

252

INDEX OF SHEETS

NOTE: FOR INDEX OF SHEETS AND SPECIAL PROVISIONS
REFER TO SHEET NO. 2

A FULLY CONTROLLED ACCESS FACILITY

STATE OF ARKANSAS

STATE HIGHWAY COMMISSION

PLAN AND PROFILE OF PROPOSED

STATE HIGHWAY HWY. 332 — HWY. 19

GRADING, STRUCTURES AND SURFACING

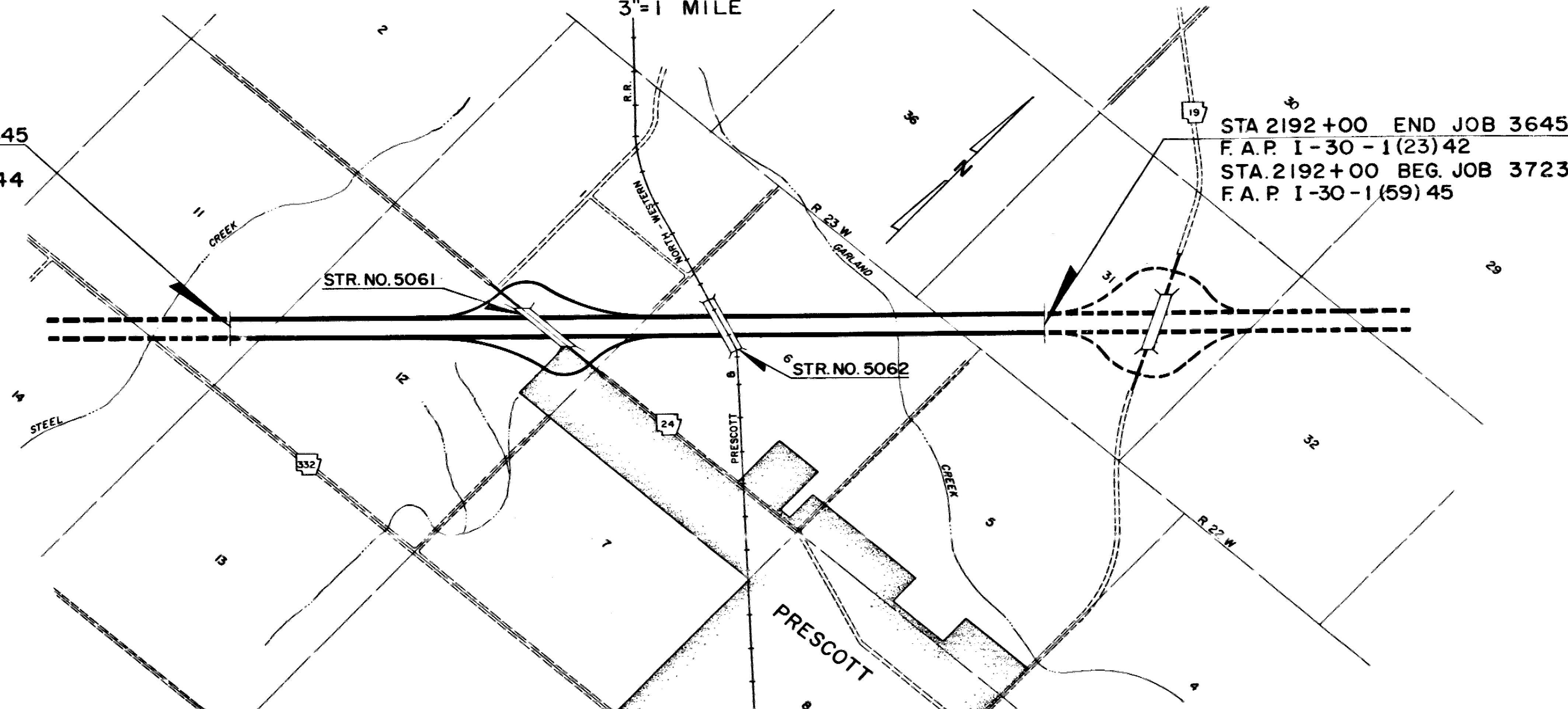
NEVADA
COUNTY

ROUTE 30 SECTION 1

FEDERAL AID PROJECT I-30-1(23)42 JOB 3645

LAYOUT SCALE
3"=1 MILE

STA 2040+00 BEG JOB 3645
F.A.P. I-30-1 (23) 42
STA 2040+00 END JOB 3644
F.A.P. I-30-1 (22) 36



STA 2192+00 END JOB 3645
F.A.P. I-30-1 (23) 42
STA 2192+00 BEG JOB 3723
F.A.P. I-30-1 (59) 45

CONVENTIONAL SIGNS

STATE LINE	RETAINING WALL
COUNTY LINE	BASE OR SURVEY LINE
CITY OR VILLAGE	LEVEE
TOWNSHIP LINE	CULVERTS
SECTION LINE	DROP INLET
GRANT LINE	TROLLEY POLE
FENCE LINE	POWER POLE
UNFENCED PROPERTY	TELEPHONE OR TELEGRAPH
RIGHT OF WAY LINE	MARSH
TRAVELED WAY	HEDGE
RAILROADS	BUILDINGS

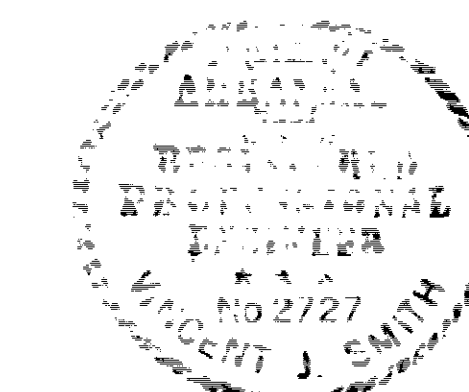
SCALES } PLAN 1"=100'
 } PROFILE HORIZ. 1"=100' VERTICAL 1"=10'

LENGTH COMPUTED ALONG RIGHT MAIN LANE			
GROSS LENGTH	OF PROJECT	15,200.00	FEET OR 2.878 MILES
NET	ROADWAY	15,200.00	" " 2.878
NET	BRIDGES	0.00	" " 0.000
NET	PROJECTS	15,200.00	" " 2.878

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FACIAL YEAR	NO. SHEET	TOTAL SHEETS
6	ARK	I-30-1 (23) 42		1	114
JOB NO.		3645			

PROPOSED BRIDGES — JOB 3645
② I-30 HWY 332 — HWY. 19

LOCATION	BRIDGE ENDS		BRIDGE LENGTH OF LIN. FT.	DEGREE OF SKEW	CLEAR RDWY WIDTH	BRIDGE NO.
	STATION	STATION				
A S H 24	12+55.33	17+44.67	489.34	48°32'40"	39'-0"	5061
PRESCOTT-NORTHWESTERN R. R.	13+27.03	16+72.99	345.96	23°19'30"		5062



