL HAVE THE OPTION OF COMPLETELY FILLING THE JOINT IN LIEU OF USING A BACKER ROD.

DETAILS OF TYPE B JOINT REHABILITATION

REFER TO SECTION 509 OF THE STANDARD SPECIFICATIONS FOR ADDITIONAL INFORMATION.

RA40056\_INDEX\_SPECS\_SPECIAL\_DETAILS.DGN CREATED: 1/6/2026 4/1/2026 SAVED: 4/1/2026

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	A40056	6	12

MAINTENANCE OF TRAFFIC DETAILS

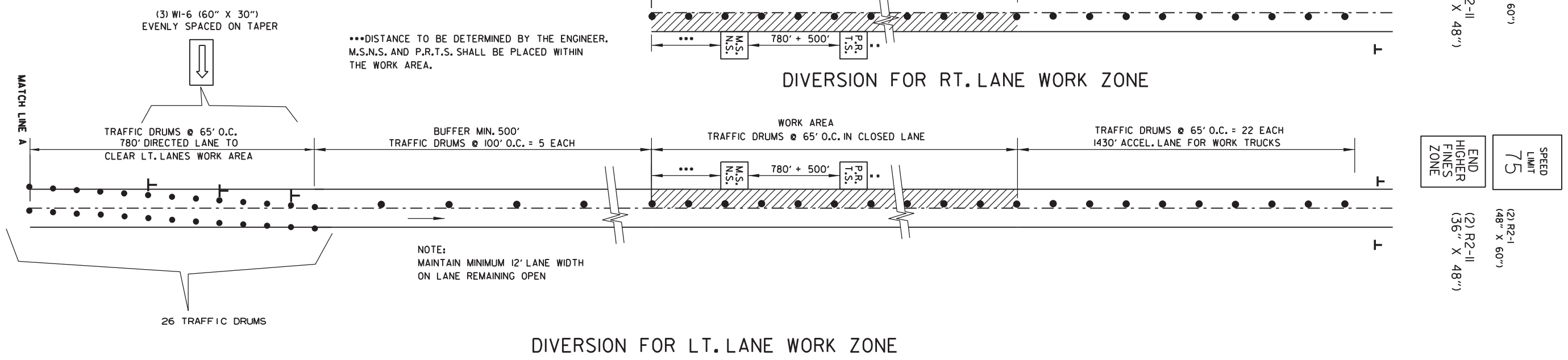
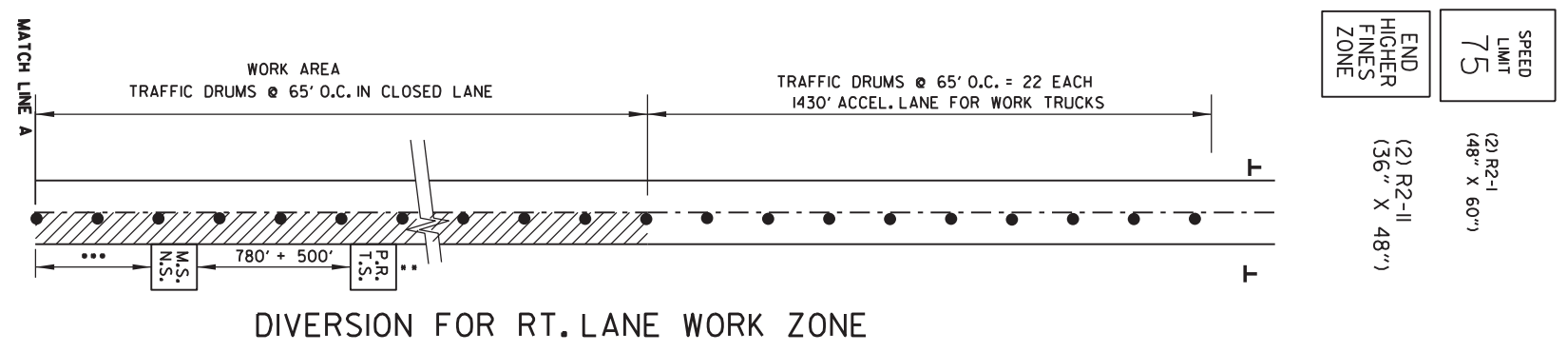
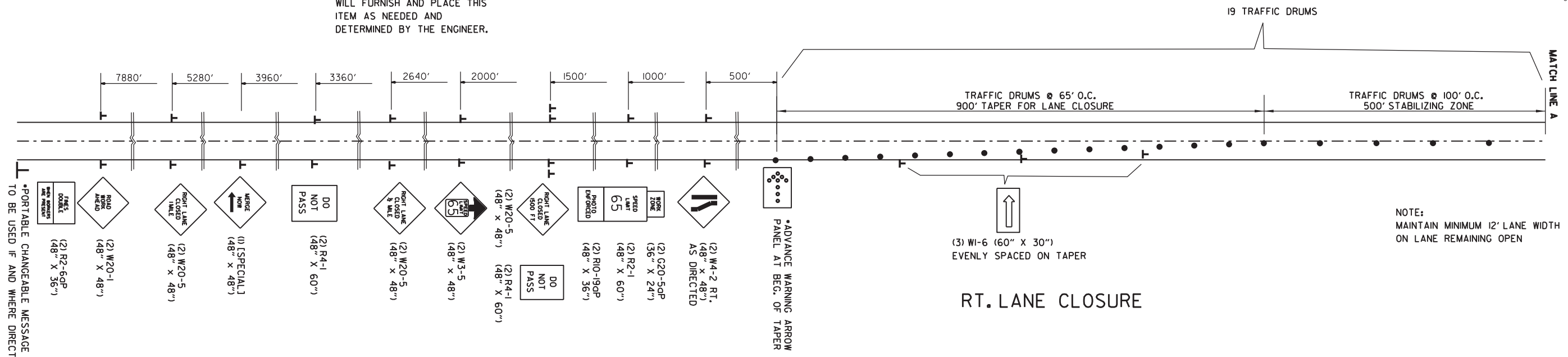


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M.S. = MOBILE SPEED NOTIFICATION SYSTEM  
N.S. =

P.R. = PHOTO RADAR TRAILER SYSTEM  
T.S. =

••NOTE: IF NO PAY ITEM FOR A PHOTO RADAR TRAILER SYSTEM IS INCLUDED IN THE CONTRACT DOCUMENTS, ARDOT WILL FURNISH AND PLACE THIS ITEM AS NEEDED AND DETERMINED BY THE ENGINEER.



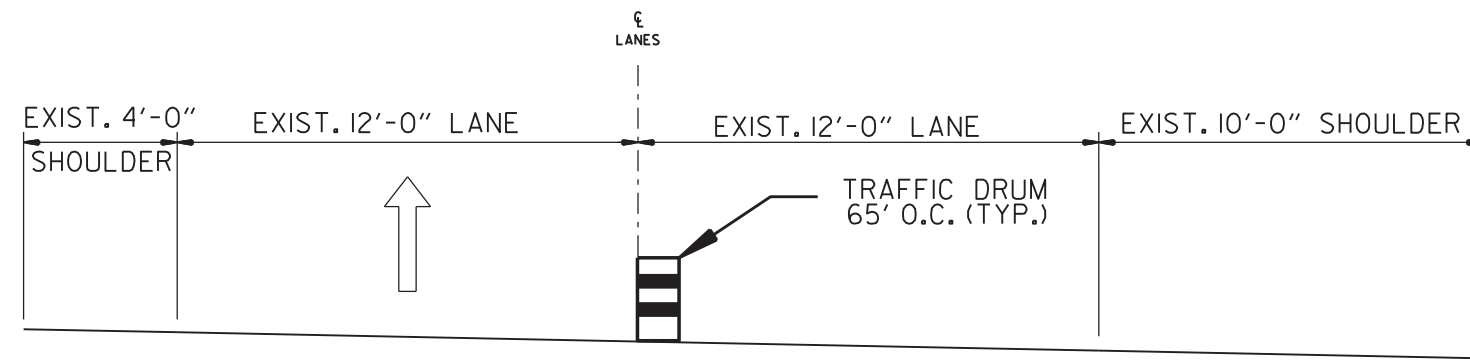
LANE CLOSURES AND DIVERSIONS FOR 1-49

RA40056\_MOT DETAILS.DGN CREATED: 11/6/2026 11:16:26 AM SAVER: 4/1/2026

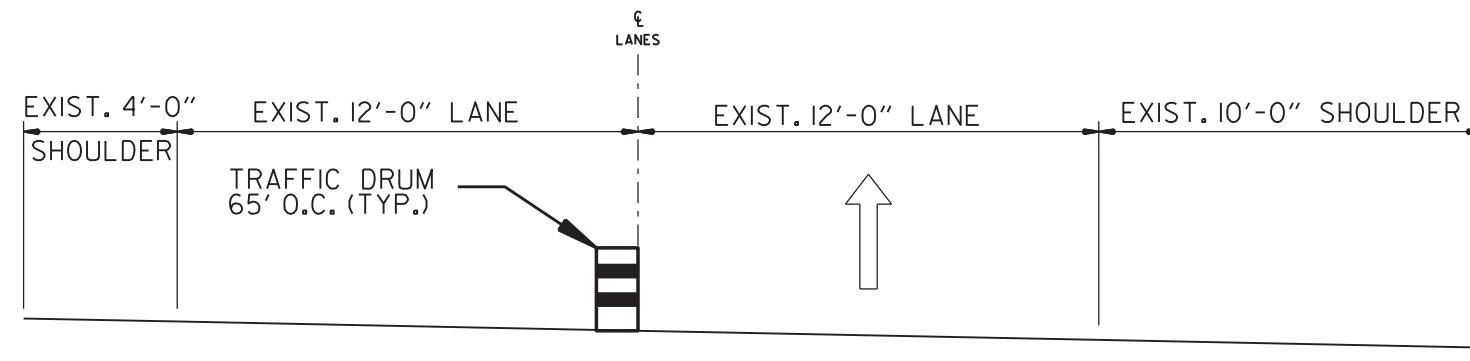
DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	A40056	7	12
MAINTENANCE OF TRAFFIC DETAILS						



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LOCATION OF TRAFFIC DRUMS FOR MAINTENANCE OF TRAFFIC  
OUTSIDE LANE CLOSED  
(SHOWN IN DIRECTION OF TRAFFIC)



LOCATION OF TRAFFIC DRUMS FOR MAINTENANCE OF TRAFFIC  
INSIDE LANE CLOSED  
(SHOWN IN DIRECTION OF TRAFFIC)

LOCATION OF TRAFFIC DRUMS  
MAINTENANCE OF TRAFFIC DETAILS

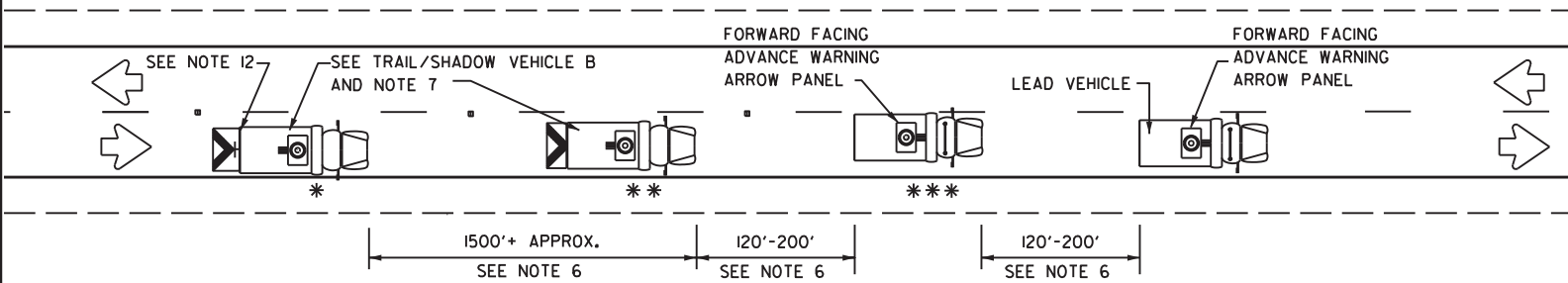
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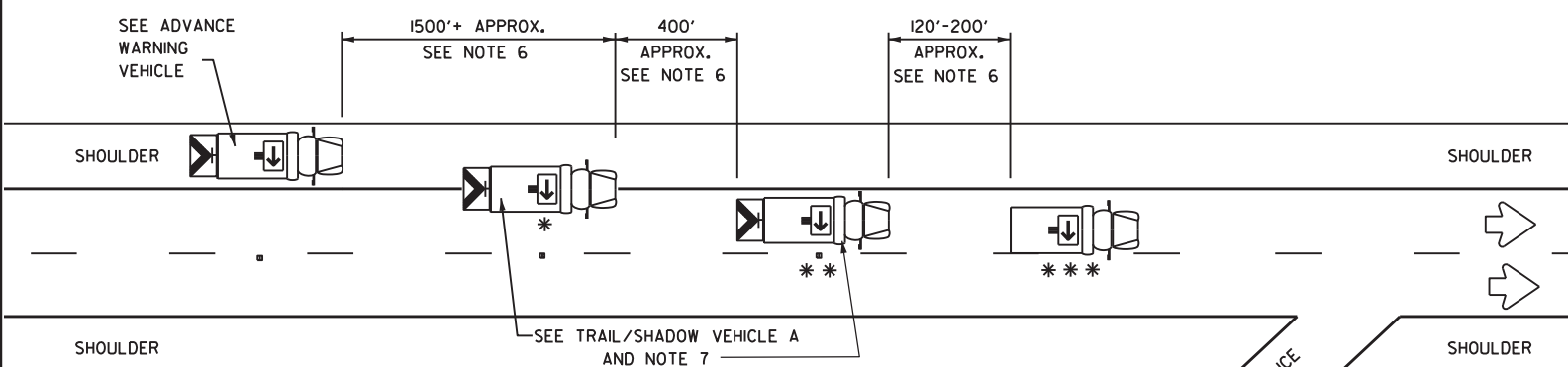
MAINTENANCE OF TRAFFIC DETAILS



