



Latitude:35.96029, Longitude:-94.36525

Route:62 Section:01 Log:11.36

Arnold Road ID:72x62x1xA, Arnold Log mile:11.343

District 04, 143 - Washington County

Owner: 1 - State Highway Agency

Inspection Direction: 4 - W to E

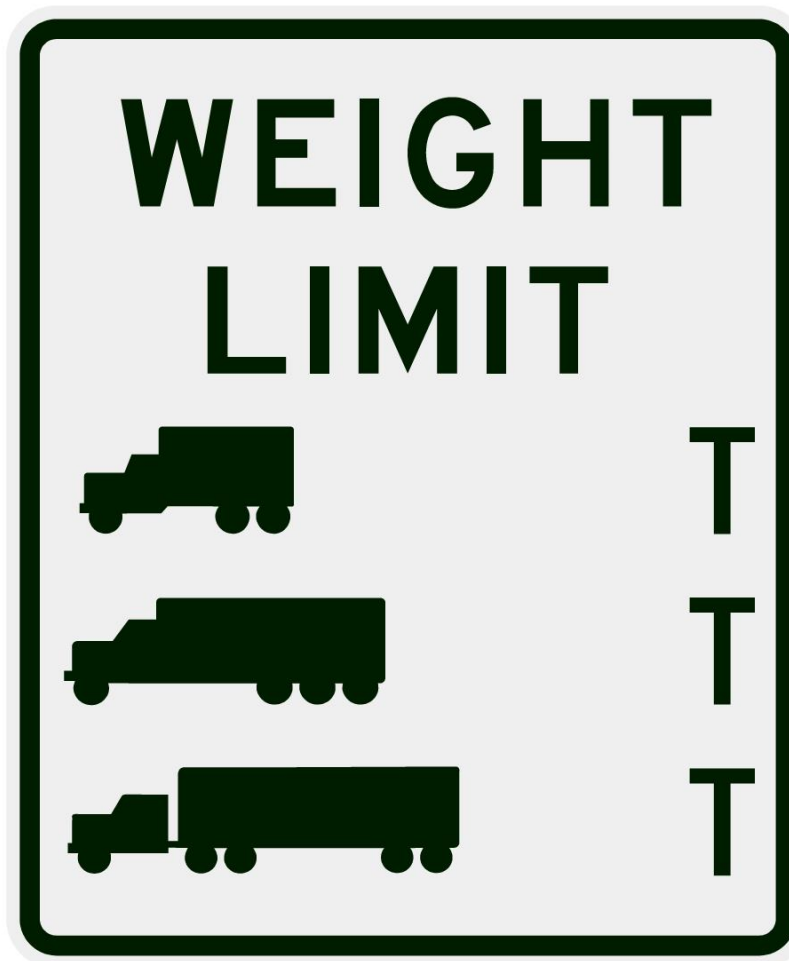
Bridge Posting Information

41 - Structure Open/Posted/Closed: A - Open, no restriction

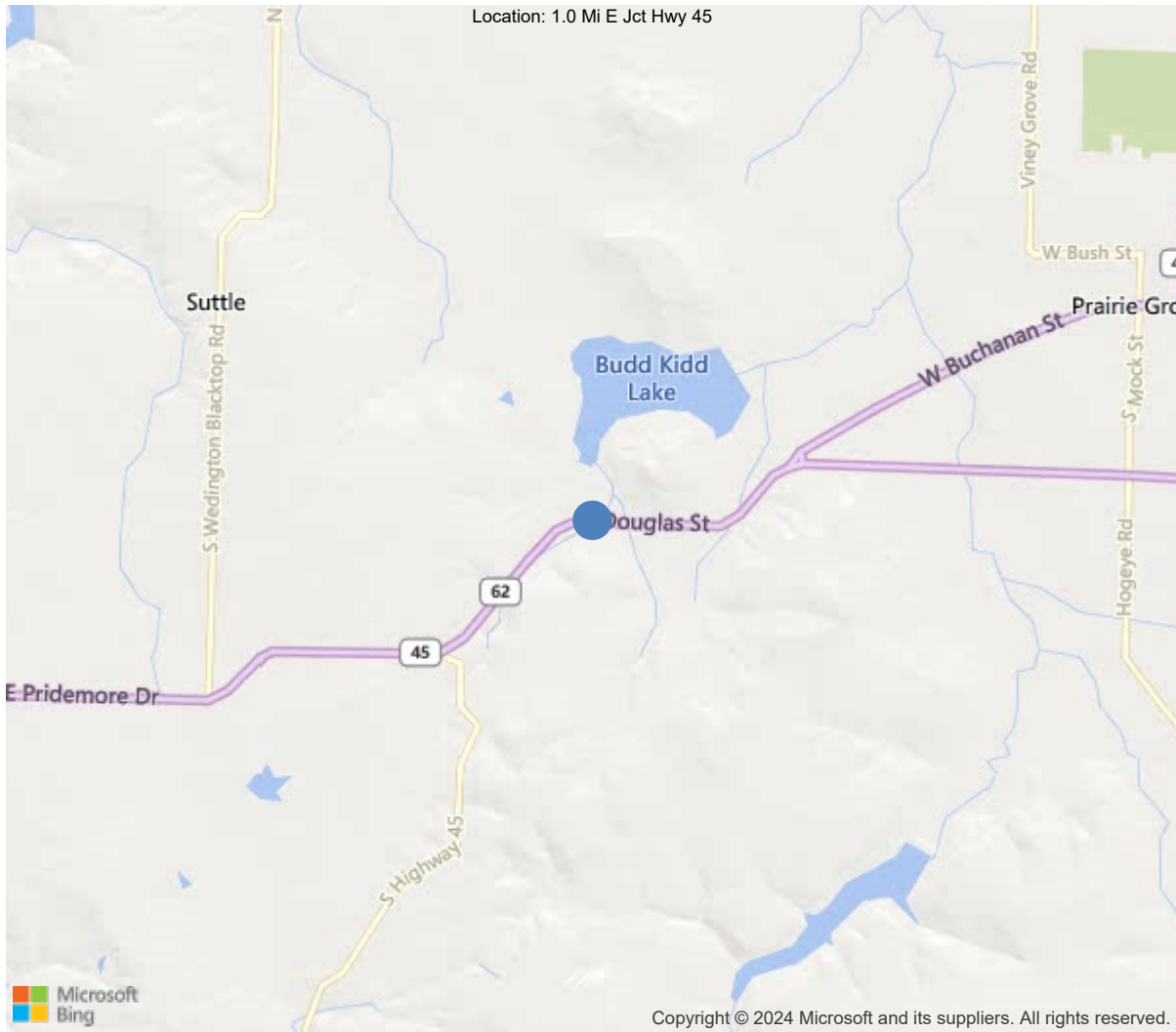
70 - Bridge Posting: 5 - Equal to or above legal loads

Legal Load	Calculated Capacity	Beginning of Bridge Sign Current Value	End of Bridge Sign Current Value
Code 4 (22 Tons)	24		
Code 9 (31 Tons)	31		
Code 5 (40 Tons)	42		

If calculated Capacity is less than the Legal Load Listed, the Bridge Legally Requires Posting Signs to be installed by the Bridge Owner



30"x36" AR



35.96029, -94.36525

IDENTIFICATION	
(1) State Names	5 - Arkansas
(8) Structure Number	X1240
(5) Inventory Route	1
(2) Highway Agency District	04 - District 04
(3) County Code	143 - Washington County
(4) Place Code	0
(6) Features Intersected	Budd Kidd Creek-Wash
(7) Facility Carried	State Highway 62
(9) Location	1.0 Mi E Jct Hwy 45
(11) Mile Point	11.36 mi
(12) Base Highway Network	Yes
(13) LRS Inventory Rte & Subrte	0000062010
(16) Latitude	35.96029
(17) Longitude	-94.36525
(98) Border Bridge State Code	
(99) Border Bridge Structure No.	
STRUCTURE TYPE AND MATERIAL	
(43) Main Structure Type	119
Material	1 - Concrete
Type	19 - Culvert
(44) Approach Structure Type	00