CONSULTING ENGINEERS
BENHAM-BLAIR & AFFILIATES
BY *Vincent J. Smith*

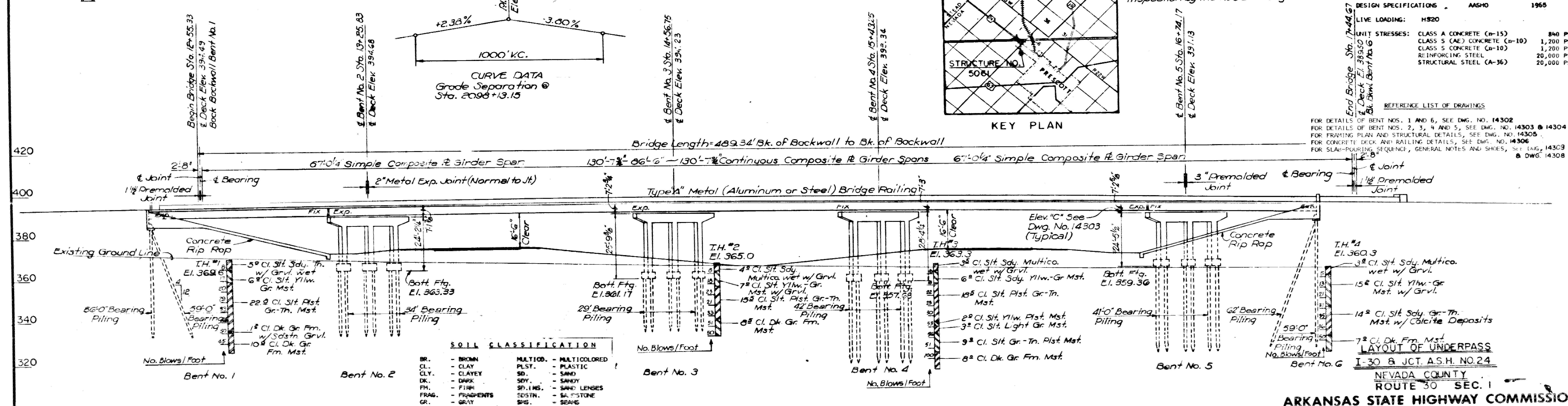
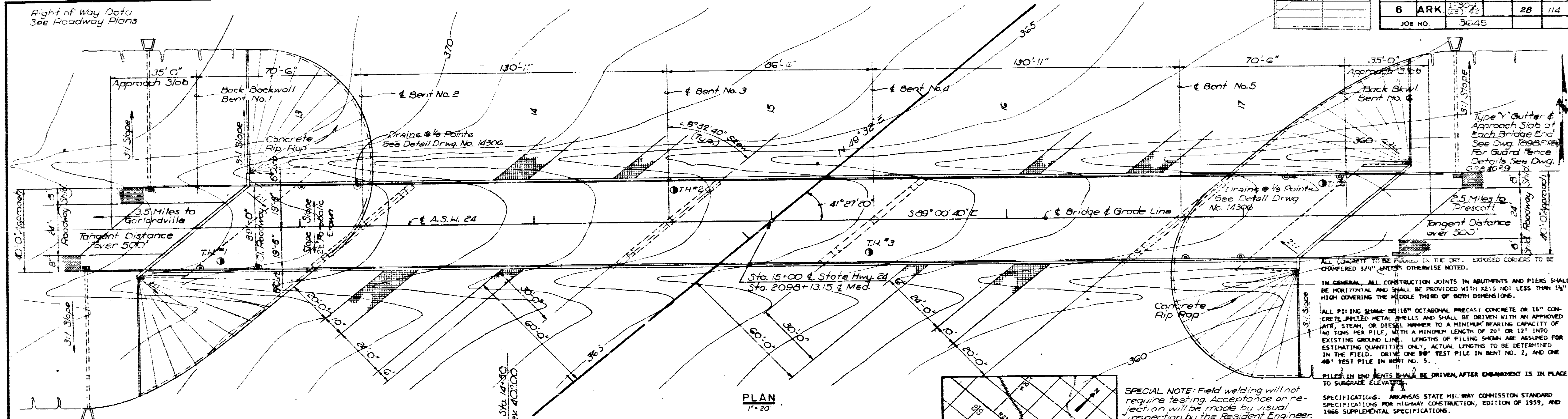
RECOMMENDED FOR APPROVAL	
RECOMMENDED FOR APPROVAL	BRIDGE DESIGN ENGINEER
RECOMMENDED FOR APPROVAL	ROADWAY DESIGN ENGINEER
RECOMMENDED FOR APPROVAL	DISTRICT ENGINEER
APPROVED	CHIEF ENGINEER

U. S. DEPARTMENT OF TRANSPORTATION
BUREAU OF PUBLIC ROADS

RECOMMENDED FOR APPROVAL	
APPROVED	
DIVISION ENGINEER	DATE

P & S JOB NO. 1355
F. A. P. I-30-1 (2) 40

FILE NO.	STATE	PROJECT NO.	FISCAL YEAR	UNIT	TOTAL SHEETS
5061	ARK.	14301		28	114
JOB NO.		3645			



DESIGN SPECIFICATIONS

ITEM	DESCRIPTION	UNIT	QUANTITY
LIVE LOADING	HS20		
UNIT STRESSES	CLASS A CONCRETE (C-15)	840 PSI	
	CLASS S (C-10) CONCRETE (C-10)	1,200 PSI	
	CLASS S CONCRETE (C-10)	1,200 PSI	
	REINFORCING STEEL	20,000 PSI	
	STRUCTURAL STEEL (A-36)	20,000 PSI	

REFERENCE LIST OF DRAWINGS

FOR DETAILS OF BENT NOS. 1 AND 6, SEE DWG. NO. 14302
 FOR DETAILS OF BENT NOS. 2, 3, 4 AND 5, SEE DWG. NO. 14303 & 14304
 FOR FRAMING, PLAN AND STRUCTURAL DETAILS, SEE DWG. NO. 14305
 FOR CONCRETE, DECK AND RAILING DETAILS, SEE DWG. NO. 14306
 FOR SLAB-FRAMING SEQUENCE, GENERAL NOTES AND SHEETS, SEE DWG. 14309 & DWG. 14308