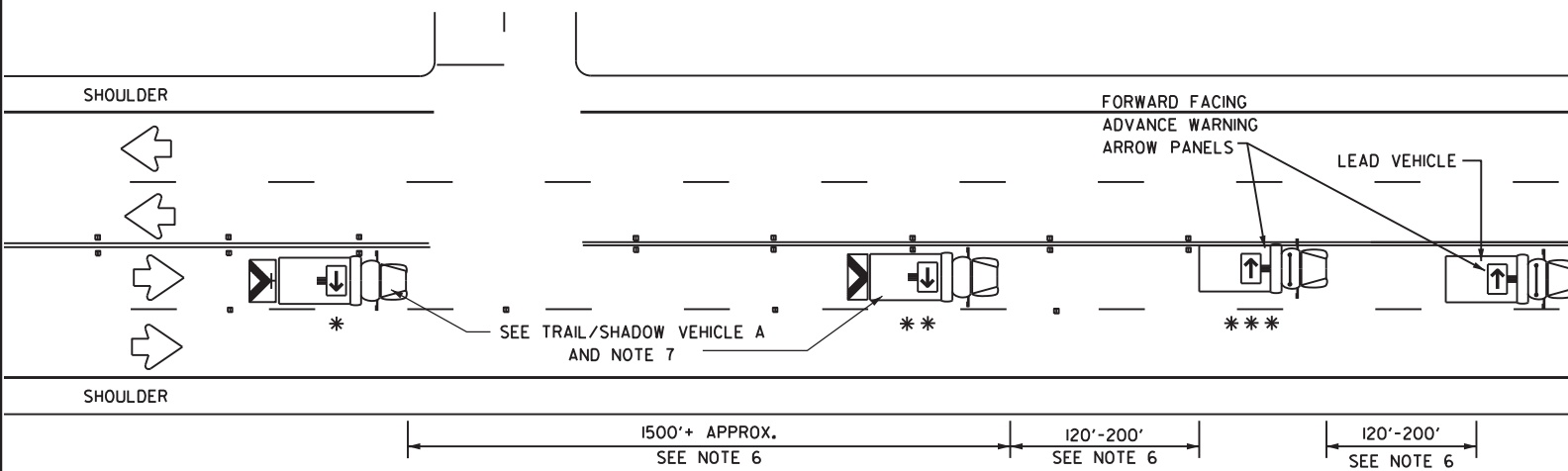
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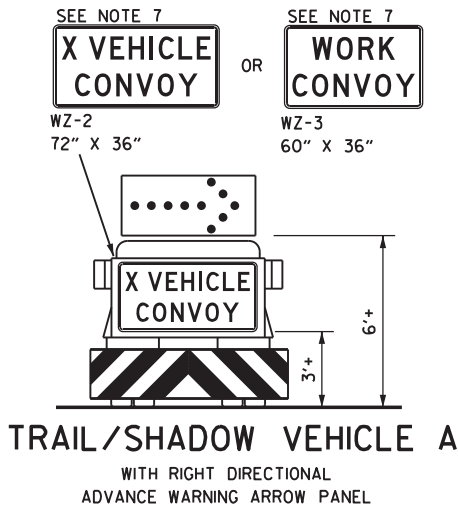
TWO LANE HIGHWAY  
(WORK ON TRAVEL LANE)



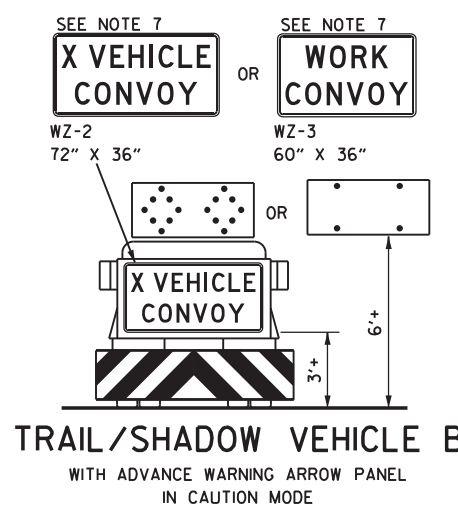
DIVIDED MULTILANE HIGHWAY



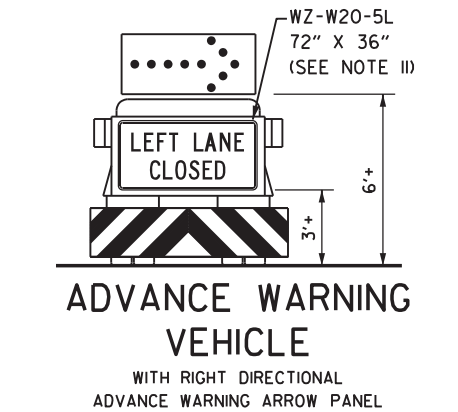
UNDIVIDED MULTILANE HIGHWAY



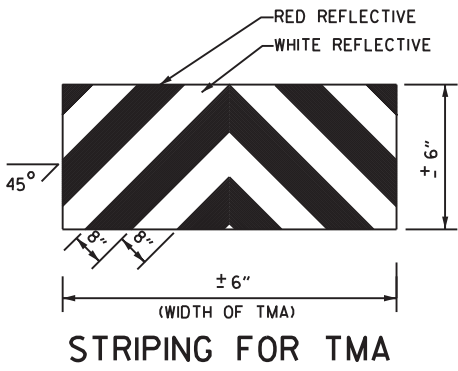
TRAIL/SHADOW VEHICLE A  
WITH RIGHT DIRECTIONAL  
ADVANCE WARNING ARROW PANEL



TRAIL/SHADOW VEHICLE B  
WITH ADVANCE WARNING ARROW PANEL  
IN CAUTION MODE



ADVANCE WARNING  
VEHICLE  
WITH RIGHT DIRECTIONAL  
ADVANCE WARNING ARROW PANEL



STRIPING FOR TMA

LEGEND			
*	TRAIL VEHICLE	ADVANCE WARNING ARROW PANEL DISPLAY	
**	SHADOW VEHICLE		
***	WORK VEHICLE		RIGHT DIRECTIONAL
	HEAVY WORK VEHICLE		LEFT DIRECTIONAL
	TRUCK MOUNTED ATTENUATOR (TMA)		DOUBLE ARROW
	TRAFFIC FLOW		CAUTION (ALTERNATING DIAMOND OR 4 CORNER FLASH)

TYPICAL APPLICATION - MOBILE WORK ZONE

GENERAL NOTES:

1. TRAIL, SHADOW, AND LEAD VEHICLES SHALL BE EQUIPPED WITH ADVANCE WARNING ARROW PANELS AS ILLUSTRATED.
2. THE USE OF AMBER OR GREEN HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS ON ALL VEHICLES ARE REQUIRED. VEHICLE HAZARD WARNING SIGNALS SHALL NOT BE USED INSTEAD OF THE VEHICLE'S HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.
3. THE USE OF TRUCK MOUNTED ATTENUATORS (TMA) ON THE SHADOW VEHICLE, ADVANCE WARNING AND TRAIL VEHICLE ARE REQUIRED.
4. EACH VEHICLE SHALL HAVE TWO-WAY RADIO COMMUNICATION CAPABILITY.
5. WHEN WORK CONVOYS MUST CHANGE LANES, THE TRAIL VEHICLE SHOULD CHANGE LANES FIRST TO SHADOW THE OTHER CONVOY VEHICLES. WHEN WORK CONVOY EXITS THE ROADWAY, THE TRAIL VEHICLE SHOULD EXIT LAST.
6. VEHICLE SPACING BETWEEN THE TRAIL VEHICLE AND THE SHADOW VEHICLE WILL VARY DEPENDING ON SIGHT DISTANCE RESTRICTIONS AND QUEUE LENGTHS. MOTORISTS APPROACHING THE CONVOY SHOULD BE ABLE TO SEE THE TRAIL VEHICLE IN TIME TO SLOW DOWN AND/OR CHANGE LANES AS THEY APPROACH THE TRAIL VEHICLE. VEHICLE SPACING BETWEEN THE WORK VEHICLE AND SHADOW VEHICLE AND VEHICLE SPACING BETWEEN THE WORK VEHICLE AND LEAD VEHICLE MAY VARY ACCORDING TO TERRAIN, WORK ACTIVITY AND OTHER FACTORS.
7. X VEHICLE CONVOY (WZ-2) OR WORK CONVOY (WZ-3) SIGNS SHALL BE USED ON TRAIL VEHICLES AND SHADOW VEHICLES AS SHOWN. AS AN OPTION 48" X 48" DIAMOND SHAPED WORK CONVOY (WZ-2A) OR X VEHICLE CONVOY (WZ-3A) SIGNS MAY BE USED WHERE ADEQUATE MOUNTING SPACE EXISTS. WHEN USED, THE X VEHICLE CONVOY SIGN SHALL HAVE THE NUMBER OF THE CONVOY VEHICLES DISPLAYED ON THE SIGN IN THE NUMBER DESIGNATION "X" LOCATION. THE X VEHICLE CONVOY SIGN SHALL NOT BE USED ON THE SHADOW VEHICLE.
8. FOR DIVIDED HIGHWAYS WITH TWO OR THREE LANES IN ONE DIRECTION, THE APPROPRIATE LEFT LANE CLOSED (WZ-W20-5L), RIGHT LANE CLOSED (WZ-W20-5R), OR CENTER LANE CLOSED (WZ-W20-5C) SIGN SHOULD BE USED ON THE ADVANCE WARNING VEHICLE. AS AN OPTION, A PORTABLE CHANGEABLE MESSAGE SIGN (PCMS) OR TRUCK MOUNTED CHANGEABLE MESSAGE SIGN (TMCMS) WITH A MINIMUM CHARACTER HEIGHT OF 12", AND DISPLAYING THE SAME LEGEND MAY BE SUBSTITUTED FOR THESE SIGNS. AN APPROPRIATE DIRECTIONAL ARROW DISPLAY, SIMULATING THE SIZE AND LEGIBILITY OF THE ADVANCE WARNING ARROW PANEL MAY BE USED IN THE SECOND PHASE OF THE PCMS/TMCMS MESSAGE. WHEN THIS IS DONE, THE ARROW BOARD WILL NOT BE REQUIRED ON THE ADVANCE WARNING VEHICLE.
9. A DOUBLE ARROW SHALL NOT BE DISPLAYED ON THE ADVANCE WARNING ARROW PANEL ON THE ADVANCE WARNING VEHICLE.
10. STANDARD DIAMOND SHAPE VERSIONS OF THE WZ SERIES SIGNS MAY BE USED AS AN OPTION IF THE RECTANGULAR SIGNS SHOWN ARE NOT AVAILABLE.
11. THE ADVANCE WARNING VEHICLE MAY STRADDLE THE EDGELINE WHEN SHOULDER WIDTH MAKES IT NECESSARY.
12. ON TWO-LANE TWO-WAY ROADWAYS, THE WORK AND PROTECTION VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW MOTOR VEHICLE TRAFFIC TO PASS. A DO NOT PASS (R4-1) SIGN SHALL BE PLACED ON THE BACK OF THE REARMOST PROTECTION VEHICLE.
13. MOBILE WORK ZONE METHODS AS DEPICTED IN THIS STANDARD DRAWING SHALL BE UTILIZED ON ALL PROJECTS THAT INCLUDE INSTALLATION OR REMOVAL OF RAISED PAVEMENT MARKERS OR PAVEMENT MARKINGS UNLESS APPROVED BY THE ENGINEER ON PROJECTS LESS THAN 0.5 MILE IN LENGTH.
14. NO DIRECT PAYMENT WILL BE MADE FOR COMPLIANCE WITH THE METHODS DEPICTED IN THIS STANDARD DRAWING (WHICH INCLUDES ALL SIGNS, DEVICES, MATERIALS, LABOR, TOOLS, EQUIPMENT, AND INCIDENTALS NECESSARY TO COMPLY) BUT SHALL BE CONSIDERED SUBSIDIARY TO THE MAINTENANCE OF TRAFFIC PAY ITEM.

