



Latitude:33.05021, Longitude:-93.53849

Route:160 Section:02 Log:7.79

Arnold Road ID:37x160x2xA, Arnold Log mile:7.764

District 03, 73 - Lafayette County

Owner: 1 - State Highway Agency

Inspection Direction: 4 - W to E

Bridge Posting Information

41 - Structure Open/Posted/Closed: A - Open, no restriction

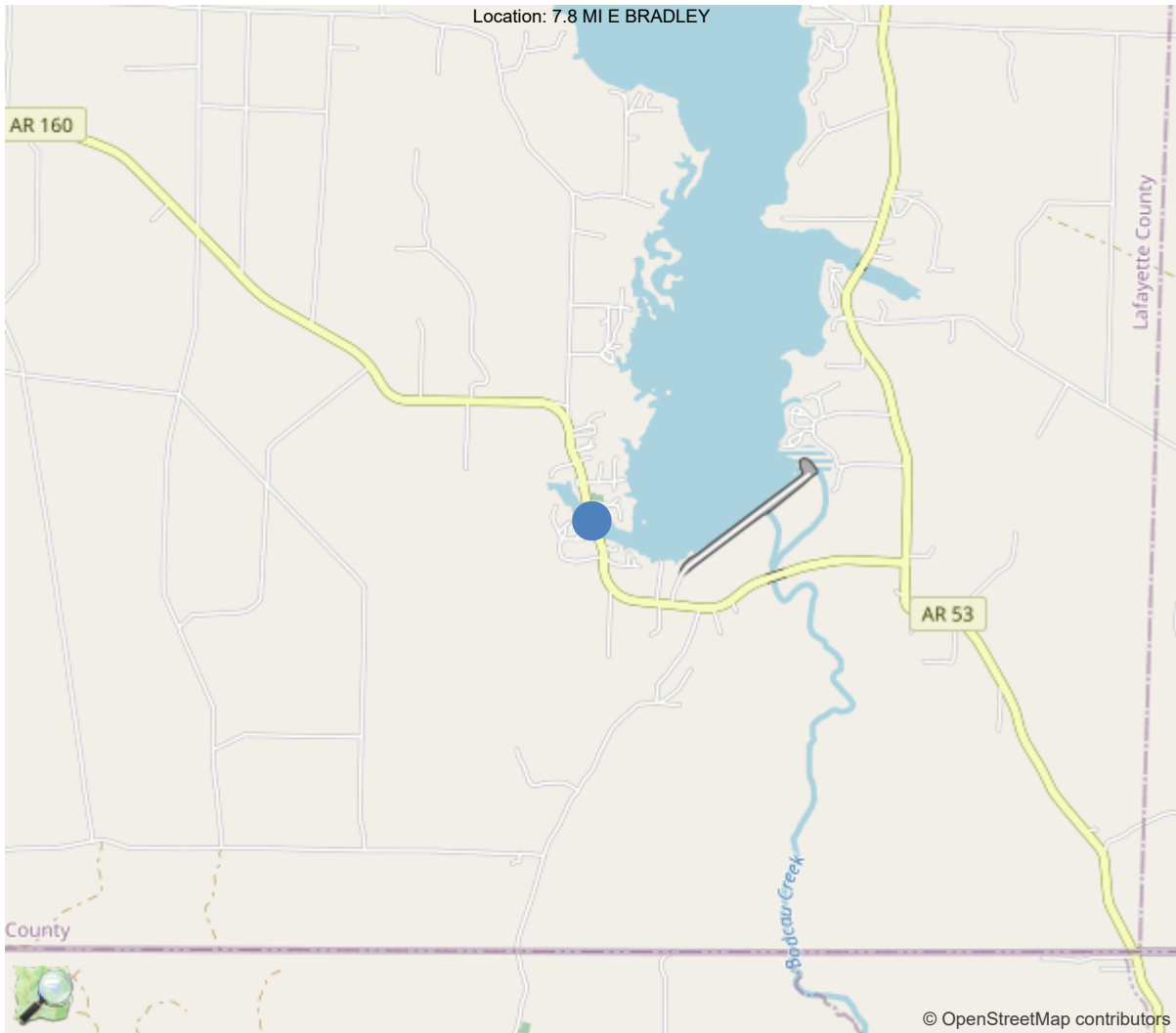
70 - Bridge Posting: 5 - Equal to or above legal loads