NEVADA COUNTY
 ROUTE 30 SEC. 1
 ARKANSAS STATE HIGHWAY COMMISSION
 LITTLE ROCK, ARK.
 BRIDGE NO. 5061
 DRAWING NO. 14301

ITEM NO.	STATE	PROJECT NO.	PICT. YEAR	SHEET NO.	TOTAL SHEETS
6	ARK.	14302	1942	29	114
JOB NO. 3645					

BAR LIST - BENT 1 OR 6

MARK	SIZE	NO.	LENGTH	"A"	"B"	FIN DIA
B1	6	4	56'-7"	-	-	Str.
B2	6	4	56'-7"	-	-	Str.
B3	4	34	11'-5"	See Diagrams	1/2"	Str.
B4	4	33	7'-10"	See Diagrams	1/2"	Str.
B5	4	14	56'-1"	-	-	Str.
B6	5	16	7'-10"	-	-	Str.
B7	4	59	3'-1"	1'-2"	0'-9"	1/2"
B8	4	5	5'-2"	-	-	Str.
B9	4	17	7'-4"	5'-10"	1'-6"	1/2"
B10	4	2	7'-0"	-	-	Str.
B11	4	1	8'-0"	-	-	Str.
B12	4	17	10'-1"	-	-	Str.
B13	4	12	8'-2"	2'-6"	4'-11 1/2"	1/2"
B14	4	16	7'-10"	-	-	Str.
B15	4	2	8'-0"	1'-8"	5'-2"	1/2"
T01	5	13	4'-3"	-	-	Str.
T02	5	3	3'-2"	-	-	Str.
T03	3	4	7'-5 1/2"	2'-9"	9'	1 1/4"
T04	3	4	10'-1 1/2"	2'-10 1/2"	2'-6"	1 1/4"

PLAN

Scale: 3/8" = 1'-0"

50-B6#6 @ 12" ctrs. Ea. Face of Backwall
 50-B7#4 @ 12" ctrs. in Paving Bracket

ELEVATION

Scale: 3/8" = 1'-0"

BENDING DIAGRAMS

GENERAL NOTES

ALL CONCRETE TO BE CLASS S AND SHALL BE POURED IN THE DRY.
 ALL EXPOSED CORNERS TO BE CHAMFERED 1" UNLESS OTHERWISE NOTED.

REINFORCING STEEL SHALL BE DEFORMED BARS OF INTERMEDIATE OR HARD GRADE. SHOP LIST AND BENDING DIAGRAMS MUST BE SUBMITTED FOR APPROVAL BEFORE FABRICATION IS BEGUN. STEEL WIRE SUPPORTS USED TO SUPPORT AND SPACE THE BARS WILL NOT BE PAID FOR DIRECTLY BUT WILL BE CONSIDERED SUBSIDIARY TO THE ITEM "REINFORCING STEEL".

CONSTRUCTION JOINTS SHALL BE HORIZONTAL AND SHALL BE PROVIDED WITH KEYS NOT LESS THAN 1 1/2" HIGH, COVERING THE MIDDLE THIRD OF BOTH DIMENSIONS.

FOR ADDITIONAL NOTES AND LAYOUT, SEE DWG. NO. 14501

FOR DETAILS OF SUPERSTRUCTURE, SEE DWG. NO.'S 14305, 14306 AND 14308.

DETAILS OF BENTS NO. 1 & 6

A.S.H. NO. 24 UNDERPASS

NEVADA COUNTY

ROUTE 30 SEC. 1

ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

DRAWN BY LRS DATE

TRACED BY LMS DATE

CHECKED BY DLY DATE

BRIDGE NO. 5061 DRAWING NO. 14302

BRIDGE ENGINEER

SCALE AS NOTED

Place Type "C" Standard
 Bridge Plate on Right-Hand
 Turnout - Bent No. 1 Only

DETAIL OF TURNOUT

Scale: 1/2" = 1'-0"

SECTION A-A

Scale: 1/2" = 1'-0"

ELEV.