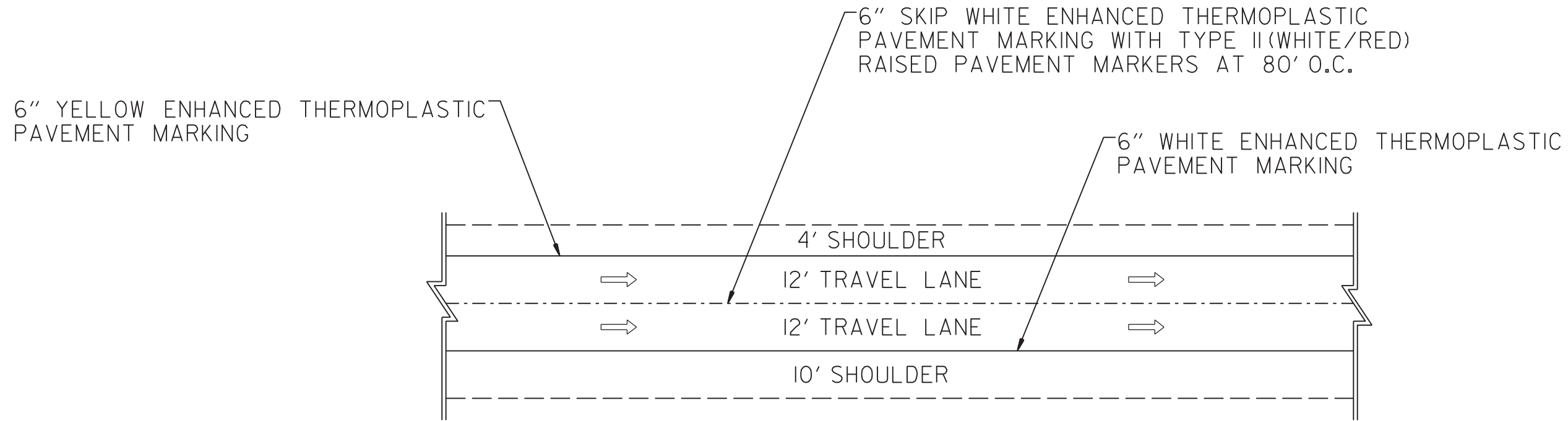
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PERMANENT PAVEMENT MARKING DETAILS						



PERMANENT PAVEMENT MARKINGS

RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) AT 80' O.C. = 40 EA.  
 6" YELLOW ENHANCED THERMOPLASTIC PAVEMENT MARKING = 3168 LIN. FT.  
 6" WHITE ENHANCED THERMOPLASTIC PAVEMENT MARKING = 3960 LIN. FT.



TYPICAL PERMANENT PAVEMENT MARKING LAYOUT

NOTE:  
 SEE PM-1 AND PM-2 FOR MAIN LANE STRIPING DETAILS

RA40056\_PPM DETAILS.DGN CREATED: 1/6/2026 SAVED: 4/1/2026

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		6	ARK.	A40056	10	12
QUANTITIES						



**ADVANCE WARNING SIGNS AND DEVICES**

SIGN NUMBER	DESCRIPTION	SIGN SIZE	I-49, SEC.28 - L.M. 41.24 - 41.54	MAXIMUM NUMBER REQUIRED	TOTAL SIGNS REQUIRED		TRAFFIC DRUMS EACH	* ADVANCE WARNING ARROW PANEL	* PORTABLE CHANGEABLE MESSAGE SIGN	MOBILE SPEED NOTIFICATION SYSTEM (SPECIAL)
			LIN. FT. - EACH		NO.	SQ. FT.		DAY	WEEK	EACH
G20-2	END ROAD WORK	48"x24"	4	4	4	32.0				
G20-5aP	WORK ZONE	36"x24"	2	2	2	12.0				
R2-1	SPEED LIMIT 65 MPH	48"x60"	2	2	2	40.0				
R2-1	SPEED LIMIT 75 MPH	48"x60"	2	2	2	40.0				
R2-6aP	FINES DOUBLE WHEN WORKERS ARE PRESENT	48"x36"	2	2	2	24.0				
R2-11	END HIGHER FINES ZONE	36"x48"	4	4	4	48.0				
R4-1	DO NOT PASS	24"x30"	4	4	4	20.0				
R10-19aP	PHOTO ENFORCED	48"x36"	2	2	2	24.0				
SPECIAL	MERGE NOW WITH ARROW	48"x48"	1	1	1	16.0				
W1-6	DIRECTIONAL ARROW	60"x30"	6	6	6	75.0				
W3-5	SPEED LIMIT 65 MPH AHEAD	48"x48"	2	2	2	32.0				
W4-2R	RIGHT LANE CLOSED	48"x48"	2	2	2	32.0				
W20-1	ROAD WORK 1 MILE	48"x48"	4	4	4	64.0				
W20-1	ROAD WORK 1/2 MILE	48"x48"	4	4	4	64.0				
W20-1	ROAD WORK 1500 FEET	48"x48"	4	4	4	64.0				
W20-1	ROAD WORK AHEAD	48"x48"	2	2	2	32.0				
W20-5	RIGHT LANE CLOSED 1 MILE	48"x48"	2	2	2	32.0				
W20-5	RIGHT LANE CLOSED 1/2 MILE	48"x48"	2	2	2	32.0				
W20-5	RIGHT LANE CLOSED 1500 FT.	48"x48"	2	2	2	32.0				
W21-5a	SHOULDER CLOSED	48"x48"	2	2	2	32.0				
WZ-1	TRAFFIC LAWS PHOTO ENFORCED	84"x60"	4	4	4	140.0				
WZ-4	NO HAND-HELD PHONE USE BY DRIVER	78"x60"	4	4	4	130.0				
	TRAFFIC DRUMS		95	95			95			
	ADVANCE WARNING ARROW PANEL		1	1			16			
	PORTABLE CHANGEABLE MESSAGE SIGN		1	1				5		
	MOBILE SPEED NOTIFICATION SYSTEM (SPECIAL)		1	1					1	
<b>TOTALS:</b>						<b>1017.0</b>	<b>95</b>	<b>16</b>	<b>5</b>	<b>1</b>

NOTE: THIS IS A HIGH TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

NOTE: THE QUANTITY OF TRAFFIC DRUMS PROVIDED IS FOR ONE SIDE OF THE ROADWAY FOR THE FULL LENGTH OF THE JOB. HOWEVER, THE INSTALLATION OF TRAFFIC DRUMS SHALL NEVER EXCEED THE ACTUAL WORK AREA BY MORE THAN 1/4 MILE, UNLESS APPROVED BY THE ENGINEER.

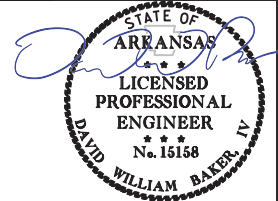
\* QUANTITY ESTIMATED.  
SEE SECTION 104.03 OF THE STD. SPECS.  
TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.

**CONSTRUCTION PAVEMENT MARKINGS AND PERMANENT PAVEMENT MARKINGS**

DESCRIPTION	END OF JOB	CONSTRUCTION PAVEMENT MARKINGS	RAISED PAVEMENT MARKERS	ENHANCED THERMOPLASTIC PAVEMENT MARKING	
	LIN. FT.		LIN. FT.	TYPE II	6"
				(WHITE/RED)	WHITE
			EACH	LIN. FT.	
CONSTRUCTION PAVEMENT MARKINGS	7128	7128			
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED)	40		40		
ENHANCED THERMOPLASTIC PAVEMENT MARKING WHITE (6")	3960			3960	
ENHANCED THERMOPLASTIC PAVEMENT MARKING YELLOW (6")	3168				3168
<b>TOTALS:</b>		<b>7128</b>	<b>40</b>	<b>3960</b>	<b>3168</b>

NOTE: THIS IS A HIGH TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	A40056	11	12
QUANTITIES						



Digitally Signed 04/24/2026

**JOINT REHABILITATION OF CONCRETE PAVEMENT**

LOG MILE	LOG MILE	LOCATION	NUMBER OF JOINTS	AVG. WIDTH PER JOINT	AVG. LENGTH PER JOINT	TYPE A	TYPE B
						LIN. FT.	LIN. FT.
41.24	41.54	NB MAIN LANES	106	30	45	3180	4770
41.24	41.54	SB MAIN LANES	106	30	45	3180	4770
<b>TOTALS:</b>			<b>212</b>			<b>6360</b>	<b>9540</b>

QUANTITY ESTIMATED.  
SEE SECTION 104.03 OF THE STD. SPECS.

**GROOVING**

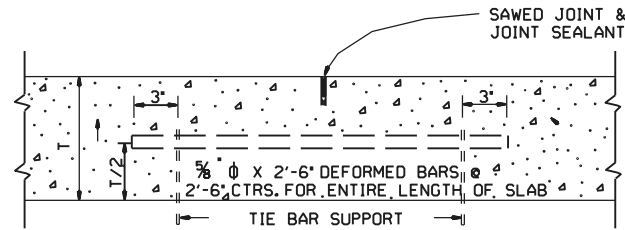
LOG MILE	LOG MILE	LOCATION	LENGTH	WIDTH	SQ. YD.
			LIN. FT.		
41.24	41.54	NB MAIN LANES	1584	30	5280.0
41.24	41.54	SB MAIN LANES	1584	30	5280.0
<b>TOTAL:</b>					<b>10560.0</b>

NOTE: THE PORTLAND CEMENT CONCRETE PAVEMENT SURFACE SHALL BE GIVEN A GROOVED FINISH AS SPECIFIED FOR FINAL FINISHING IN SUBSECTION 802.19 FOR CLASS 7 GROOVED BRIDGE ROADWAY SURFACE FINISH.

**SILANE PROTECTIVE SURFACE TREATMENT**

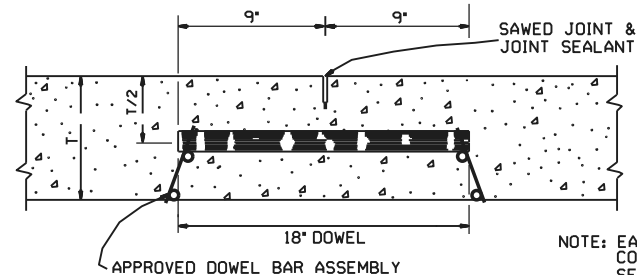
LOG MILE	LOG MILE	LOCATION	LENGTH	WIDTH	SILANE PROTECTIVE SURFACE TREATMENT
			LIN. FT.		SQ. YD.
41.24	41.54	NB MAIN LANES	1584	30	5280
41.24	41.54	SB MAIN LANES	1584	30	5280
<b>TOTAL:</b>					<b>10560</b>





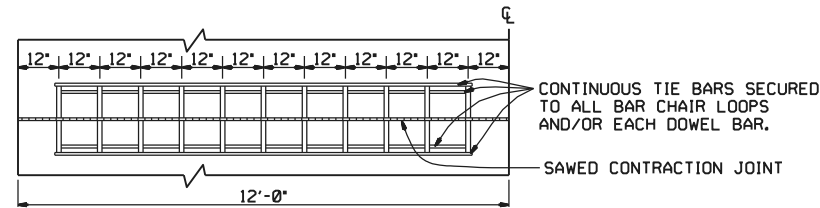
LONGITUDINAL JOINT

NOTE: THE TIE BAR SUPPORT SHOWN ABOVE MAY BE ELIMINATED IF OTHER APPROVED METHODS FOR PLACING AND SUPPORTING THE TIE BARS ARE PROVIDED. TIE BARS SHALL BE 30" FROM TRANSVERSE JOINTS.



ROUND STEEL BAR DOWEL  
1 1/4" DIA. WHEN T < 10'  
1 1/2" DIA. WHEN T ≥ 10'

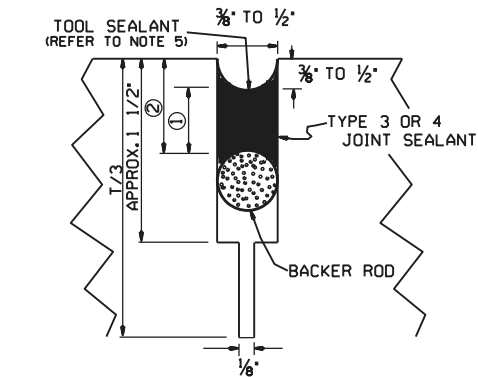
NOTE: EACH DOWEL TO BE COATED ACCORDING TO SECTION 502 OF THE STANDARD SPECIFICATIONS.