Legal Load	Calculated Capacity	Beginning of Bridge Sign Current Value	End of Bridge Sign Current Value
Code 4 (22 Tons)	40		
Code 9 (31 Tons)	49		
Code 5 (40 Tons)	60		

If calculated Capacity is less than the Legal Load Listed, the Bridge Legally Requires Posting Signs to be installed by the Bridge Owner



30"x36" AR



33.05021, -93.53849



Asset #06007(Routine)

SH 160 - 02 - 7.79 over POLK CR. & LAKE ERLING

Location: 7.8 MI E BRADLEY

Team Lead: Jared Kegley Inspection Date: 09/05/2024

IDENTIFICATION	
(1) State Names	5 - Arkansas
(8) Structure Number	06007
(5) Inventory Route	1
(2) Highway Agency District	03 - District 03
(3) County Code	73 - Lafayette County
(4) Place Code	0
(6) Features Intersected	POLK CR. & LAKE ERLING
(7) Facility Carried	SH 160 - 02 - 7.79
(9) Location	7.8 MI E BRADLEY
(11) Mile Point	7.79 mi
(12) Base Highway Network	No
(13) LRS Inventory Rte & Subrte	0000000000
(16) Latitude	33.050209
(17) Longitude	-93.53849
(98) Border Bridge State Code	
(99) Border Bridge Structure No.	
STRUCTURE TYPE AND MATERIAL	
(43) Main Structure Type	11
Material	1 - Concrete
Type	1 - Slab
(44) Approach Structure Type	00
Material	0 - Other
Type	0 - Other
(45) No. of Spans in Main Unit	3
(46) No. of Approach Spans	0
(107) Deck Structure Type	1 - Concrete Cast-in-Place
(108) Wearing Surface/Protective System	
Type of Wearing Surface	1 - Monolithic Concrete (concurrently pl
Type of Membrane	0 - None
Type of Deck Protection	0 - None
AGE AND SERVICE	
(27) Year Built	1983
(106) Year Reconstructed	0
(42) Type of Service	15
On	1 - Highway
Under	5 - Waterway
(28) Lane	
On	2
Under	0
(29) Average Daily Traffic	1300
(30) Year of ADT	2018
(109) Truck ADT	1 %
(19) Bypass, Detour Length	25 mi
GEOMETRIC DATA	
(48) Length of Maximum Span	30 ft
(49) Structure Length	90 ft
(50) Curb or Sidewalk Width	
Left	0 ft
Right	0 ft
(51) Bridge Roadway Width Curb to Curb	30 ft
(52) Deck Width Out to Out	32.8 ft
(32) Approach Roadway Width (W/Shoulders)	38.1 ft
(33) Bridge Median	0 - No median
(34) Skew	0 Deg
(35) Structure Flared	0 - No flare
(10) Inventory Route Min Vert Clear	99.99 ft
(47) Inventory Route Total Horiz Clear	30 ft
(53) Min Vert Clear Over Bridge Rdwy	99.99 ft
(54) Min Vert Underclear	0 ft
Ref:	
(55) Min Lat Underclear RT	99.9 ft
Ref:	
(56) Min Lat Underclear LT	0 ft
NAVIGATION DATA	
(38) Navigation Control	0 - No navigation control on w
(111) Pier Protection	1 - Navigation protection not
(39) Navigation Vertical Clearance	0 ft
(116) Vert-Lift Bridge Nav Min Vert Clear	0 ft
(40) Navigation Horizontal Clearance	0 ft