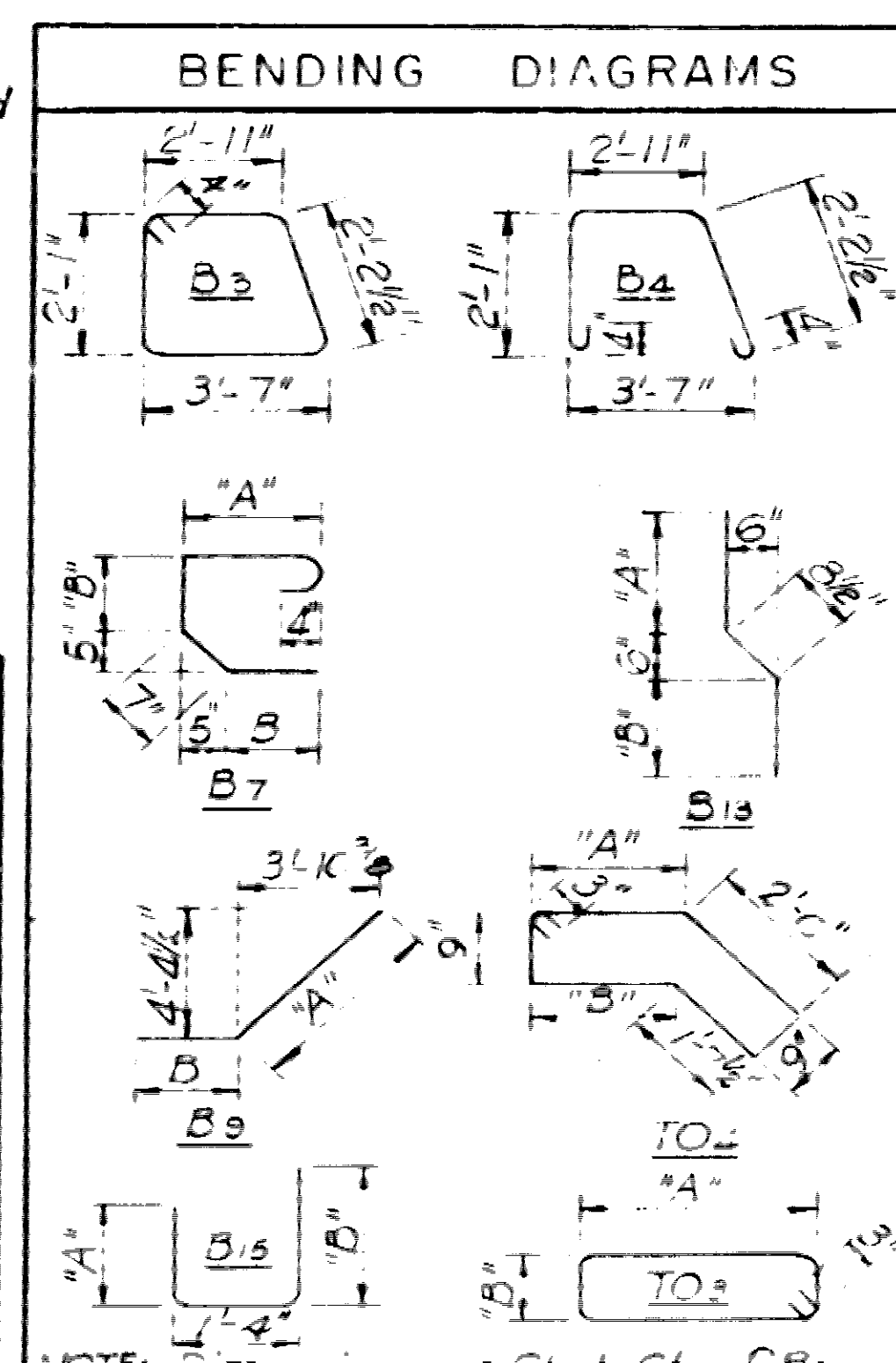
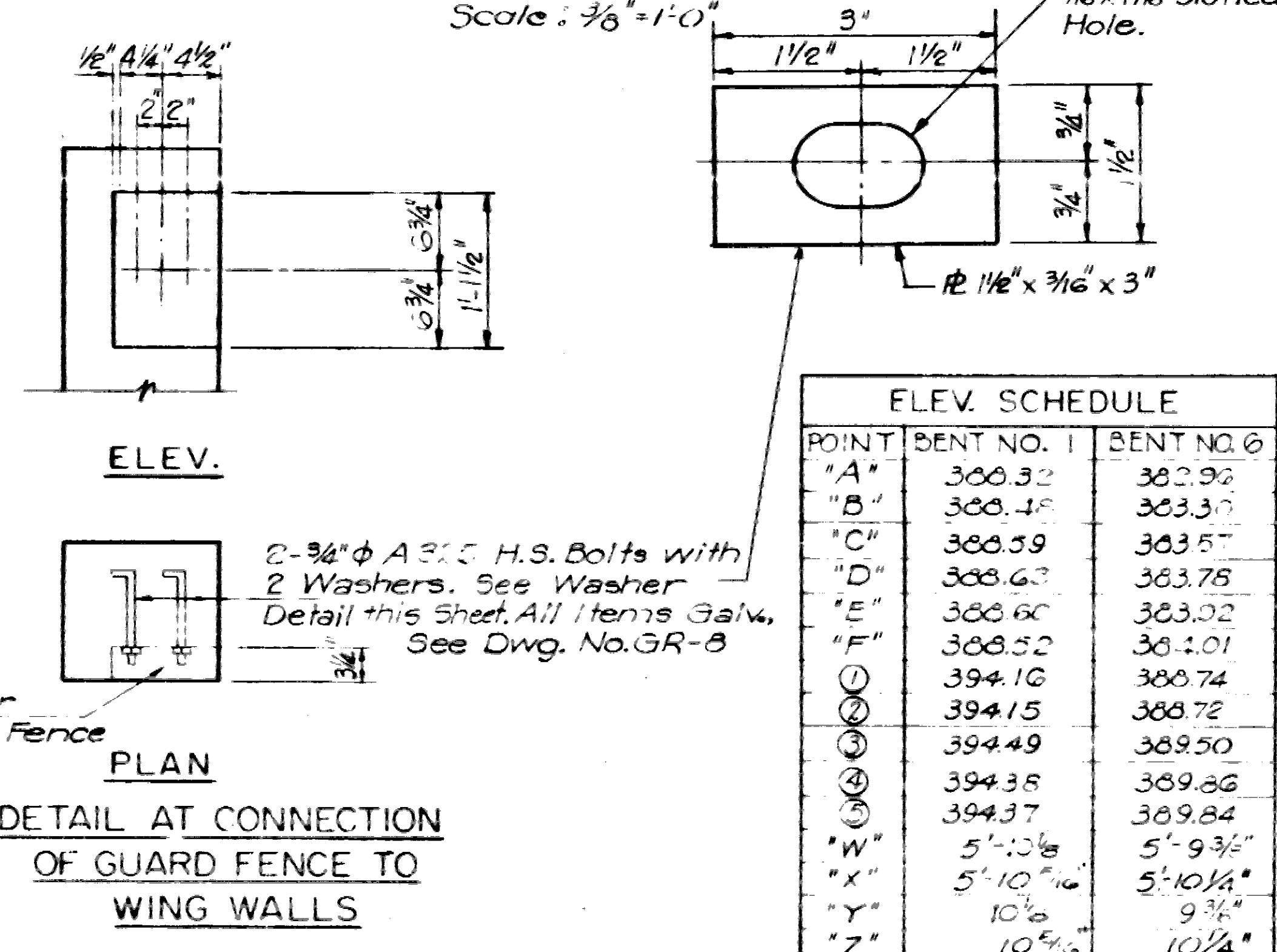
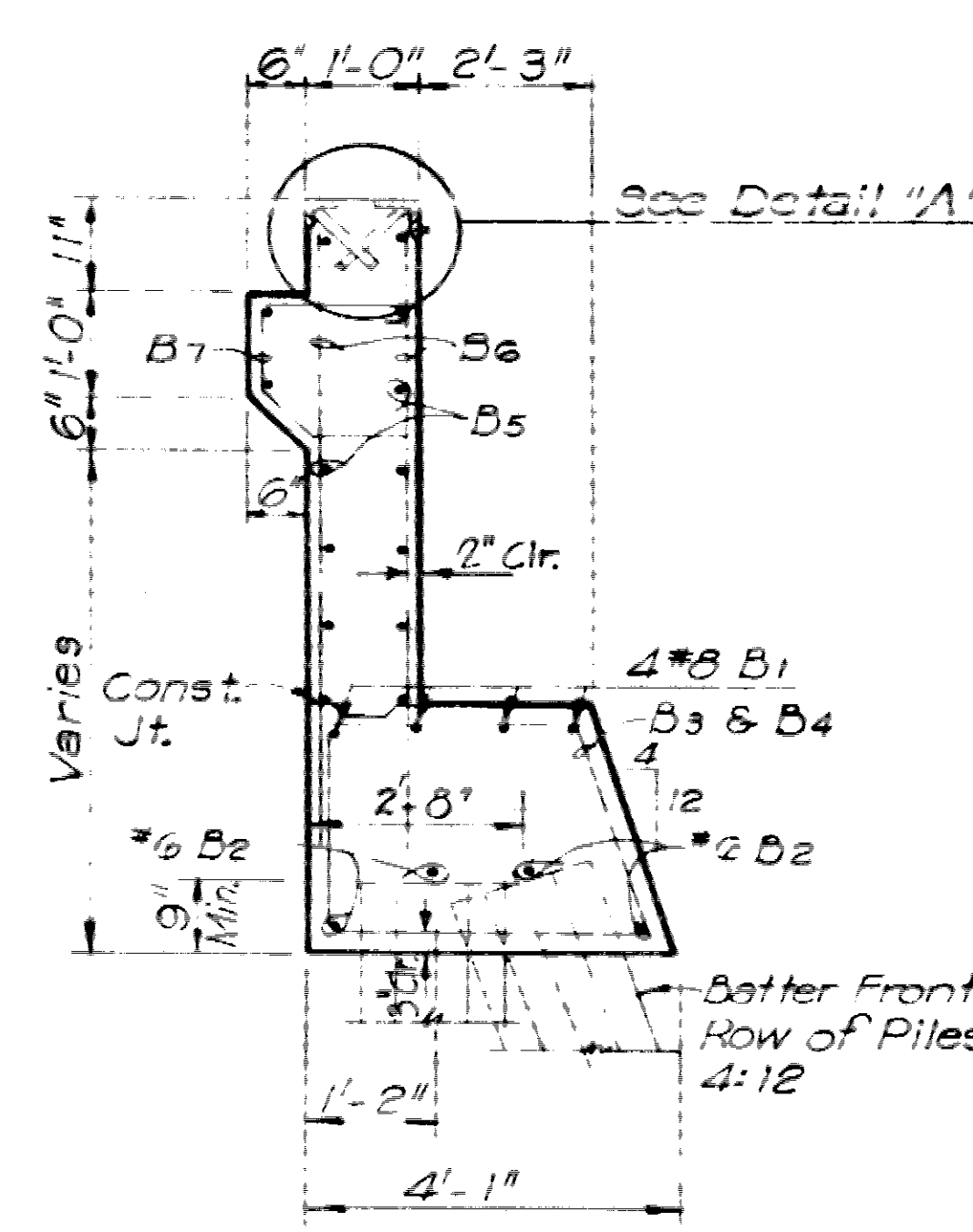
Scale: 3/8" = 1'-0"