ONE-HALF 24' PAVEMENT  
11 DOWELS  
PLAN

NOTE: FOR 20' PAVEMENT USE 19 DOWELS @ 12" CTRS. WITH 12" SPACING FROM C.L. AND EDGE OF SLAB TO FIRST BAR. FOR 15' PAVEMENT USE 14 DOWELS @ 12" CTRS. WITH 12" SPACING FROM C.L. AND EDGE OF SLAB TO FIRST BAR. FOR 26' PAVEMENT USE 25 DOWELS @ 12" CTRS. WITH 12" SPACING FROM C.L. AND EDGE OF SLAB TO FIRST BAR. FOR PAVEMENT WIDTHS OTHER THAN THOSE SHOWN ABOVE, USE DOWELS AT 12" CTRS. WITH 12" MAX. SPACING FROM C.L. TO FIRST BAR. DISTANCE FROM EDGE OF SLAB TO FIRST BAR SHALL BE ADJUSTED TO MAINTAIN 12" DOWEL BAR SPACING

CONTRACTION JOINT DETAILS



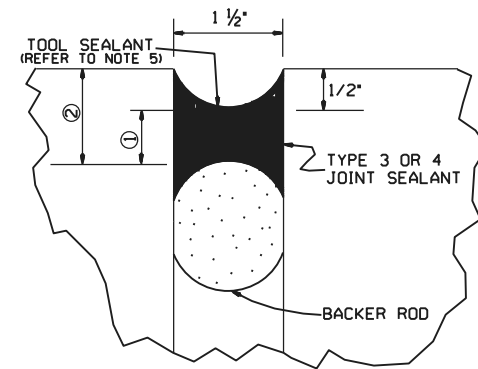
DETAIL OF SAWED CONTRACTION JOINT

JOINT CONFIGURATION FOR TYPE 3 OR 4 JOINT SEALANT

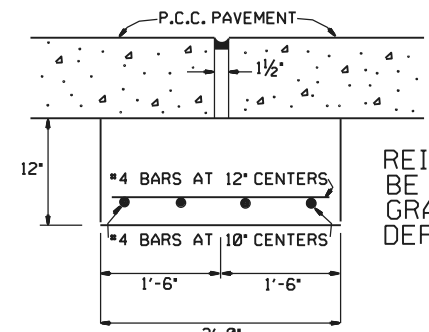
JOINT WIDTH	SEALANT THICKNESS ①	BACKER ROD DIAMETER	BACKER ROD PLACEMENT DEPTH ②
INCHES			
1/4	1/4	3/8	1/2
3/8	1/4	1/2	1/2
1/2	1/4	3/8	1/2
3/4	3/8	1/2	3/4
1	1/2	3/4	3/4
1 1/2	3/4	2	1 1/4

JOINT CONFIGURATION FOR TYPE 5 JOINT SEALANT

JOINT WIDTH	SEALANT THICKNESS ①	BACKER ROD DIAMETER	BACKER ROD PLACEMENT DEPTH ②
INCHES			
1/4	1/4	3/8	3/4
3/8	3/8	1/2	1

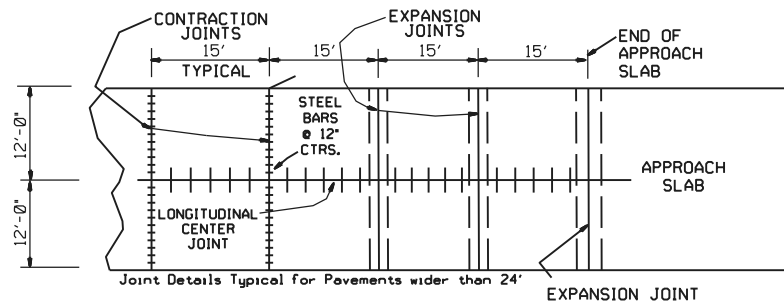


DETAIL OF EXPANSION JOINT

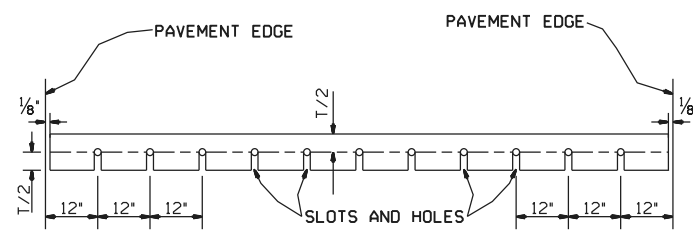


DETAIL OF JOINT SUPPORT FOR EXPANSION JOINTS

REINFORCING SHALL BE GRADE 40 OR GRADE 60 DEFORMED BARS.

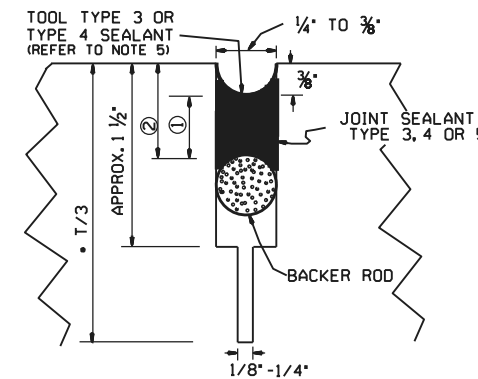


PLAN SHOWING EXPANSION JOINTS AT BRIDGE APPROACH SLABS



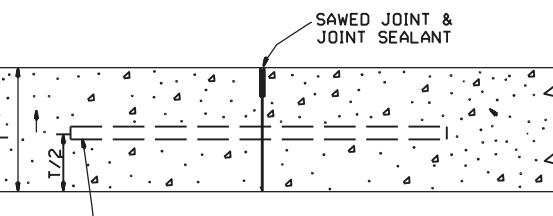
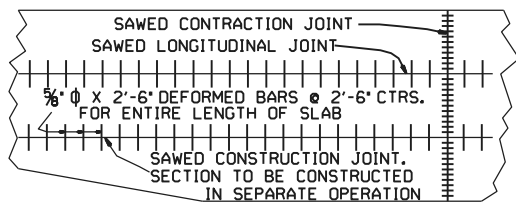
ELEVATION

NOTE: ALL DOWEL BARS SHALL CONFORM TO THE DETAILS FOR CONTRACTION JOINTS.

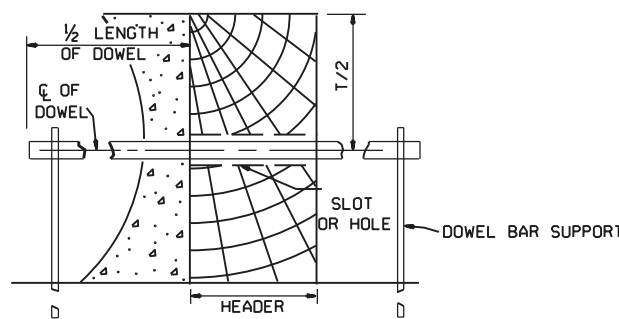


DETAIL OF SAWED LONGITUDINAL JOINT AND LONGITUDINAL CONSTRUCTION JOINT

\*NOTE: T/3 SAW CUT NOT REQUIRED FOR LONGITUDINAL CONSTRUCTION JOINT.



5/8" Ø X 2'-6" DEFORMED BARS @ 2'-6" CTRS. FOR ENTIRE LENGTH OF SLAB  
NOTE: TIE BARS SHALL BE 30" FROM TRANSVERSE JOINTS.  
LONGITUDINAL CONSTRUCTION JOINT




SECTION

TRANSVERSE CONSTRUCTION JOINT

- GENERAL NOTES
1. "T" DENOTES THICKNESS OF SLAB.
  2. DOWEL BARS SHALL BE PLACED IN ACCORDANCE WITH THE DIMENSIONS SHOWN. A TOLERANCE OF PLUS OR MINUS ONE INCH WILL BE ALLOWED FOR THE VERTICAL AND LATERAL PLACEMENT AND A TOLERANCE OF PLUS OR MINUS 1/4" WILL BE ALLOWED FOR THE TILT AND SKEW. DOWEL BARS SHALL BE FIELD COATED FOR A MINIMUM DISTANCE OF 2' GREATER THAN HALF THE LENGTH OF THE BAR WITH AN APPROVED GREASE AS A BOND BREAKER JUST PRIOR TO PLACEMENT OF CONCRETE.
  3. THE EXPANSION JOINT SUPPORT MAY BE CONSTRUCTED WITH CLASS "A", "S" OR PAVING CONCRETE. PAYMENT FOR THE JOINT SUPPORT SHALL BE FOR THE CONTRACT UNIT PRICE BID FOR THE CLASS OF CONCRETE SPECIFIED IN THE PLANS. PAYMENT FOR ALL OTHER WORK AND MATERIALS REQUIRED FOR THE CONSTRUCTION OF THE JOINT SUPPORT SHALL BE INCLUDED IN THE PRICE BID FOR THE ABOVE ITEMS.
  4. CONTRACTION JOINTS SHALL BE CONSTRUCTED ON 15' CENTERS.
  5. TOOLING NOT REQUIRED FOR SELF-LEVELING SILICONE.
  6. UNLESS OTHERWISE SPECIFIED IN THE PLANS, CONCRETE SHOULDERS SHALL BE CONSTRUCTED ACCORDING TO THE DETAILS SHOWN HEREON. CONTRACTION JOINTS SHALL MATCH CONTRACTION JOINTS IN THE LANES.
  7. TIE WIRES IN DOWEL BAR ASSEMBLIES SHALL NOT BE CUT PRIOR TO PLACEMENT OF PAVING CONCRETE.

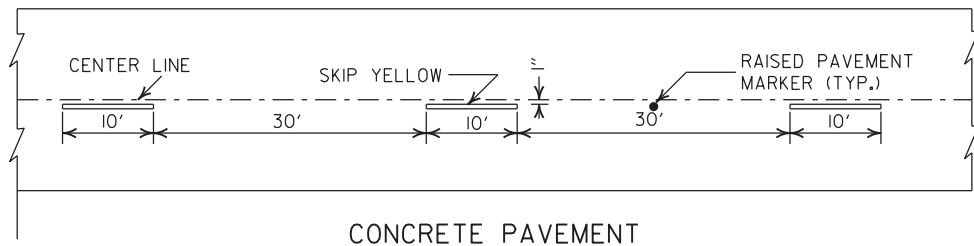
REVISED TIE BAR AND DOWEL SPACING	REV. EXP. JOINT REF ON APP. SLAB	ADDED GENERAL NOTE 7	REMOVED TIE BAR COATING & REVISED GENERAL NOTES	ADDED TOOL SEALANT AND NOTE 5f. REVISED NOTE 3	DATE	REV	DESCRIPTION
11-06-25	11-07-19	05-25-06	10-09-03	11-16-01			



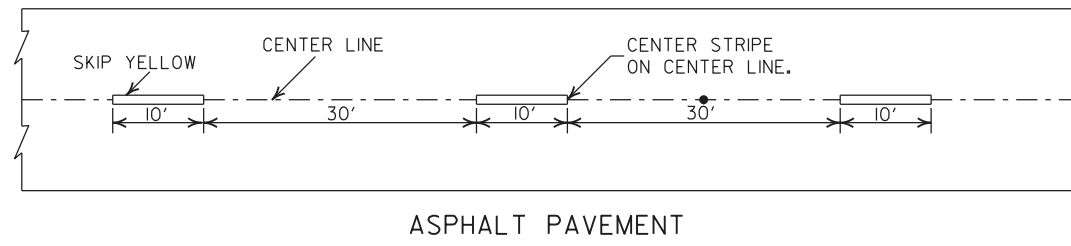
ARKANSAS STATE  
HIGHWAY COMMISSION

**TRANSVERSE & LONGITUDINAL JOINTS FOR CONCRETE PAVEMENT (NON-REINFORCED)**

DATE EFFECTIVE	STANDARD DRAWING	CPTJ-6A
11-06-2025		

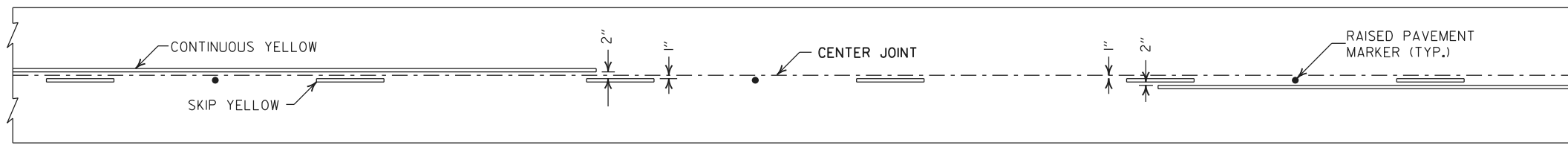


CONCRETE PAVEMENT

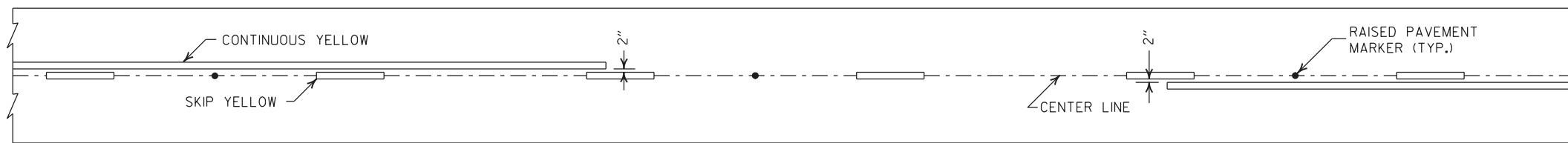


ASPHALT PAVEMENT

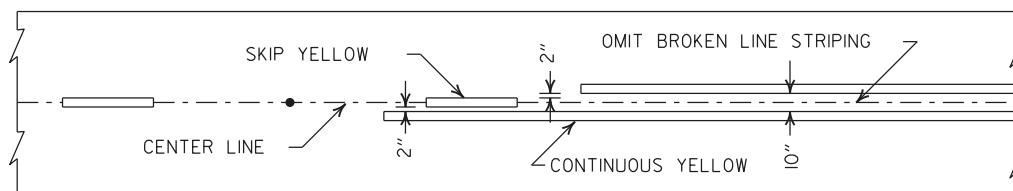
**BROKEN LINE STRIPING**



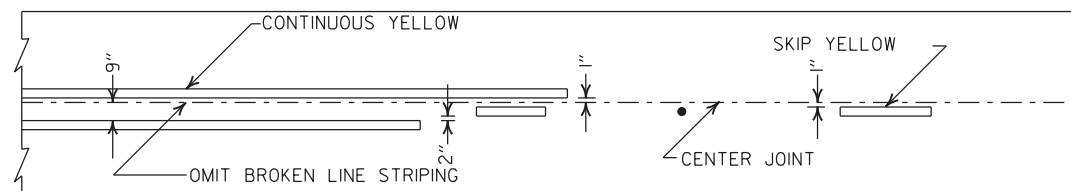
**SOLID LINE STRIPING ON CONCRETE PAVEMENT**



**SOLID LINE STRIPING ON ASPHALT PAVEMENT**



ASPHALT PAVEMENT

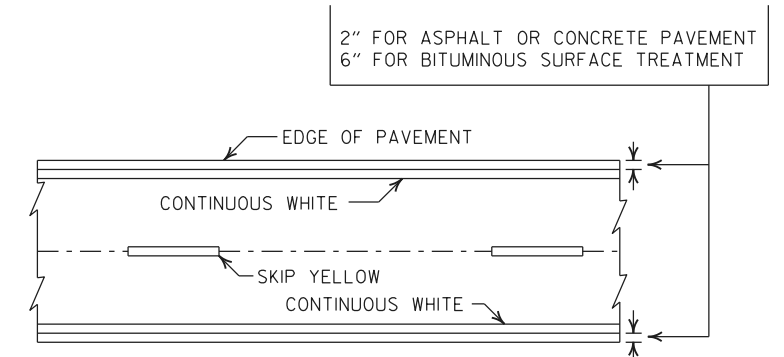


CONCRETE PAVEMENT

**STRIPING AT ADJACENT NO PASSING LANES**

NOTES:

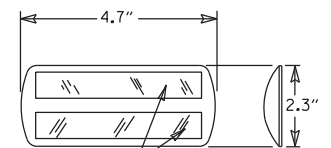
1. REFER TO THE STRIPING DETAILS FOR PAVEMENT MARKING LINE WIDTHS.
2. THIS DRAWING SHALL BE USED IN CONJUNCTION WITH THE LATEST REVISED ADDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."
3. RAISED PAVEMENT MARKERS SHALL BE PLACED ON AN 80 FEET SPACING UNLESS OTHERWISE SHOWN IN THE PLANS.