CLASSIFICATION	
(112) NBIS Bridge Length	Y
(104) Highway System	0
(26) Functional Class	7 - Rural Major Collector
(100) Defense Highway	0 - The inventory route is not
(101) Parallel Structure	N - No parallel structure exists
(102) Direction of Traffic	2 - way traffic
(103) Temporary Structure	
(105) Federal Lands Highways	0 - N/A
(110) Designated National Network	0 - The inventory route is not
(20) Toll	3 - On free road. The structure
(21) Maintain	1 - State Highway Agency
(22) Owner	1 - State Highway Agency
(37) Historical Significance	5 - Bridge is not eligible for
CONDITION	
(58) Deck	6
(59) Superstructure	6
(60) Substructure	7
(61) Channel & Channel Protection	8
(62) Culverts	N
LOAD RATING AND POSTING	
(31) Design Load	4 - M 18 / H 20
(63) Operating Rating Method	1
(64) Operating Rating	
Type	1 - Load Factor(LF)
Rating	60
(65) Inventory Rating Method	1 - Load Factor(LF)
(66) Inventory Rating	
Type	
Rating	36
(70) Bridge Posting	5 - Equal to or above legal loads
(41) Structure Open/Posted/Closed	A - Open, no restriction
APPRAISAL	
(67) Structural Evaluation	
(68) Deck Geometry	5
(69) Clearances, Vertical/Horizontal	N
(71) Waterway Adequacy	7
(72) Approach Roadway Alignment	7
(36A) Bridge Railings	1 - Inspected feature meets current
(36B) Transitions	1 - Inspected feature meets current
(36C) Approach Guardrail	1 - Inspected feature meets current
(36D) Approach Guardrail Ends	1 - Inspected feature meets current
(113) Scour Critical Bridges	5 - Bridge foundations determined t
PROPOSED IMPROVEMENTS	
(75) Type of Work	
(76) Length of Structure Improvement	0 ft
(94) Bridge Improvement Cost	\$ 0
(95) Roadway Improvement Cost	\$ 0
(96) Total Project Cost	\$ 0
(97) Year of Improvement Cost Estimate	
(114) Future ADT	1811
(115) Year of Future ADT	2028

INSPECTIONS *			
(90) Inspection Date	09/05/2024		
(91) Frequency	24		
(92) Critical Feature Inspection	Done	Freq. (Mon)	Date
A: Fracture Critical Detail	No		
B: Underwater Inspection	No		
C: Other Special Inspection	No		
* The inspection date and frequency information in this box contains the current NBI date and frequency information. Please refer to the report header for the date this inspection was conducted.			



58 - Deck (6 - SATISFACTORY CONDITION - structural elements show some minor deterioration.)

Deck was dropped to a 6 this inspection due to cracking on the top side of the deck and cracking with efflorescence on the sides of the slab in various locations.

59 - Superstructure (6 - SATISFACTORY CONDITION - structural elements show some minor deterioration.)

Superstructure was dropped to a 6 this inspection due to cracking on the top side of the deck and cracking with efflorescence on the sides of the slab in various locations.

60 - Substructure (7 - GOOD CONDITION - some minor problems.)

Substructure is rated a 7 this inspection due minor deficiencies in various locations.

61 - Channel/Channel Protection (8 - Banks are protected or well vegetated. River control devices such as spur dikes and embankment protection are not required or are in a stable condition.)

The channel is rated a 8 this inspection due to the upstream and downstream channel being well vegetated.

A-54 - Sealable Deck Cracks (Y)

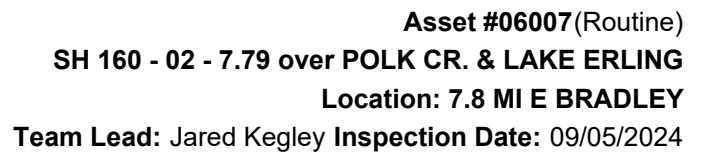
Numerous cracks in various locations of all spans.

A-64 - Vegetation Removal Requested (Y)

There is heavy vegetation on the left and right side at the beginning and end of the bridge.

A-B.C.11 - B.C.11 Scour Condition Rating (New NBIS) (8 - Insignificant scour.)

The scour is rated a 8 this inspection due to there being insignificant scour.

[illegible]



Elevation.



Typical photo of the under surface.



Typical photo of the topside of the deck.



Typical photo of the superstructure.



Typical photo of bent 1 abutment.



Typical photo of the substructure.



Typical photo of bent 4 abutment.



Upstream Channel.