Material	0 - Other
Type	0 - Other
(45) No. of Spans in Main Unit	3
(46) No. of Approach Spans	0
(107) Deck Structure Type	N - Not applicable
(108) Wearing Surface/Protective System	
Type of Wearing Surface	N - Not applicable (applies only to stru
Type of Membrane	N - Not applicable (applies only to stru
Type of Deck Protection	N - Not applicable (applies only to stru
AGE AND SERVICE	
(27) Year Built	2006
(106) Year Reconstructed	0
(42) Type of Service	15
On	1 - Highway
Under	5 - Waterway
(28) Lane	
On	2
Under	0
(29) Average Daily Traffic	7600
(30) Year of ADT	2018
(109) Truck ADT	12 %
(19) Bypass, Detour Length	20 mi
GEOMETRIC DATA	
(48) Length of Maximum Span	18.3 ft
(49) Structure Length	53 ft
(50) Curb or Sidewalk Width	
Left	0 ft
Right	0 ft
(51) Bridge Roadway Width Curb to Curb	0 ft
(52) Deck Width Out to Out	0 ft
(32) Approach Roadway Width (W/Shoulders)	40 ft
(33) Bridge Median	0 - No median
(34) Skew	45 Deg
(35) Structure Flared	0 - No flare
(10) Inventory Route Min Vert Clear	99.99 ft
(47) Inventory Route Total Horiz Clear	99.9 ft
(53) Min Vert Clear Over Bridge Rdwy	99.9 ft
(54) Min Vert Underclear	0 ft
Ref:	
(55) Min Lat Underclear RT	0 ft
Ref:	
(56) Min Lat Underclear LT	0 ft
NAVIGATION DATA	
(38) Navigation Control	0 - No navigation control on w
(111) Pier Protection	
(39) Navigation Vertical Clearance	0 ft
(116) Vert-Lift Bridge Nav Min Vert Clear	0 ft
(40) Navigation Horizontal Clearance	0 ft