**PAVEMENT EDGE LINE MARKING**

NOTE: THE RED LENS OF THE TYPE II R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.

TYPE II RED/CLEAR OR YELLOW/YELLOW

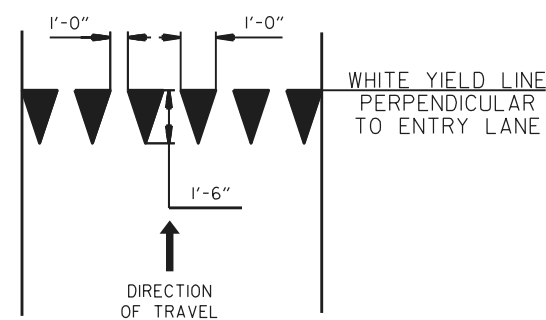


PRISMATIC REFLECTOR

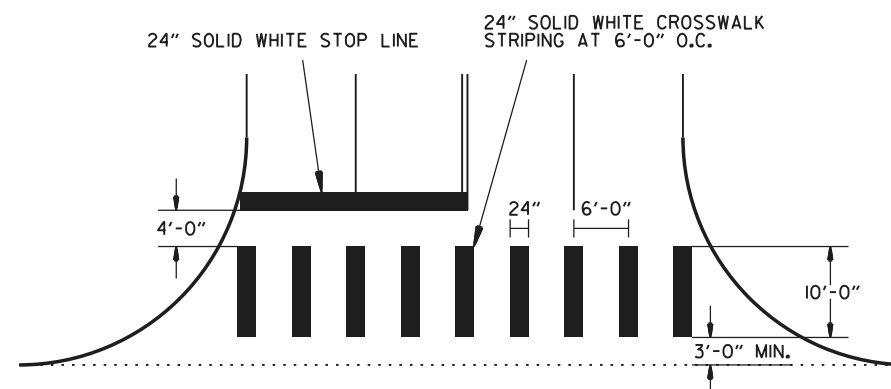
NOTE: DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER, REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE ARDOT QUALIFIED PRODUCTS LIST.



**DETAIL OF STANDARD RAISED PAVEMENT MARKERS**



**YIELD LINE DETAIL**

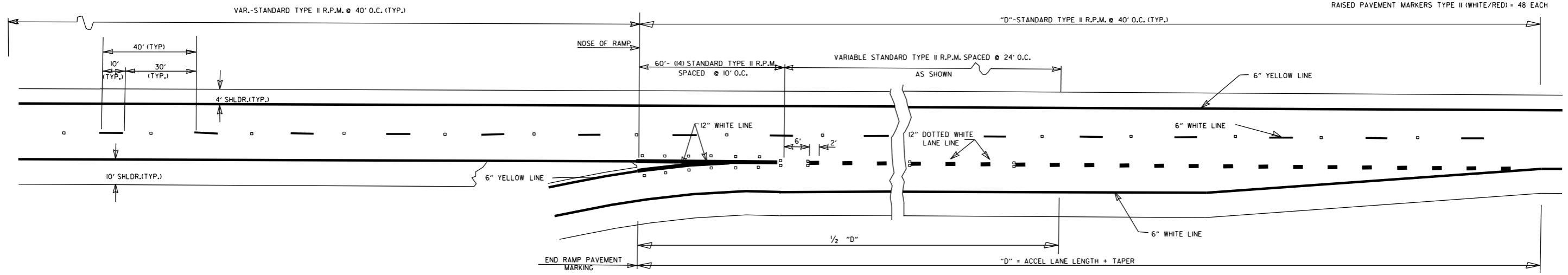


**CROSSWALK AND STOP LINE DETAILS**

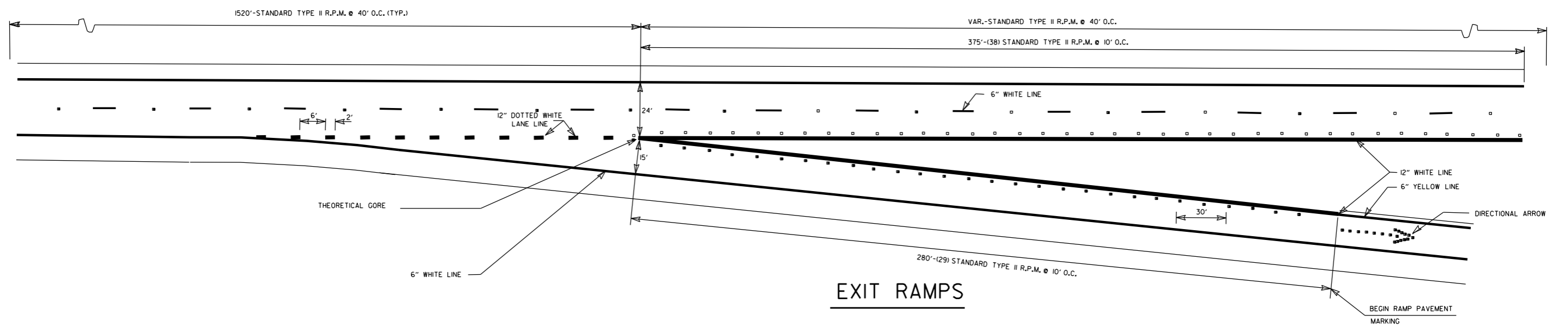
REV. CROSSWALK & STOP LINE DETAILS 04-09-26 REV. STOP LINE DETAILS 02-27-20 REV. STOP LINE DETAILS 06-01-17 ADDED YIELD LINE DETAIL 05-12-16 REVISED LINE WIDTHS, SPACING, & NOTES REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS 09-12-13		<b>AR DOT</b> ARKANSAS DEPARTMENT OF TRANSPORTATION ARKANSAS STATE HIGHWAY COMMISSION	
DATE		DESCRIPTION	
DATE EFFECTIVE		STANDARD DRAWING	
04-09-2026		PM-1	
<b>PAVEMENT MARKING DETAILS</b>			

ENTRANCE RAMP  
12" WHITE = 370 LIN. FT.  
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH

EXIT RAMP  
6" WHITE = 280 LIN. FT.  
12" WHITE = 815 LIN. FT.  
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH  
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 48 EACH



### ENTRANCE RAMPS

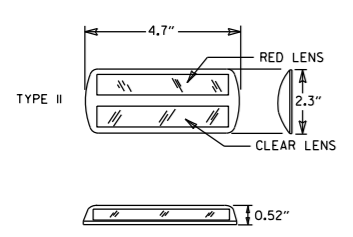


### EXIT RAMPS

GENERAL NOTES:  
THIS DRAWING SHOULD BE CONSIDERED AS TYPICAL ONLY AND THE FINAL LOCATION OF THE STRIPING AND PAVEMENT MARKERS SHALL BE DETERMINED BY THE ENGINEER.

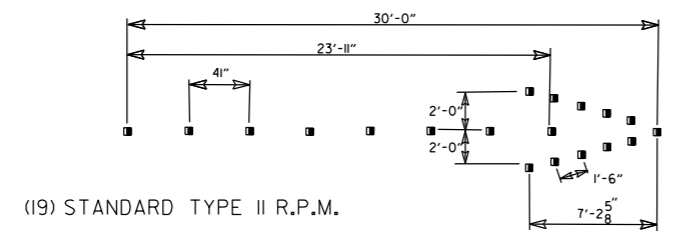
THIS DRAWING SHOULD BE USED IN CONJUNCTION WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", LATEST REVISION.

NOTE:  
DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE ARDOT QUALIFIED PRODUCTS LIST.



DETAIL OF STANDARD RAISED PAVEMENT MARKERS

NOTE:  
THE RED LENS OF THE TYPE II R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.




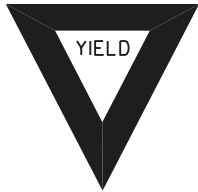



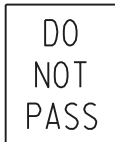

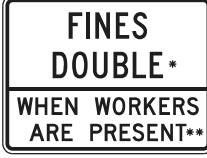


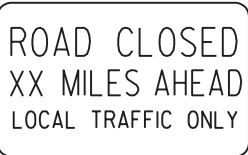
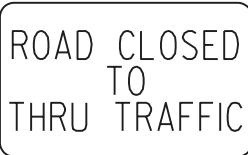








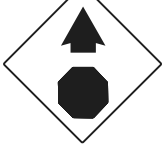
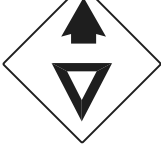
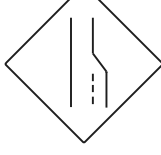














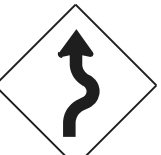


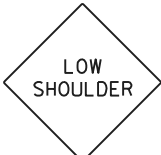

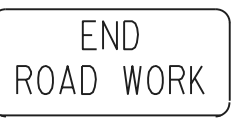
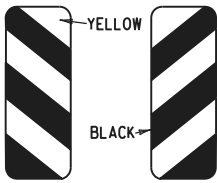
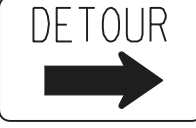

(19) STANDARD TYPE II R.P.M.  
DIRECTIONAL ARROWS

DATE	REVISION	FILMED
05-14-20	REMOVED CROSSHATCH MARKINGS ON EXIT RAMPS	
11-07-19	REVISED DOTTED PAV'T MARKINGS; ADDED CROSSHATCH MARKINGS ON EXIT RAMPS	
12-8-16	REVISED RAISED PAV'T MARKERS FOR 80' SPACING; REVISED WIDTH OF STRIPING	
9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
7-26-12	REVISED RPM NOTATION	
12-15-11	REVISED RPMs ACCORDING TO LATEST POLICY	
11-17-10	REMOVED PLOWABLE PAVEMENT MARKERS	
6-3-10	REVISED PER 2009 MUTCD	
11-18-04	REVISED NOTES	
8-22-02	ADDED & REVISED NOTES; REV. ENTRANCE & EXIT RAMPS	
5-18-00	REMOVED HASHMARKS	
7-02-98	CHANGED TYPES TO ROMAN NUMERALS	
4-26-96	ADDED DIMENSIONS & QUANTITIES; REVISED LANE WIDTH ON EXIT RAMP	
2-2-95	PLACED IN USE	2-2-95
		FILMED

**ARKANSAS STATE HIGHWAY COMMISSION**

**PAVEMENT MARKING DETAILS  
ON  
ACCESS CONTROLLED ROADWAYS**


**STANDARD DRAWING PM-2**

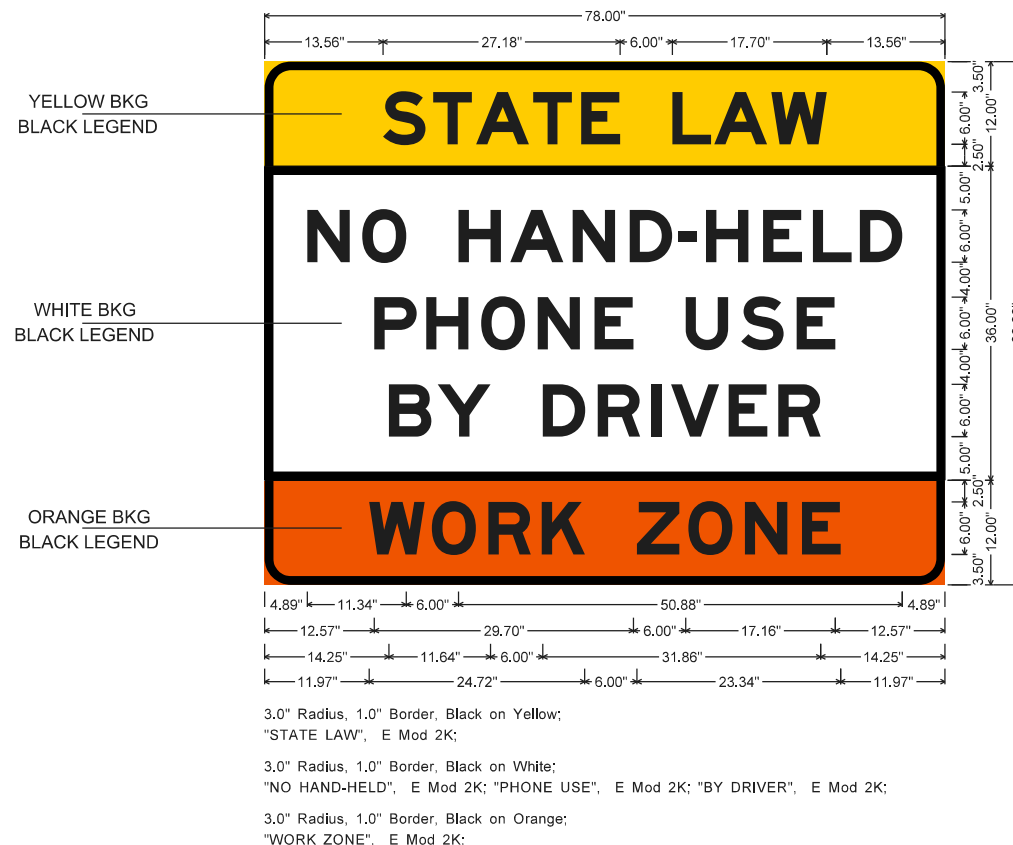
<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>W3-5</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>W3-5a</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R2-6aP</p>  <p>48"x36" *USE 6" C LETTERS **USE 4" D LETTERS</p>
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>W21-5a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>WI-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>WI-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>R2-II</p>  <p>36"x48"</p>
<p>WI-3</p>  <p>STD. 48"x48"</p>	<p>WI-4</p>  <p>STD. 48"x48"</p>	<p>WI-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>WI-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>R56-1</p>  <p>STD. 18"x18"</p>
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>	<p>W20-3</p>  <p>STD. 48"x48"</p>
<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>18" 500 FEET W20-2 24"</p> <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>	<p>WI-4b</p>  <p>STD. 48"x48"</p>	
<p>W8-II</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>	<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>	