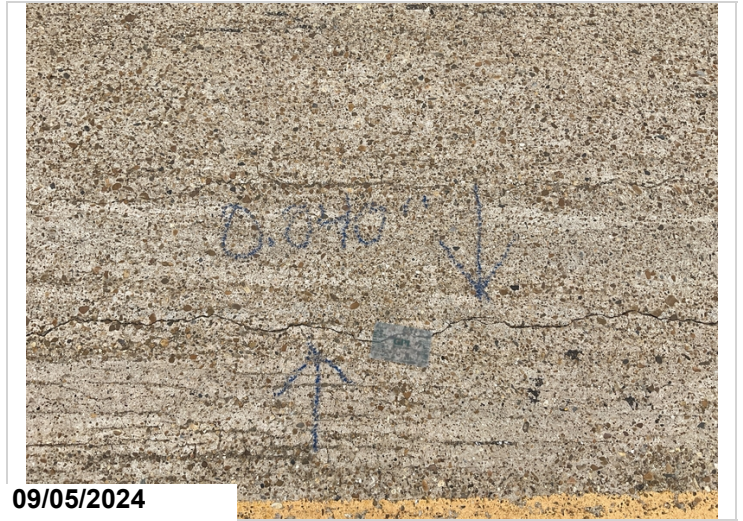
Downstream Channel.



Inventory.



Span 1, centerline, 15' of large cracking.



Numerous cracks in various locations of all spans.



heavy vegetation on the left side at the end of the bridge.



heavy vegetation on the right side at the end of the bridge.



heavy vegetation on the right side at the beginning of the bridge.



Vegetation on the left side at the beginning of the bridge is growing above the bridge.



Bridge railing.



Bridge railing transition.



Bridge joints.



bent 3, right side, 1' of large delam, CS3



09/05/2024

Bent 3, Right side, 2' of cracking with efflorescence, CS2.



09/05/2024

Bent 2, right side, 1' of cracking with efflorescence, CS2.



09/05/2024

Span 1, Left side, 1' of exposed rebar, CS2.



09/05/2024

Span 1, centerline, 15' of large cracking, CS3.



Numerous cracks in various locations of all spans, CS2.



All joints have leakage and debris impaction.



The beginning of the bridge on the left side has minor spalling. Note: The metal plate is loose on the bottom of the parapet wall.

Routine Maintenance

Check Box Maintenance Items

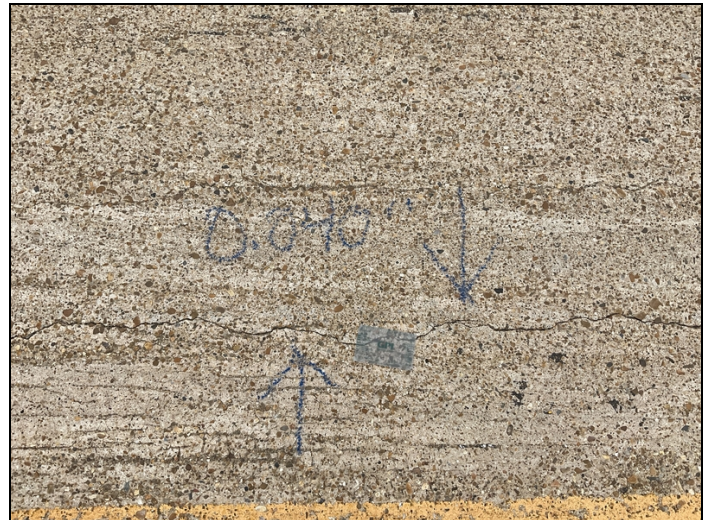
Type of Maintenance	Is recommended?
A-54 - Sealable Deck Cracks	Yes
A-55 - Deck Washing Needed	No
A-56 - Joint Cleaning/Flushing Needed	No
A-57 - Beam End and Bearing Paint Needed	No
A-58 - Cap Cleaning/Flushing Needed	No
A-59 - Joint Repair Needed	No
A-60 - Full Beam Painting Needed	No
A-61 - Polymer Overlay Advised	No
A-62 - Hydro and LMC Advised	No
A-63 - Missing/Incorrect Log Mile Signage	No
A-64 - Vegetation Removal Requested	Yes

A-54 - Sealable Deck Cracks (Yes)

Numerous cracks in various locations of all spans.



Span 1, centerline, 15' of large cracking.



Numerous cracks in various locations of all spans.