CLASSIFICATION	
(112) NBIS Bridge Length	Y
(104) Highway System	0
(26) Functional Class	6 - Rural Minor Arterial
(100) Defense Highway	0 - The inventory route is not
(101) Parallel Structure	N - No parallel structure exists
(102) Direction of Traffic	2 - way traffic
(103) Temporary Structure	
(105) Federal Lands Highways	0 - N/A
(110) Designated National Network	1 - The inventory route is par
(20) Toll	3 - On free road. The structu
(21) Maintain	1 - State Highway Agency
(22) Owner	1 - State Highway Agency
(37) Historical Significance	4 - Historical significance is
CONDITION	
(58) Deck	N
(59) Superstructure	N
(60) Substructure	N
(61) Channel & Channel Protection	6
(62) Culverts	7
LOAD RATING AND POSTING	
(31) Design Load	4 - M 18 / H 20
(63) Operating Rating Method	1
(64) Operating Rating	
Type	1 - Load Factor(LF)
Rating	35
(65) Inventory Rating Method	1 - Load Factor(LF)
(66) Inventory Rating	
Type	
Rating	21
(70) Bridge Posting	5 - Equal to or above legal loads
(41) Structure Open/Posted/Closed	A - Open, no restriction
APPRAISAL	
(67) Structural Evaluation	
(68) Deck Geometry	N
(69) Clearances, Vertical/Horizontal	N
(71) Waterway Adequacy	8
(72) Approach Roadway Alignment	8
(36A) Bridge Railings	N - Not applicable or a safety feat
(36B) Transitions	N - Not applicable or a safety feat
(36C) Approach Guardrail	N - Not applicable or a safety feat
(36D) Approach Guardrail Ends	N - Not applicable or a safety feat
(113) Scour Critical Bridges	8 - Bridge foundations determined t
PROPOSED IMPROVEMENTS	
(75) Type of Work	
(76) Length of Structure Improvement	0 ft
(94) Bridge Improvement Cost	\$ 0
(95) Roadway Improvement Cost	\$ 0
(96) Total Project Cost	\$ 0
(97) Year of Improvement Cost Estimate	
(114) Future ADT	12377
(115) Year of Future ADT	2028

INSPECTIONS *			
(90) Inspection Date	05/14/2024		
(91) Frequency	24		
(92) Critical Feature Inspection	Done	Freq. (Mon)	Date
A: Fracture Critical Detail	No		
B: Underwater Inspection	No		
C: Other Special Inspection	No		
* The inspection date and frequency information in this box contains the current NBI date and frequency information. Please refer to the report header for the date this inspection was conducted.			



Asset #X1240(Routine)

State Highway 62 over Budd Kidd Creek-Wash

Location: 1.0 Mi E Jct Hwy 45

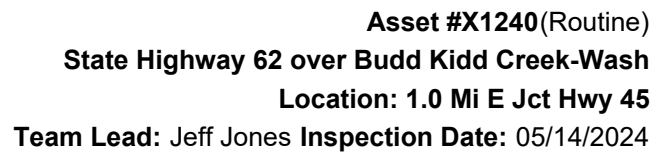
Team Lead: Jeff Jones **Inspection Date:** 05/14/2024

General Observation

05/14/2024 - JCJ & TJL - Routine Inspection conducted this date.