GENERAL NOTES:

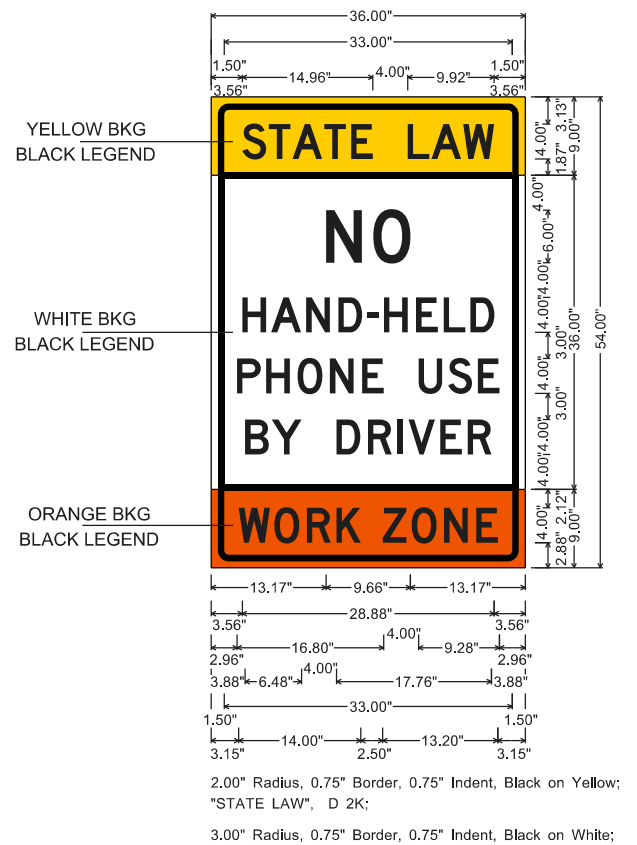
- ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.
- TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.
- EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.
- SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SO. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE.
- SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.
- POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.
- ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE, EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.
- FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
- MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.
- R2-6aP PLAQUES SHALL BE MOUNTED BELOW AN APPLICABLE ADVANCE WARNING SIGN THAT IS LOCATED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE PLAQUE SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE W3-5 "REDUCED SPEED AHEAD" SIGN. R2-II SIGNS SHALL BE INSTALLED AT OR NEAR THE DOWNSTREAM END OF THE WORK ZONE. SEE STANDARD DRAWINGS TC-2, TC-3, AND TC-6 FOR TYPICAL PLACEMENT LOCATIONS.

\*NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.

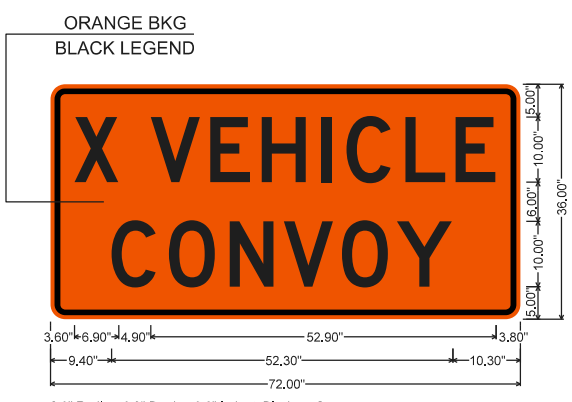
		<p>ARKANSAS STATE HIGHWAY COMMISSION</p>	
<p>STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION</p>			
<p>REPLACED R55-1 WITH R2-6aP, ADDED R2-11 &amp; REVISED NOTE 10</p>	<p>REVISED FOR MASH</p>	<p>DELETED RSP-1 &amp; ADDED W21-5a</p>	<p>REVISED REDUCED SPEED SIGN TO SHOW NEXT SIX MILES</p>
<p>08-14-25</p>	<p>11-07-19</p>	<p>04-13-17</p>	<p>09-02-15</p>
<p>DATE</p>	<p>REV</p>	<p>DATE</p>	<p>REV</p>
<p>08-14-2025</p>	<p>STANDARD DRAWING</p>	<p>TC-1</p>	<p>DESCRIPTION</p>



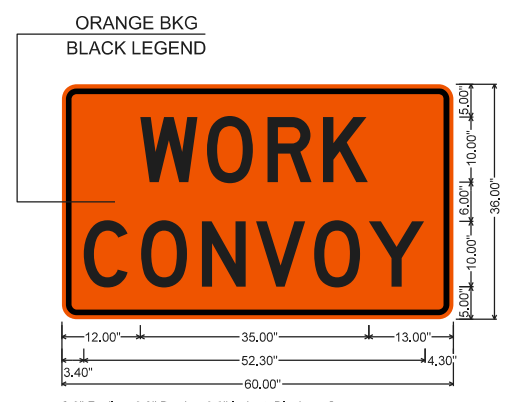
**WZ-4 (INTERSTATE) SIGN**



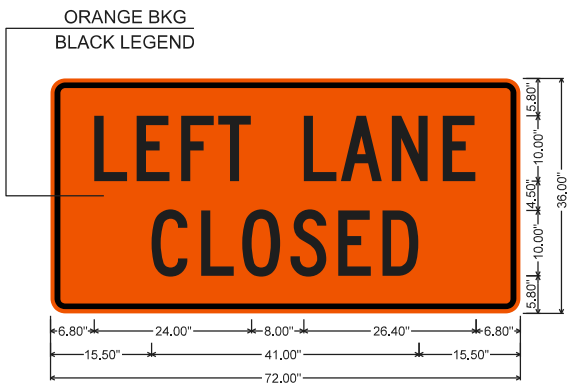
**WZ-4 (NON-INTERSTATE) SIGN**



**WZ-2 SIGN**



**WZ-3 SIGN**

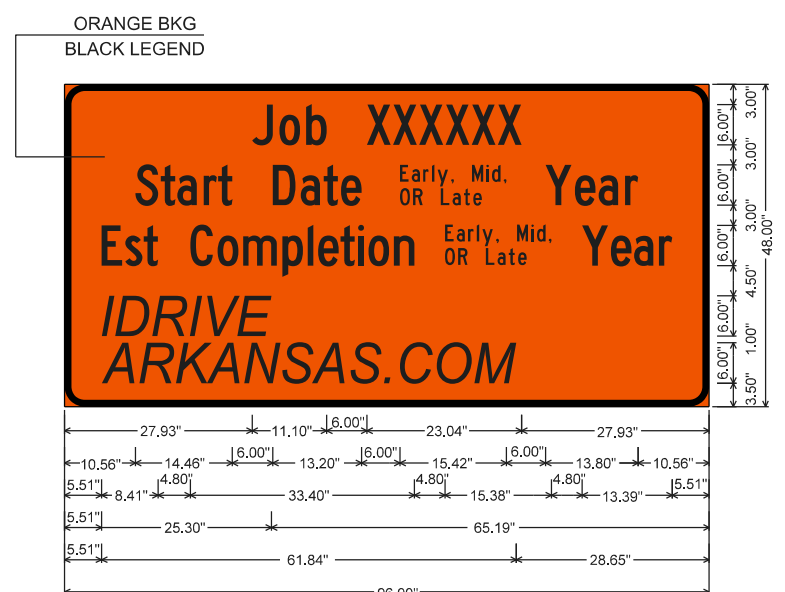


**WZ-W20-5L SIGN**



**WZ-W20-5R SIGN**

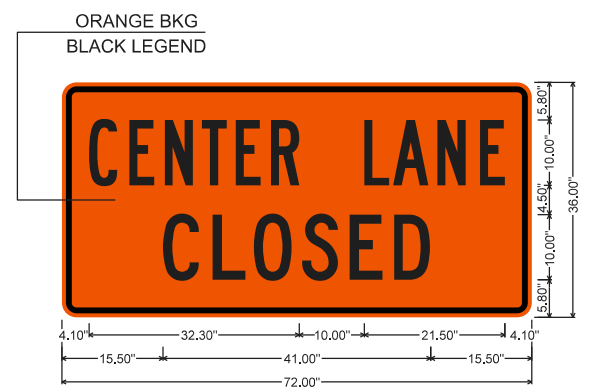
NOTE: REFER TO STANDARD DRAWING TC-1 FOR GENERAL NOTES.