PLAN

Scale: 3/8" = 1'-0"

DETAIL AT CONNECTION
OF GUARD FENCE TO
WING WALLS

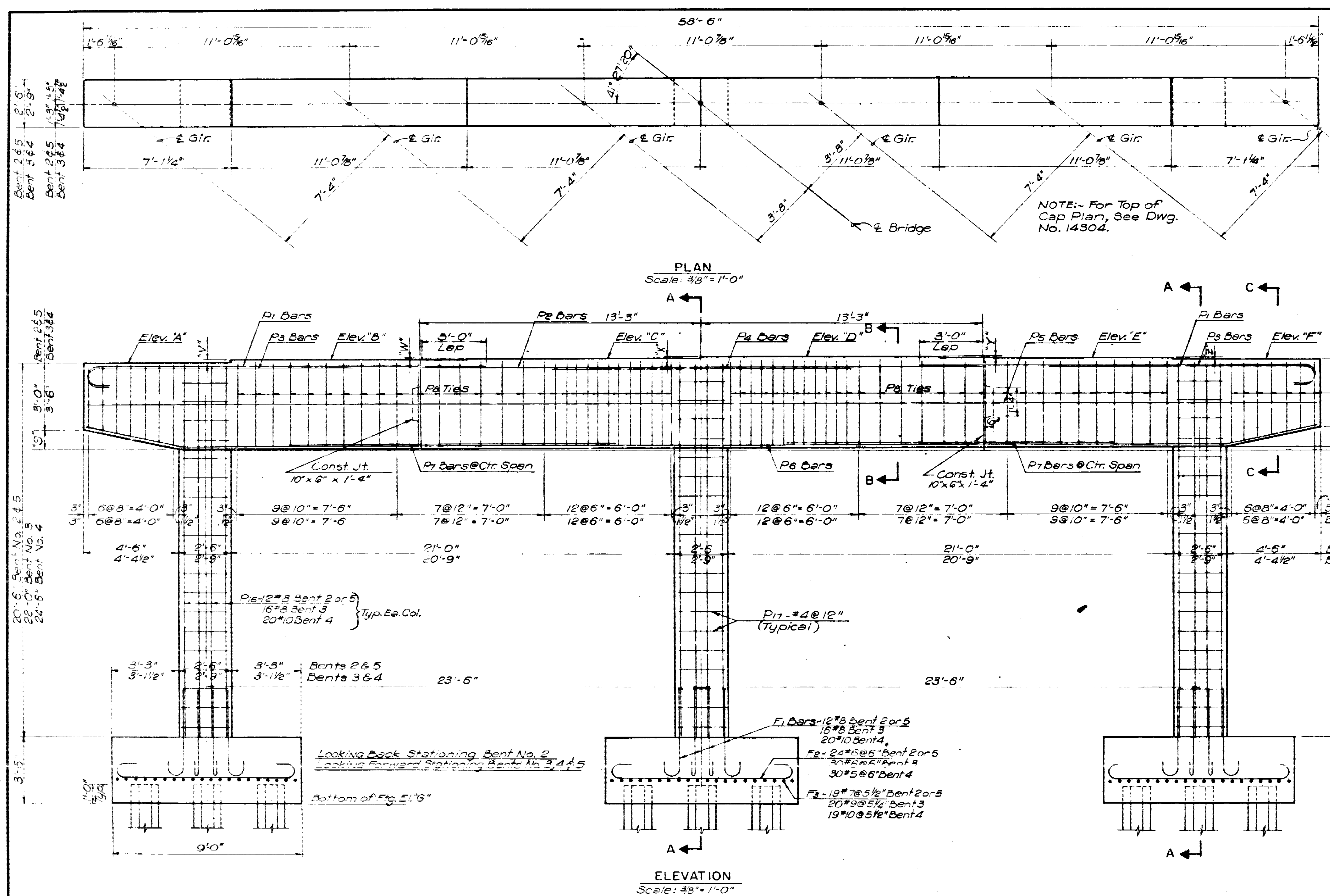
Block out for
 Metal Guard Fence



POINT	BENT NO. 1	BENT NO. 6
"A"	300.52	302.92
"B"	300.48	303.37
"C"	300.59	303.51
"D"	300.63	303.75
"E"	300.66	303.02
"F"	300.52	303.01
"G"	394.16	300.74
"H"	394.15	300.72
"I"	394.49	300.50
"J"	394.38	300.83
"K"	394.37	300.84
"L"	5'-10 1/2"	5'-9 3/4"
"M"	5'-10 1/2"	5'-10 1/4"
"N"	10'-0"	9'-3"
"O"	10'-0"	10'-1/4"