A-64 - Vegetation Removal Requested (Y)

The channel is restricted by trees and vegetation at the outlet end of structure.



ELEMENTS	DESCRIPTION	UNITS	TOTAL	CS1	CS2	CS3	CS4
241	Reinforced Concrete Culvert	LF	363	297	50	16	0
1080	Delamination/Spall/Patched Area	LF	5	0	3	2	0
1120	Efflorescence/Rust Staining	LF	31	0	17	14	0
1130	Cracking (RC and Other)	LF	30	0	30	0	0
<p>(241) Top slab, Right side measures 11" thick. Headwall is 2" tall.</p> <p>There is up to 3' of rock and stream bed material accumulation in Barrels # 1, 2, & 3.</p> <p>There is shallow spalling with no exposed reinforcing steel in the interior walls and headwall at the inlet end of structure from apparent maintenance debris removal activities. 3 LF CS2, 2LF CS3</p> <p>Vertical hairline shrinkage cracks in the walls. 22LF CS2</p> <p>Transverse cracks with efflorescence in the top slab. 17LF CS2, 14LF CS3</p> <p>The repairs to the outlet end of box where dirt was incorporated into the top slab of concrete are still holding during this inspection.</p> <p>Barrel 1, Right side has 5' of horizontal cracking at the wing wall juncture. 4LF CS2</p> <p>Barrel 1, Right, Wall 2 has a 3' tall spall with no exposed reinforcing steel.</p> <p>Barrel 2 has the general above referenced defects during this inspection.</p> <p>Barrel 3, has 2" of minor distortion in top slab where the form-work moved during the construction process.</p> <p>Barrel 3, Left side has 4' of horizontal cracking at the wing wall juncture. 4LF CS2</p>							



Elevation. Right side of structure.



Elevation. Left side of structure.



Approach roadway facing East.



Right side of structure, Up to approximately 7' of the top slab exposed.



05/14/2024

Barrel 3. Typical.



05/14/2024

Barrel 3, Top slab, Area where forms sagged approximately 2" during the construction process.



05/14/2024

Barrel 2, Top slab, Transverse crack with efflorescence and rust stains.



05/14/2024

Barrel 2. Typical.



Barrel 1, Right, Wall 2 has a 3' tall spall with no exposed reinforcing steel.



Barrel 1, Top slab, Grouted repairs appear to be functioning as intended.



Barrel 1. Typical.



Roadway over structure.



Typical cracking with efflorescence.



Barrel 1, Right, Wing Wall: Cracking.



Barrel 3, Right, Wing Wall: Spalling.



Head Wall, Right: Minor spalling.



Intermediate Wall: Spalling.



Barrel 1, Right, Wing Wall: Spalling.

Maintenance Needs

Date Reported: 05/09/2018

Priority: D- Routine

Type of Work: Miscellaneous

Status: Monitor

Component: Culverts

Deficiency Description

There is up to 3' of gravel accumulation in Barrels # 1, 2, & 3.

Remarks



Barrel 3. Up to approximately 3' of gravel accumulation.



Barrel 2. Gravel accumulation.



Barrel 1. Gravel accumulation.



Barrel 3 Debris accumulation.



07/29/2020

Barrel 3 Debris accumulation.



07/29/2020

Barrel 2 Debris accumulation.

Maintenance Needs

Date Reported: 07/30/2020

Priority: D- Routine

Type of Work: Channel Work/Drift Removal

Status: Monitor

Component: Channel

Deficiency Description

Channel

Inlet and outlet end of all barrels have large drift accumulation/small trees restricting channel flow.

Remarks

05/14/2024 - JCJ & TJL - There is no significant drift accumulation visible at the inlet end of structure with a large gravel deposit during this inspection. The channel is restricted by trees and vegetation at the outlet end of structure.



Right side of structure. Gravel accumulation.



Vegetation and debris restricting the outlet end of channel.



Outlet end small trees restrict the channel flow.



Inlet end.



Barrel 2 Large trees restrict channel flow.