**CONSTRUCTION PROJECT INFORMATION SIGN**



**WZ-1 (INTERSTATE) SIGN**

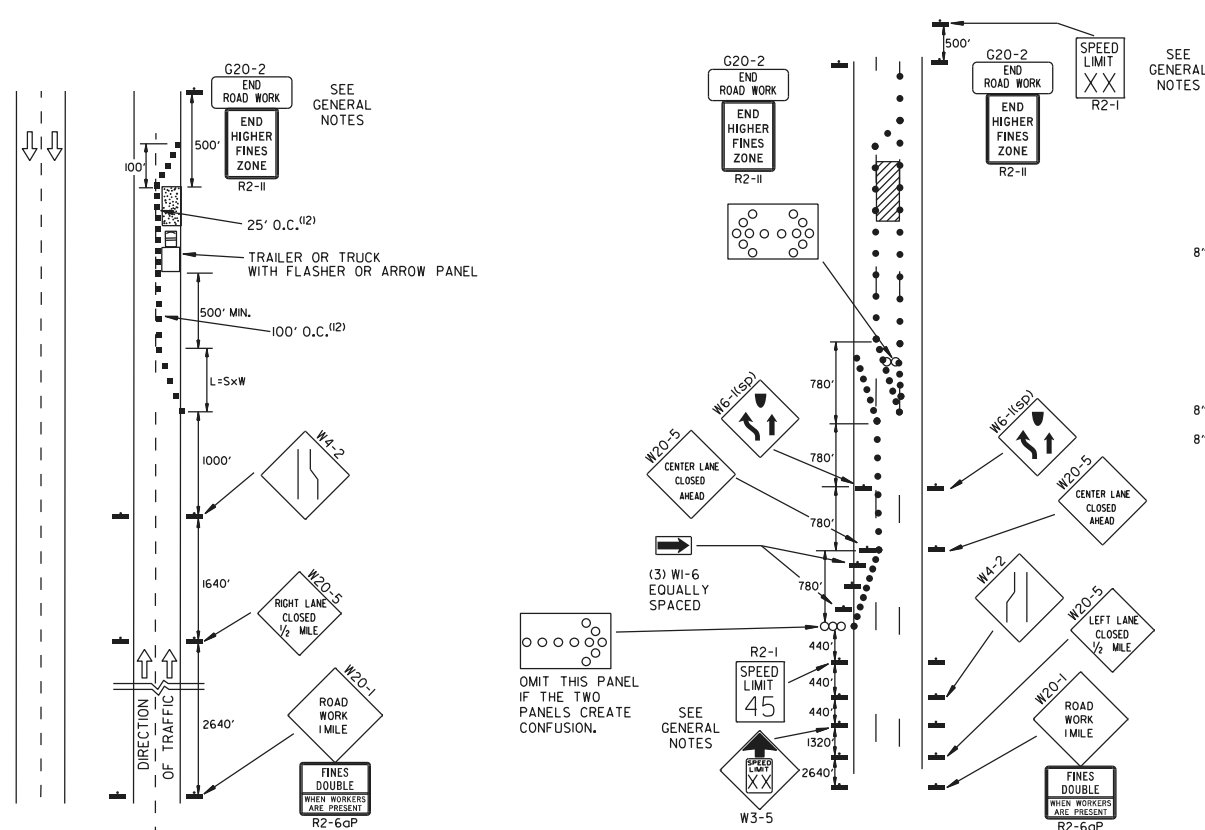


**WZ-W20-5C SIGN**

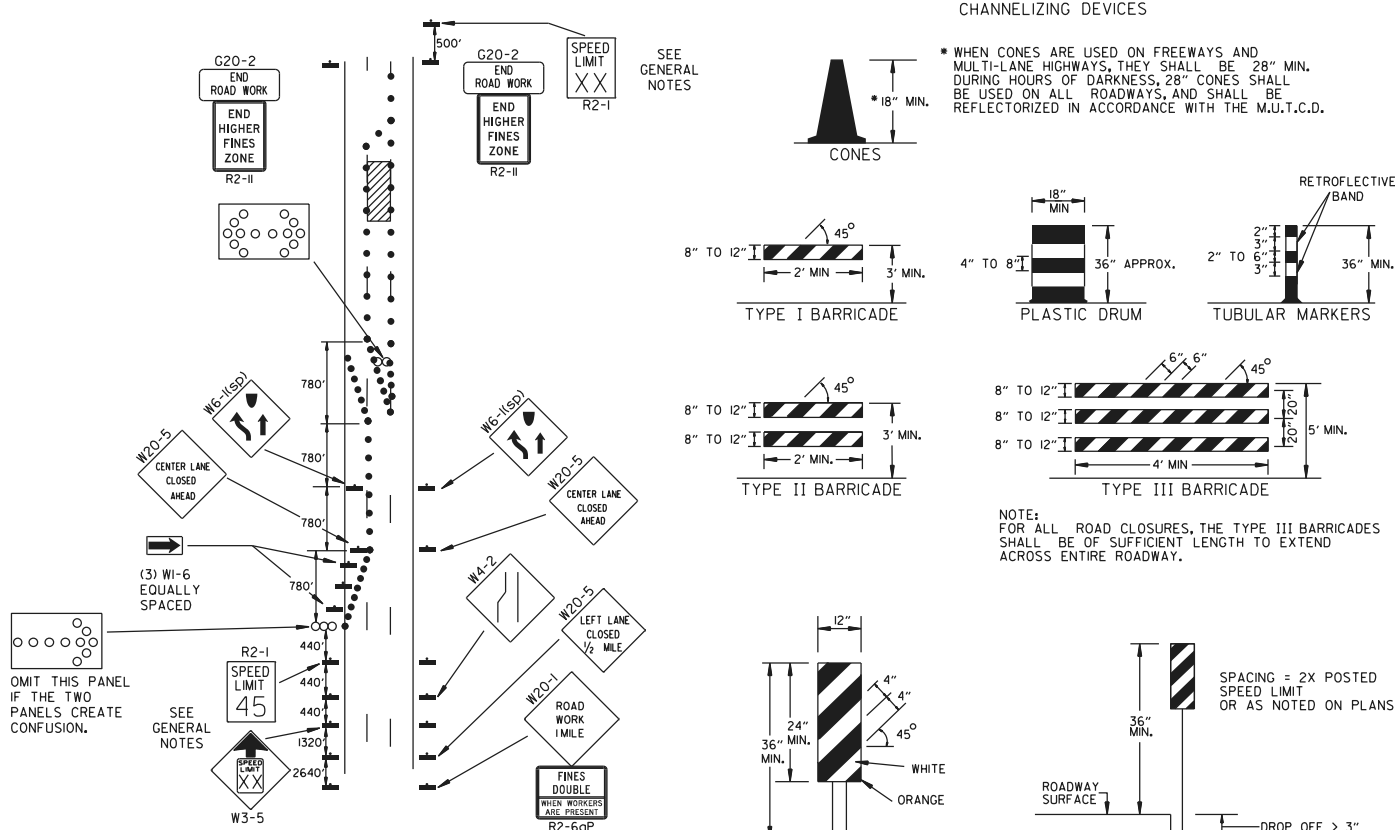
**MOBILE WORK ZONE SIGNS**

		ARKANSAS STATE HIGHWAY COMMISSION		
		<b>STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION</b>		
04-09-26 ISSUED DATE REV	DESCRIPTION	DATE EFFECTIVE 04-09-2026	STANDARD DRAWING	TC-1A





(A) TYPICAL APPLICATION - DAYTIME MAINTENANCE OPERATIONS OF SHORT DURATION ON A 4-LANE DIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.



(B) TYPICAL APPLICATION - 3-LANE ONEWAY ROADWAY WHERE CENTER LANE IS CLOSED.

KEY:

○ ○ ○ ○ ○ ARROW PANEL (IF REQUIRED)

■ CHANNELIZING DEVICE

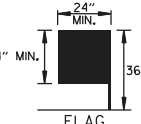
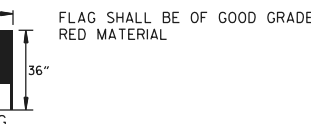
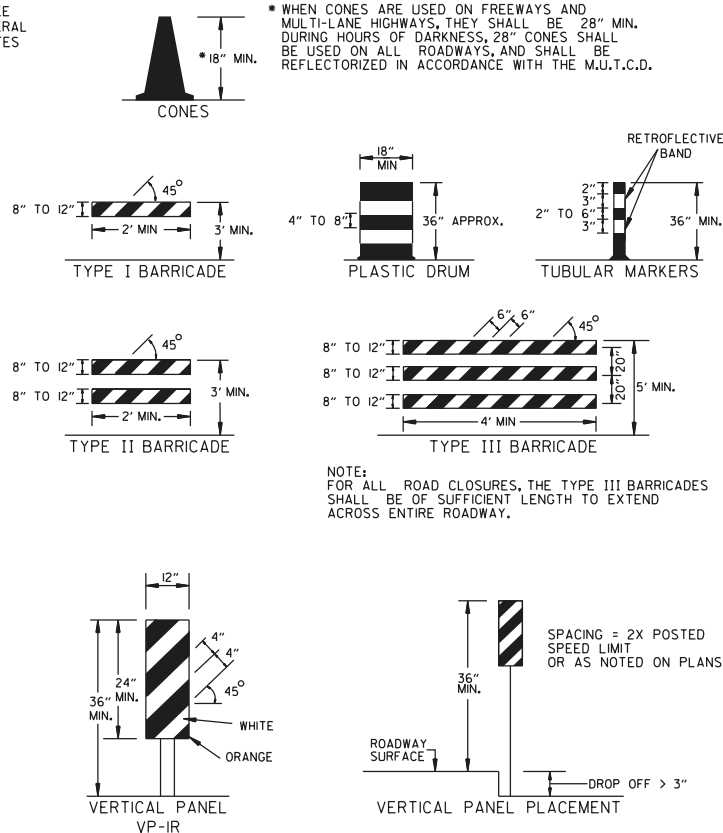
● TRAFFIC DRUM

GENERAL NOTES:

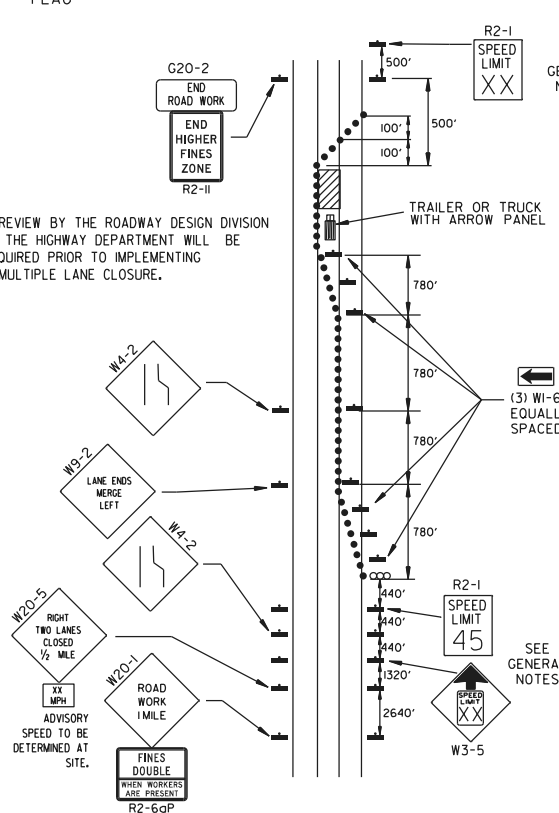
- A SPEED LIMIT REDUCTION MAY BE IMPLEMENTED ONLY WHEN DESIGNATED IN THE PLAN OR WHEN RECOMMENDED BY THE ROADWAY DESIGN DIVISION.
- WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-1(55) SHALL BE OMITTED. ADDITIONAL R2-1(45) SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-1(45)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/4 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
- WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-1(65) SHALL BE OMITTED. ADDITIONAL R2-1(55)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/4 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
- THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT OR AS DIRECTED BY THE ENGINEER.
- WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
- PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
- THE G20-1 SIGN SHOULD BE INSTALLED ON JOBS OF OVER TWO MILES IN LENGTH. WHEN THE LANE CLOSURE IS NOT AT THE BEGINNING OF THE PROJECT, THE G20-1 SIGN SHALL BE ERECTED 125' IN ADVANCE OF THE JOB LIMIT. ADDITIONAL W20-1(1/2 MILE) SIGNS ARE NOT REQUIRED IN ADVANCE OF LANE CLOSURES THAT BEGIN INSIDE THE PROJECT LIMITS. THE DISTANCE DISPLAYED ON THE G20-1 SIGN SHALL BE STATED TO THE NEAREST WHOLE MILE.
- FLAGGERS SHALL USE STOP/SLOW PADDLES FOR CONTROLLING TRAFFIC THROUGH WORK ZONES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
- ALL PLASTIC DRUMS AND CONES SHALL MEET THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).
- TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUOUS MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE. PAYMENT FOR TRAFFIC DRUMS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR VARIOUS TRAILER MOUNTED DEVICES.
- ALL TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL MEET THE REQUIREMENTS OF THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).
- ONLY WHERE SPACE RESTRICTIONS DO NOT ALLOW FOR TRAFFIC DRUMS, TUBULAR MARKERS MAY BE USED AT 50' O.C. IN STABILIZATION ZONES AND AT 10' O.C. DIRECTLY ADJACENT TO CONSTRUCTION OPERATIONS AND AT EXIT TAPERS. TUBULAR MARKERS SHALL BE STABILIZED WITH WEIGHTED BASES IN ACCORDANCE WITH THE M.U.T.C.D.

(C) TYPICAL APPLICATION - CONSTRUCTION OPERATIONS OF INTERMEDIATE TO LONG TERM DURATION ON A 4-LANE DIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.

CHANNELIZING DEVICES



A REVIEW BY THE ROADWAY DESIGN DIVISION OF THE HIGHWAY DEPARTMENT WILL BE REQUIRED PRIOR TO IMPLEMENTING A MULTIPLE LANE CLOSURE.



(D) TYPICAL APPLICATION - CLOSING MULTIPLE LANES OF A MULTILANE HIGHWAY.

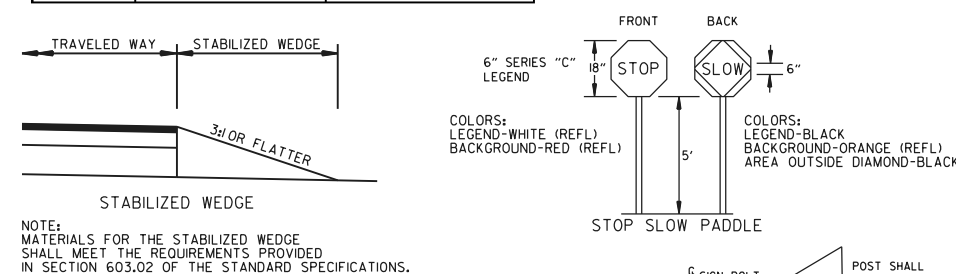
TRAFFIC CONTROL DEVICES

VERTICAL DIFFERENTIAL	LOCATION	TRAFFIC CONTROL	
		≤ 45 MPH	> 45 MPH
≤ 1"	CENTERLINE	W8-11	W8-11
> 1"	CENTERLINE	W8-11 AND CENTERLINE LANE STRIPING	W8-11 AND CENTERLINE LANE STRIPING
> 3"	CENTERLINE	STANDARD LANE CLOSURE <sup>(1)</sup>	STANDARD LANE CLOSURE <sup>(1)</sup>
≤ 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-9 AND TRAFFIC DRUMS <sup>(1)</sup>	W8-9 AND TRAFFIC DRUMS <sup>(1)</sup>
> 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS <sup>(1)</sup>	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS <sup>(1)</sup>
> 6"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS <sup>(1)</sup>	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS <sup>(2)</sup>
> 18"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS <sup>(1)</sup>	A STABILIZED WEDGE, W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS <sup>(2)</sup>
> 24"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	PRECAST CONCRETE BARRIER <sup>(4)</sup> & EDGE LINES	PRECAST CONCRETE BARRIER <sup>(4)</sup> & EDGE LINES

INTERSTATE		
VERTICAL DIFFERENTIAL	LOCATION	TRAFFIC CONTROL
≤ 3"	CENTERLINE	W8-11 AND LANE STRIPING
≤ 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-9, EDGE LINE STRIPING, AND TRAFFIC DRUMS <sup>(2)</sup>
> 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS <sup>(2)</sup>
> 6"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	PRECAST CONCRETE BARRIER & EDGE LINES

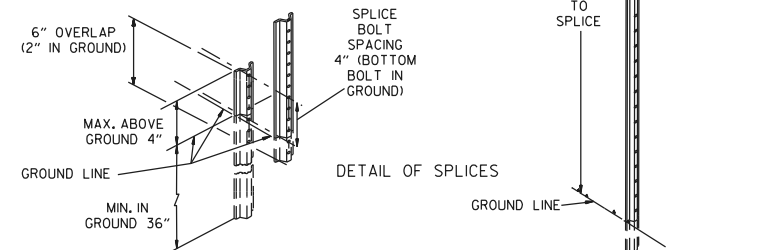
INTERSTATE AND NON-INTERSTATE		
FORESLOPE	HEIGHT	TRAFFIC CONTROL
1:1	> 2 FT	PRECAST CONCRETE BARRIER
2:1	≤ 5 FT	TRAFFIC DRUMS
2:1	> 5 FT	PRECAST CONCRETE BARRIER
Flatter than 2:1	N/A	TRAFFIC DRUMS

- GENERAL NOTES:
- WHEN THE SHOULDER AREA IS USED AS PART OF THE TRAVELED LANE AND THERE IS INSUFFICIENT WIDTH TO PLACE TRAFFIC DRUMS ON THE REMAINING SHOULDER WIDTH, THEN VERTICAL PANELS SHALL BE USED.
  - WHEN THERE IS INSUFFICIENT WIDTH TO PLACE TRAFFIC DRUMS ON THE REMAINING SHOULDER WIDTH, A STABILIZED WEDGE SHALL BE USED. PRECAST CONCRETE BARRIER WALL CAN BE USED IN LIEU OF A STABILIZED WEDGE, W8-17 SIGN, EDGE LINE STRIPING, AND TRAFFIC DRUMS. IF AND WHERE DIRECTED BY THE ENGINEER, A STABILIZED WEDGE, W8-17 SIGN, EDGE LINE STRIPING, AND TRAFFIC DRUMS CAN BE USED IN LIEU OF PRECAST CONCRETE BARRIER WALL. IF AND WHERE DIRECTED BY THE ENGINEER, W21-5, W21-5a, AND/OR W21-5b SIGNS SHALL BE USED WHERE THE ROADWAY IS UNOBSTRUCTED IF AND WHERE DIRECTED BY THE ENGINEER. TIME LIMITATIONS MUST CONFORM TO SECTION 603 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION).