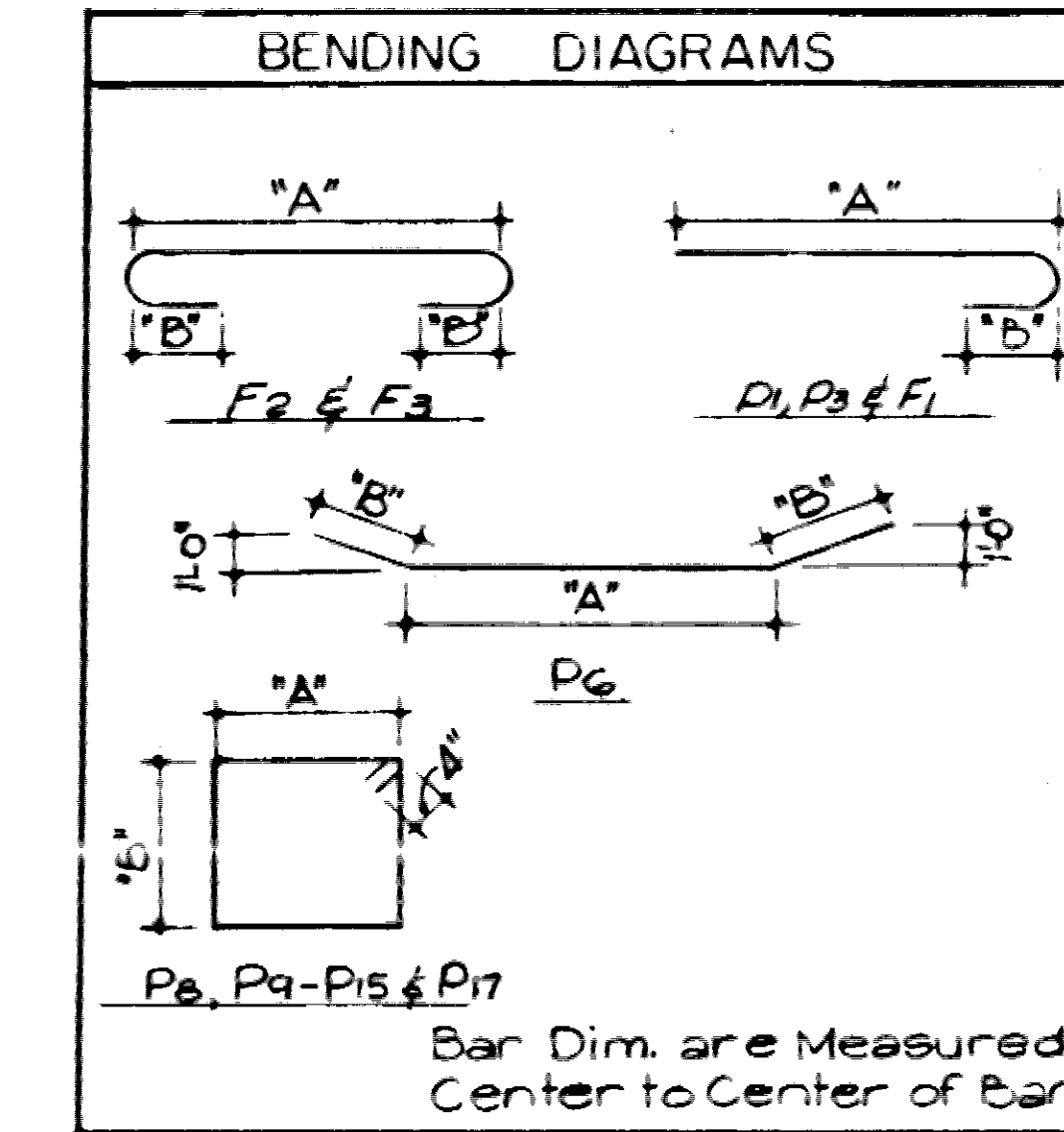
DETAIL "A"

Scale: 1/2" = 1'-0"



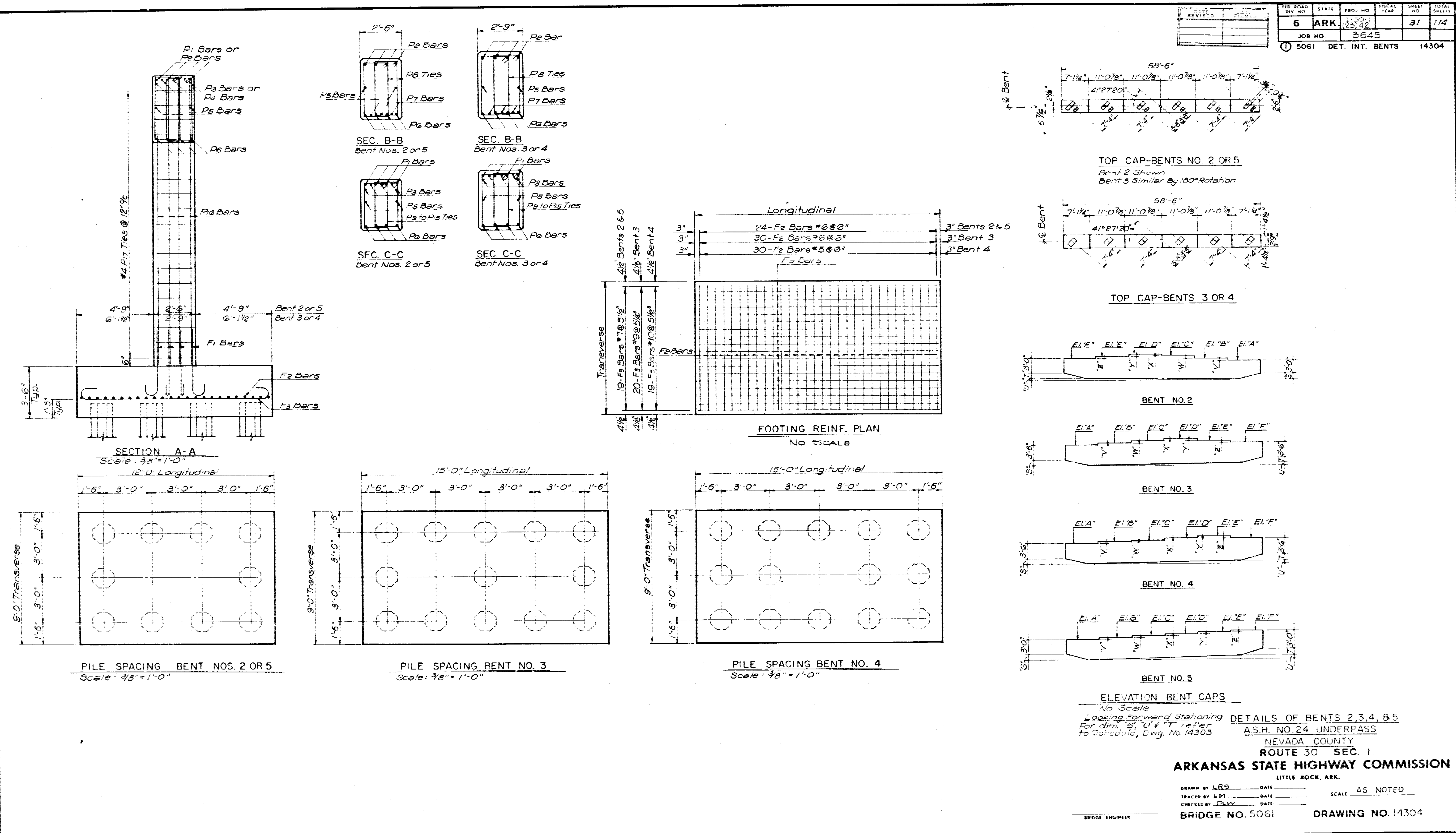
ELEVATION SCHEDULE				
POINT	BENT#2	BENT#3	BENT#4	BENT#5
"A"	387.33	386.67	385.68	383.36
"B"	387.46	386.86	385.92	383.66
"C"	387.52	386.99	386.09	383.90
"D"	387.53	387.05	386.19	384.07
"E"	387.47	387.05	386.24	384.18
"F"	387.35	386.99	386.22	384.23
"G"	383.33	361.17	357.68	359.36
"S"	1'-0"	1'-0"	1'-0"	1'-0"
"T"	1'-0"	1'-0"	1'-0"	1'-0"
"U"	0'-6"	3'-4"	5'-6"	8'-6"
"V"	1'-6"	2'-6"	2'-6"	3'-6"
"W"	0'-6"	1'-6"	2'-6"	2'-6"
"X"	0'-6"	0'-6"	1'-6"	2'-6"
"Y"	0'-6"	0"	0'-6"	1'-6"
"Z"	1'-6"	1'-6"	0'-6"	0'-6"