Routine Maintenance

Check Box Maintenance Items

Type of Maintenance	Is recommended?
A-54 - Sealable Deck Cracks	No
A-55 - Deck Washing Needed	No
A-56 - Joint Cleaning/Flushing Needed	No
A-57 - Beam End and Bearing Paint Needed	No
A-58 - Cap Cleaning/Flushing Needed	No
A-59 - Joint Repair Needed	No
A-60 - Full Beam Painting Needed	No
A-61 - Polymer Overlay Advised	No
A-62 - Hydro and LMC Advised	No
A-63 - Missing/Incorrect Log Mile Signage	No
A-64 - Vegetation Removal Requested	Yes

A-54 - Sealable Deck Cracks (No)

A-55 - Deck Washing Needed (No)

A-56 - Joint Cleaning/Flushing Needed (No)



Asset #X1240(Routine)
State Highway 62 over Budd Kidd Creek-Wash
Location: 1.0 Mi E Jct Hwy 45
Team Lead: Jeff Jones Inspection Date: 05/14/2024

A-57 - Girder End and Bearing Painting Needed (No)

A-58 - Cap Cleaning/Flushing Needed (No)

A-59 - Joint Repair Needed (No)

A-60 - Full Girder Painting Needed (No)

A-61 - Polymer Overlay Advised (No)

A-62 - Hydro and LMC Advised (No)

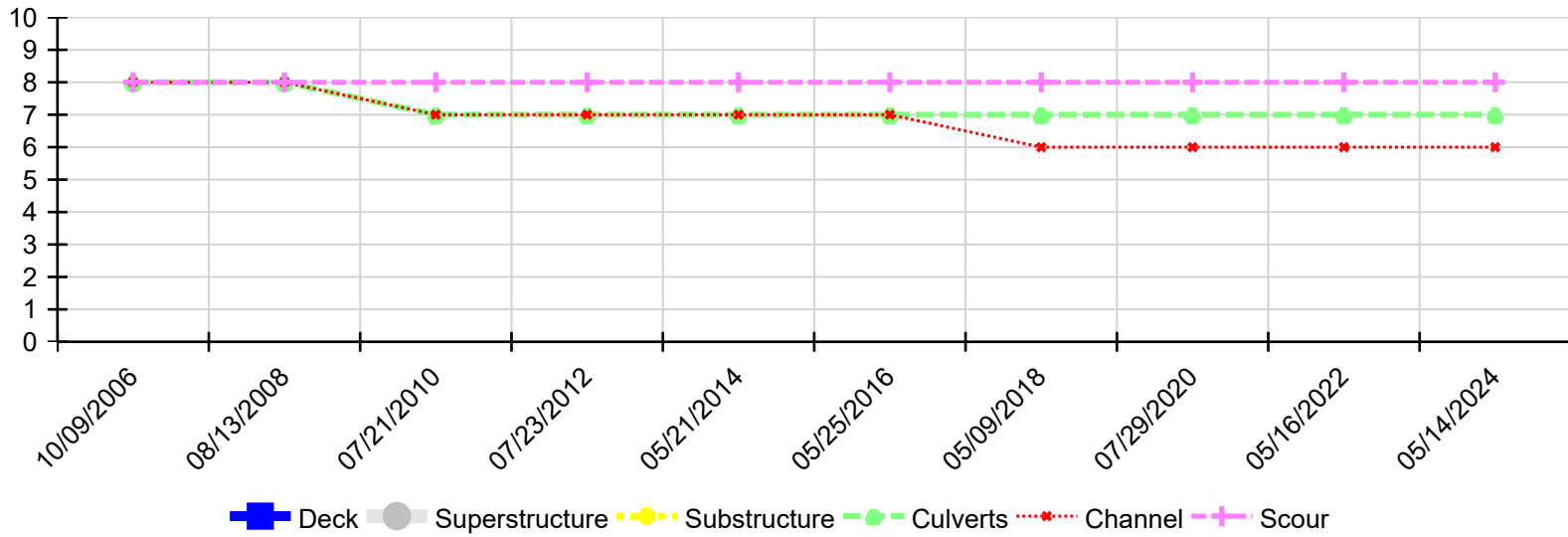
A-63 - Missing/Incorrect Log Mile Signage (No)

A-64 - Vegetation Removal Requested (Yes)

The channel is restricted by trees and vegetation at the outlet end of structure.



Condition History



Inspection Date	Deck	Superstructure	Substructure	Culverts	Channel	Scour
05/14/2024	N	N	N	7	6	8
05/16/2022	N	N	N	7	6	8
07/29/2020	N	N	N	7	6	8
05/09/2018	N	N	N	7	6	8
05/25/2016	N	N	N	7	7	8
05/21/2014	N	N	N	7	7	8
07/23/2012	N	N	N	7	7	8
07/21/2010	N	N	N	7	7	8
08/13/2008	N	N	N	8	8	8
10/09/2006	N	N	N	8	8	8