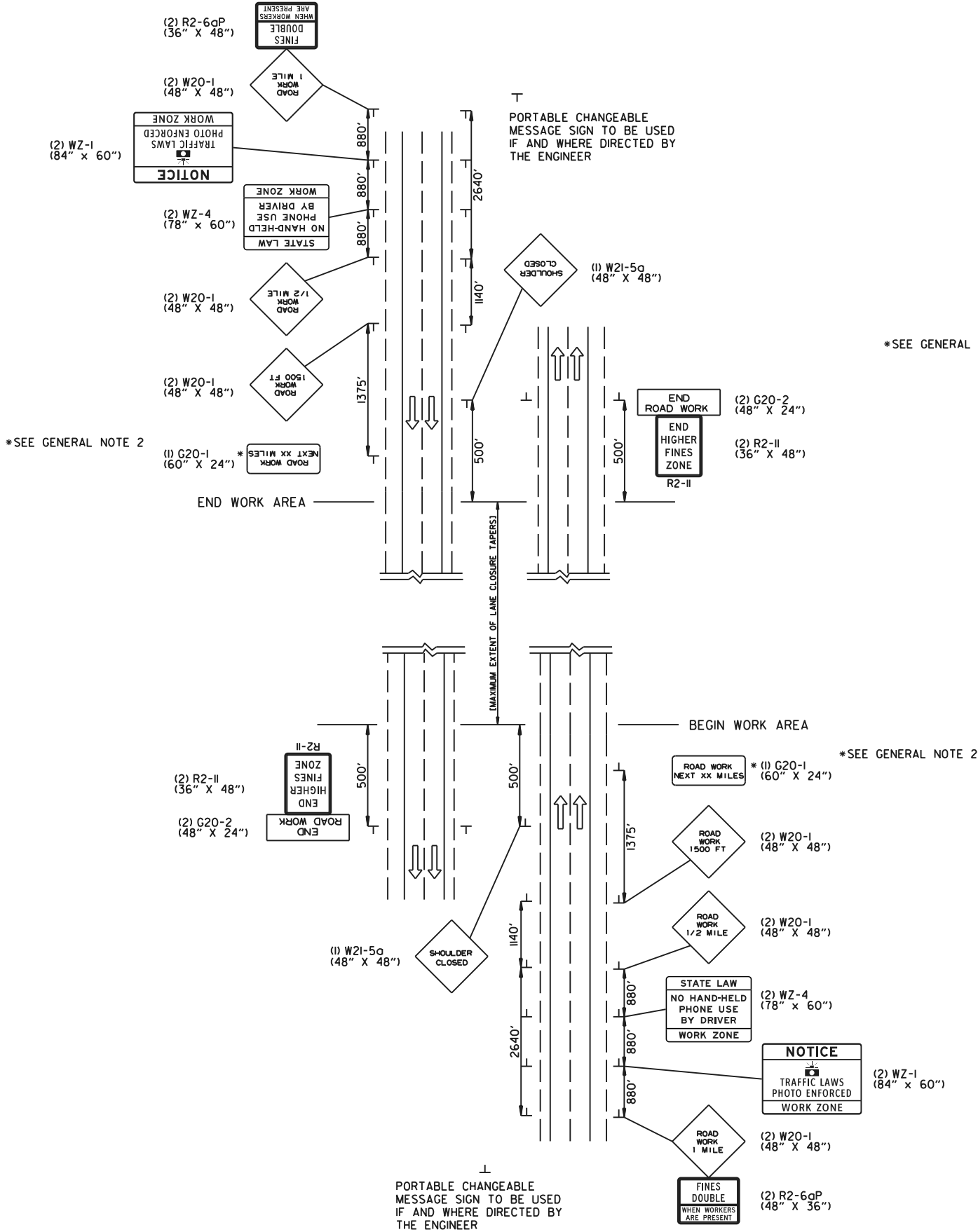
NOTE: MATERIALS FOR THE STABILIZED WEDGE SHALL MEET THE REQUIREMENTS PROVIDED IN SECTION 603.02 OF THE STANDARD SPECIFICATIONS.

NOTES:  
 USE SPLICES ONLY WHEN NECESSARY FOR INSTALLATION. TYPICAL INSTALLATION SHOULD HAVE NO SPLICES (SEE STD. DRAWING NO. SHS-2)  
 NORMAL INSTALLATIONS WILL REQUIRE 1/4" DIA. BOLTS TO MOUNT SIGNS TO POST AND 5/16" DIA. BOLTS TO ASSEMBLE THE VARIOUS POST SUPPORTS. EACH OF THESE BOLTS SHALL BE CARRIAGE BOLTS.  
 SIGN POSTS SHALL BE PAINTED GREEN; SIGNS SHALL NOT BE PAINTED, AND ALL SIGN POSTS SHALL BE PLUMB.



08-14-25	ADDED R2-6aP AND R2-11	REVISOR	AR DOT	ARKANSAS STATE HIGHWAY COMMISSION
05-22-25	REVISOR	ADDED NOTE 12		
08-12-21	REVISOR	ADDED NOTE 10		
05-20-21	REVISOR	ADDED NOTE 10		
02-27-20	REVISOR	ADDED DETAILS		
DATE	REV	DESCRIPTION		
08-14-2025		STANDARD DRAWING		TC-3

NOTE : W20-1 (VARIOUS DISTANCE) ADVANCE SIGNS TO BE REPLACED AS NEEDED BY EQUIVALENT W20-5 SIGNS AS WORKING AREA SHIFTS.



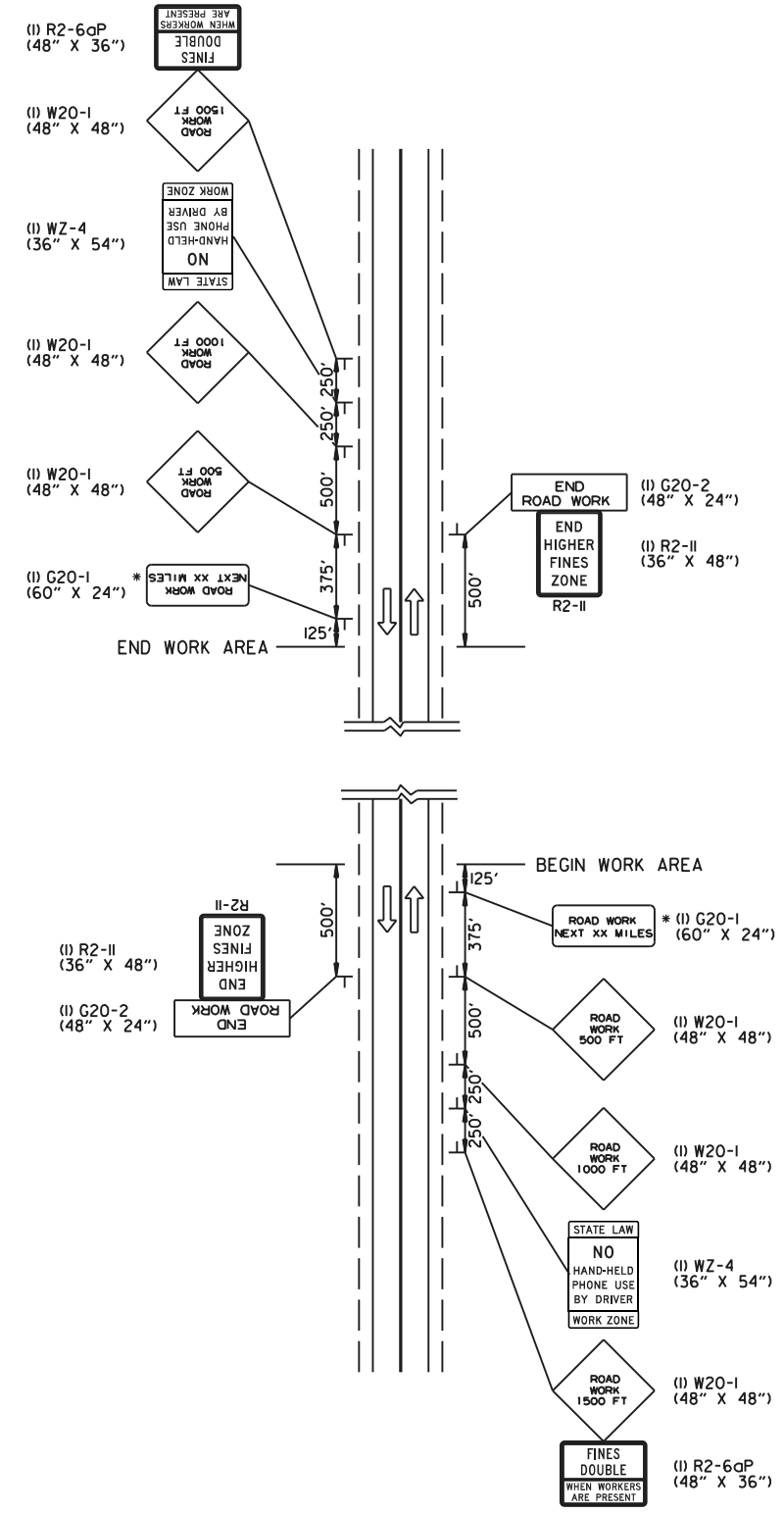
\*SEE GENERAL NOTE 2

\*SEE GENERAL NOTE 2

\*SEE GENERAL NOTE 2

NOTE : W20-1 (VARIOUS DISTANCE) ADVANCE SIGNS TO BE REPLACED AS NEEDED BY EQUIVALENT W20-5 SIGNS AS WORKING AREA SHIFTS.

(A) TYPICAL APPLICATION - ADVANCE WARNING SIGNS AT BEGINNING AND END OF JOB (FULLY CONTROLLED ACCESS FACILITIES) (ALL STAGES)



- GENERAL NOTES:
1. FLAGS MAY BE MOUNTED TO SIGNS AT NIGHT AS NEEDED.
  2. THE G20-1 SIGN SHOULD BE INSTALLED ON JOBS OF OVER TWO MILES IN LENGTH. WHEN THE LANE CLOSURE IS NOT AT THE BEGINNING OF THE PROJECT, THE G20-1 SIGN SHALL BE ERRECTED 125' IN ADVANCE OF THE JOB LIMIT. ADDITIONAL W20-1 (1 MILE OR 1500 FT) SIGNS ARE NOT REQUIRED IN ADVANCE OF LANE CLOSURES THAT BEGIN INSIDE THE PROJECT LIMITS. THE DISTANCE DISPLAYED ON THE G20-1 SIGN SHALL BE STATED TO THE NEAREST WHOLE MILE.
  3. PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THIS DEVICE SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE. PAYMENT FOR TRAFFIC DRUMS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR PORTABLE CHANGEABLE MESSAGE SIGNS.
  4. PORTABLE CHANGEABLE MESSAGE SIGNS SHALL MEET THE REQUIREMENTS OF THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).

(B) TYPICAL APPLICATION - ADVANCE WARNING SIGNS AT BEGINNING AND END OF JOB (NON-FULLY CONTROLLED ACCESS FACILITIES) (ALL STAGES)

CORRECTED WZ-4 SIGN DIMENSIONS			ARKANSAS STATE HIGHWAY COMMISSION		
ADDED WZ-4 AND REVISED GENERAL NOTE 1			<p align="center"><b>STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION</b></p>		
REPLACED "SPEED" WITH "TRAFFIC LAWS" ON WZ-1					
ISSUED					
DATE	REV	DESCRIPTION	DATE EFFECTIVE	STANDARD DRAWING	TC-6
04-09-26			04-09-2026		