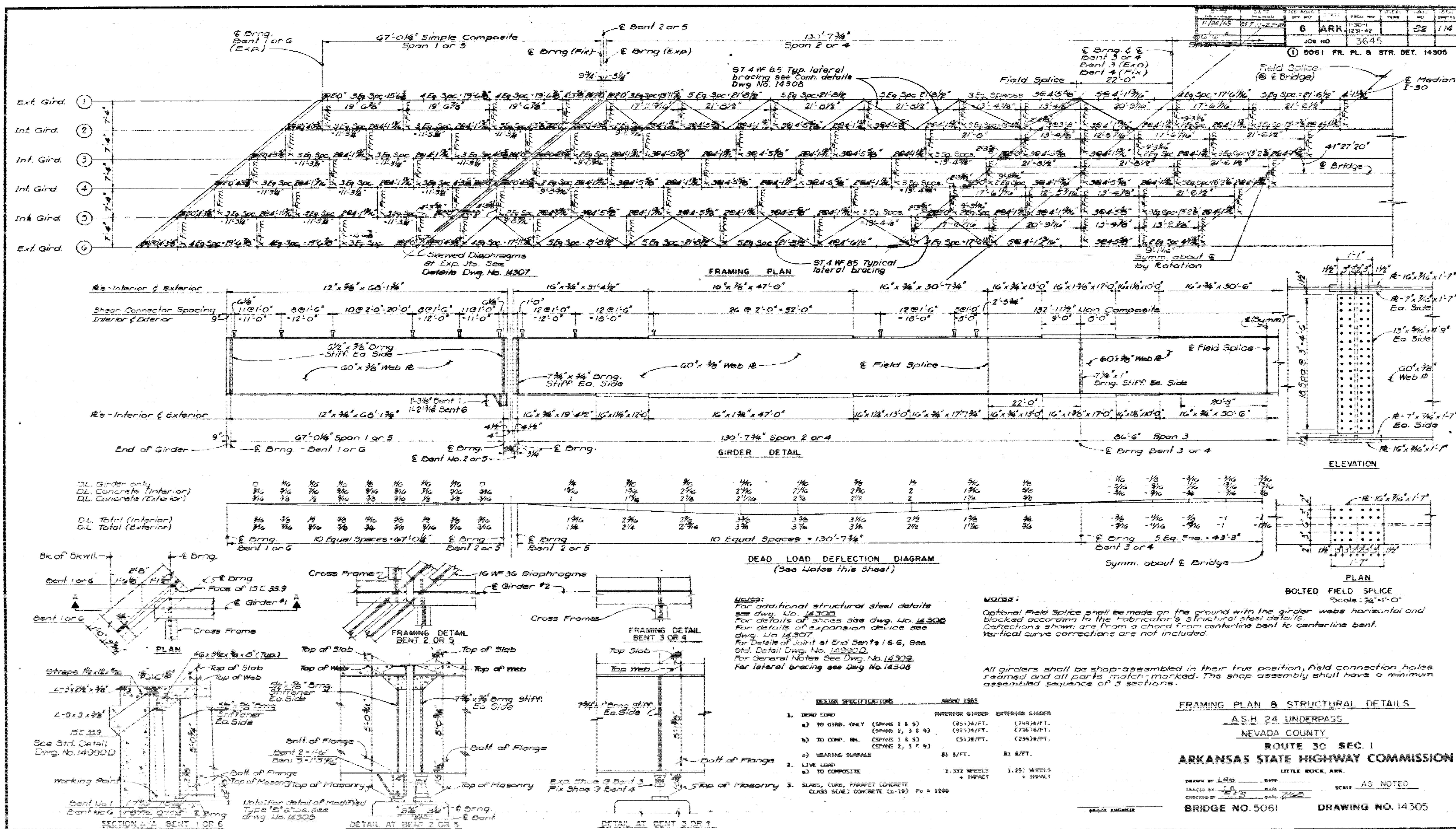
10 5061 DET. INT. BENTS 14303									
JOB ROAD DIV NO.		STATE	PROJ. NO.	FISCAL YEAR		SHEET NO.		TOTAL SHEETS	
6		ARK.	1-563-1 23-42			30		114	
JOB NO. 3645									
BAR LIST									
BENT NO. 2 OR 5									
MARK	NO.	SIZE	LENGTH	"A"	"B"	PIN DIA.			
P ₁	8	10	20'-2"	13'-10"	12"	1 1/2"			
P ₂	4	10	26'-6"			Str.			
P ₃	10	8	14'-10 1/2"	13'-9"	9"	8"			
P ₄	5	10	14'-0"			Str.			
P ₅	2	4	58'-2"			Str.			
P ₆	4	8	58'-6"	49'-6"	4'-6"	3"			
P ₇	6	9	15'-6"			Str.			
P ₈	116	4	10'-9"	1'-5 1/2"	3'-7 1/2"	1 1/2"			
P ₉	1	4	8'-9"			3'-1 1/2"			
P ₁₀	58	4	7'-0"	1'-5 1/2"	3'-7 1/2"	1 1/2"			
P ₁₁	28	8	0'-7"			4'-0 1/2"			
P ₁₂	36	8	20'-2"			Str.			
P ₁₃	60	4	8'-11"	2'-1"	2'-1"	1 1/2"			
F ₁	36	8	5'-10 1/2"	4'-9"	8"	8"			
F ₂	72	5	9'-1 1/2"	8'-6"	6"	4 1/2"			
F ₃	57	7	13'-2"	11'-6"	7"	5 1/2"			
BENT NO. 3									
MARK	NO.	SIZE	LENGTH	"A"	"B"	PIN DIA.			
P ₁	8	11	20'-4"	13'-10"	12"	1 1/2"			
P ₂	4	11	26'-6"			Str.			
P ₃	10	9	15'-0"	13'-9"	10"	9"			
P ₄	5	11	14'-0"			Str.			
P ₅	2	4	58'-2"			Str.			
P ₆	4	10	58'-5"	49'-9"	4'-4"	3"			
P ₇	6	10	15'-6"			Str.			
P ₈	116	5	10'-9"	1'-7"	4'-1 1/2"	1 1/2"			
P ₉	1	5	10'-9"			3'-1 1/2"			
P ₁₀	58	5	7'-0"	1'-7"	4'-1 1/2"	1 1/2"			
P ₁₁	28	8	21'-8"			4'-0 1/2"			
P ₁₂	44	8	21'-8"			Str.			
P ₁₃	66	4	9'-11"	2'-4"	2'-4"	1 1/2"			
F ₁	48	8	5'-10 1/2"	4'-9"	8"	8"			
F ₂	96	5	9'-1 1/2"	8'-6"	6"	4 1/2"			
F ₃	60	9	17'-0"	14'-5"	10"	9"			
BENT NO. 4									
MARK	NO.	SIZE	LENGTH	"A"	"B"	PIN DIA.			
P ₁	8	11	20'-4"	13'-10"	12"	1 1/2"			
P ₂	4	11	26'-6"			Str.			
P ₃	10	9	15'-0"	13'-9"	10"	9"			
P ₄	5	11	14'-0"			Str.			
P ₅	2	4	58'-2"			Str.			
P ₆	4	10	58'-5"	49'-6"	4'-4"	3"			
P ₇	6	10	15'-6"			Str.			
P ₈	116	5	10'-9"	1'-7"	4'-1 1/2"	1 1/2"			
P ₉	1	5	10'-9"			3'-1 1/2"			
P ₁₀	58	5	7'-0"	1'-7"	4'-1 1/2"	1 1/2"			
P ₁₁	28	8	21'-8"			4'-0 1/2"			
P ₁₂	44	8	21'-8"			Str.			
P ₁₃	60	10	24'-2"			Str.			
P ₁₄	72	4	9'-11"	2'-4"	2'-4"	1 1/2"			
F ₁	60	10	6'-7 1/2"	5'-3"	11"	10"			
F ₂	90	5	9'-9"	8'-6"	5"	5 1/2"			
F ₃	57	10	17'-3"	14'-5"	11"	10"			