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Team Lead: Jared Kegley Inspection Date: 09/05/2024

A-55 - Deck Washing Needed (No)

A-56 - Joint Cleaning/Flushing Needed (No)

A-57 - Girder End and Bearing Painting Needed (No)

A-58 - Cap Cleaning/Flushing Needed (No)

A-59 - Joint Repair Needed (No)

A-60 - Full Girder Painting Needed (No)

A-61 - Polymer Overlay Advised (No)

A-62 - Hydro and LMC Advised (No)

A-63 - Missing/Incorrect Log Mile Signage (No)

A-64 - Vegetation Removal Requested (Yes)

There is heavy vegetation on the left and right side at the beginning and end of the bridge.



heavy vegetation on the left side at the end of the bridge.



heavy vegetation on the right side at the end of the bridge.



heavy vegetation on the right side at the beginning of the bridge.

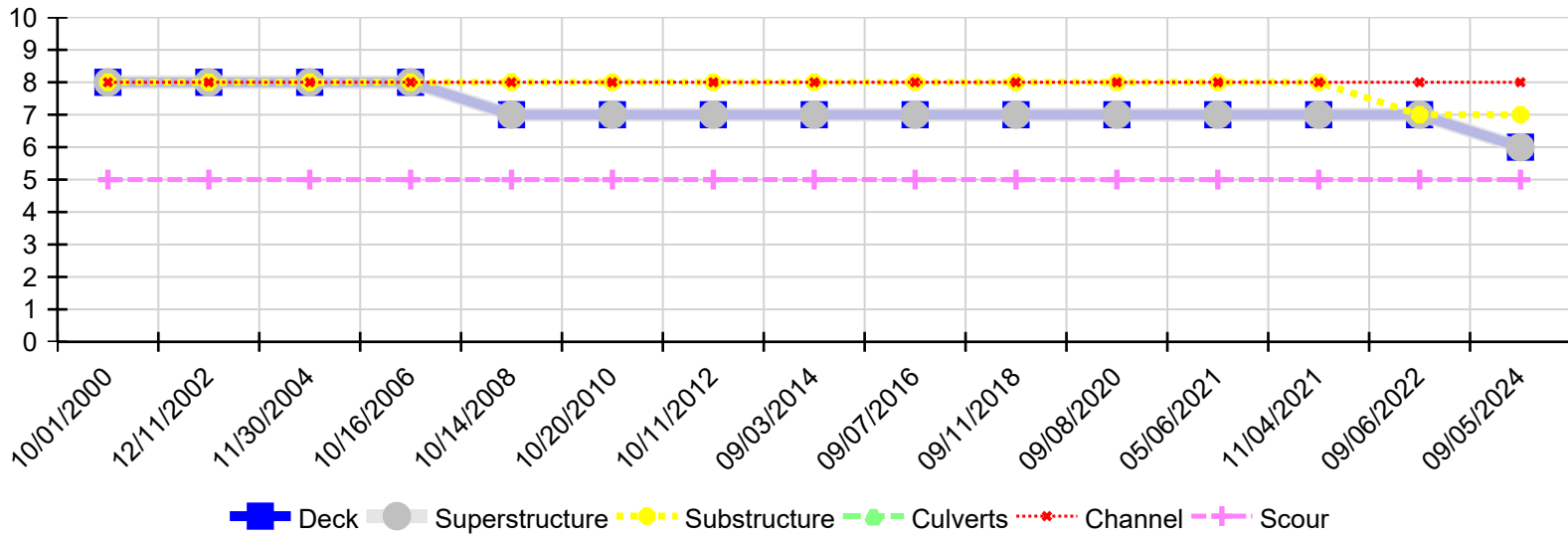


Vegetation on the left side at the beginning of the bridge is growing above the bridge.



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Condition History



Inspection Date	Deck	Superstructure	Substructure	Culverts	Channel	Scour
09/05/2024	6	6	7	N	8	5
09/06/2022	7	7	7	N	8	5
11/04/2021	7	7	8	N	8	5
05/06/2021	7	7	8	N	8	5
09/08/2020	7	7	8	N	8	5
09/11/2018	7	7	8	N	8	5
09/07/2016	7	7	8	N	8	5
09/03/2014	7	7	8	N	8	5
10/11/2012	7	7	8	N	8	5
10/20/2010	7	7	8	N	8	5
10/14/2008	7	7	8	N	8	5
10/16/2006	8	8	8	N	8	5
11/30/2004	8	8	8	N	8	5
12/11/2002	8	8	8	N	8	5
10/01/2000	8	8	8	N	8	5