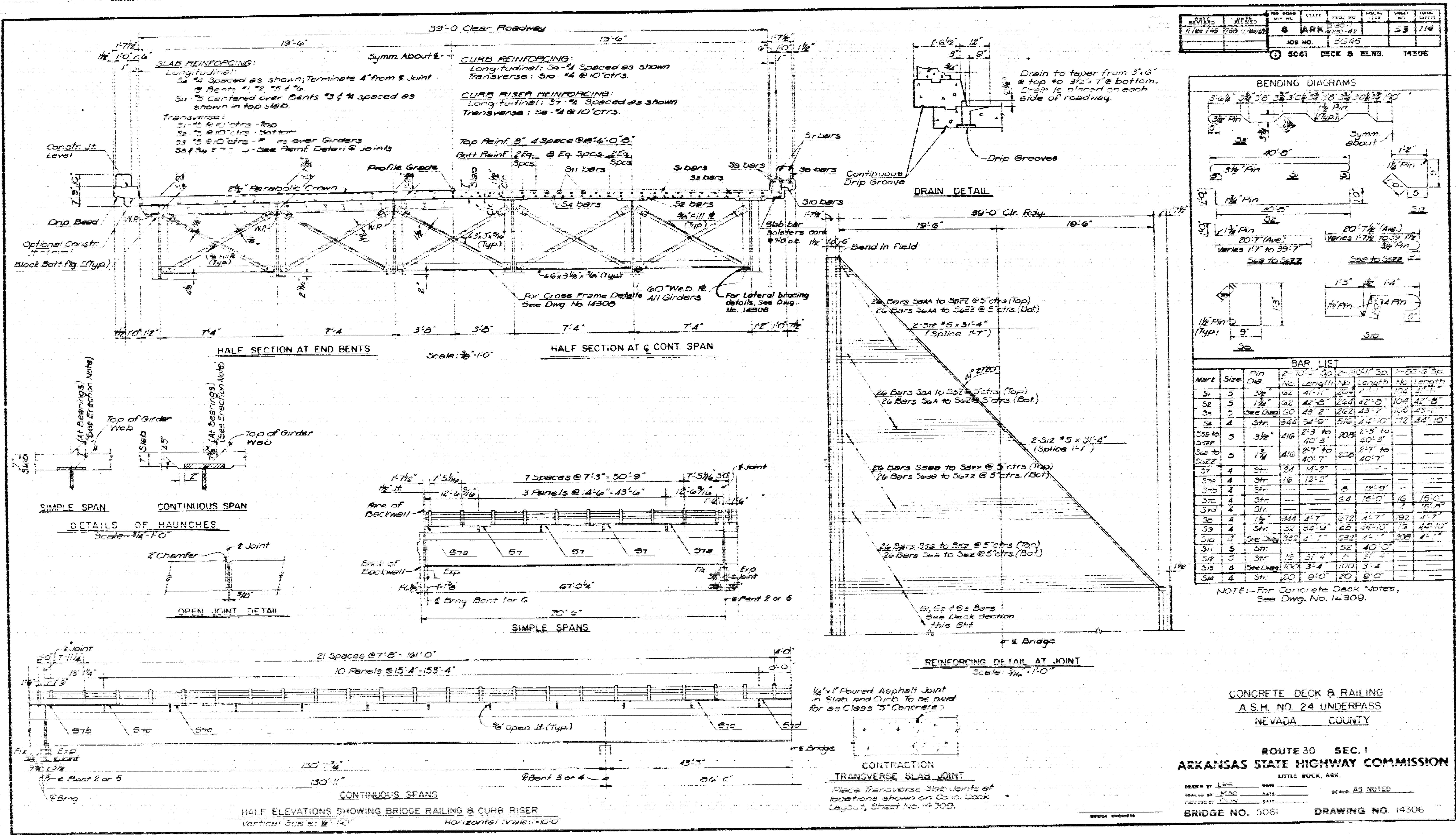


NOTES:-
1. Concrete in Ftg. to be Class "A" and Concrete in Columns and Cap to be Class "S".
2. For Additional Notes, See Dwg. No. 14302.
3. For Sections and Details, See Dwg. No. 14304.

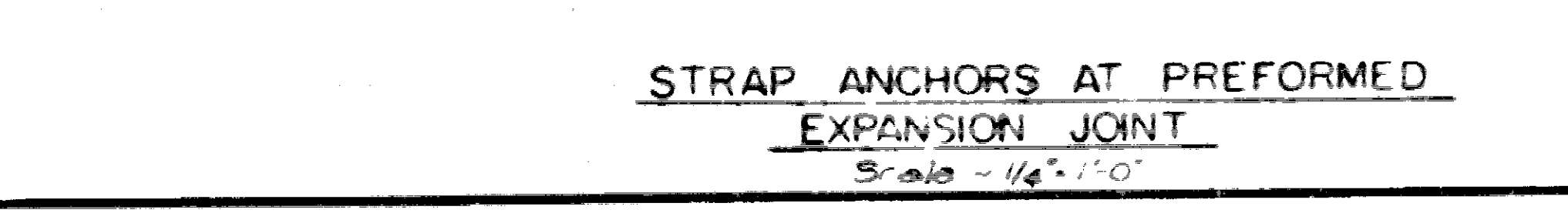
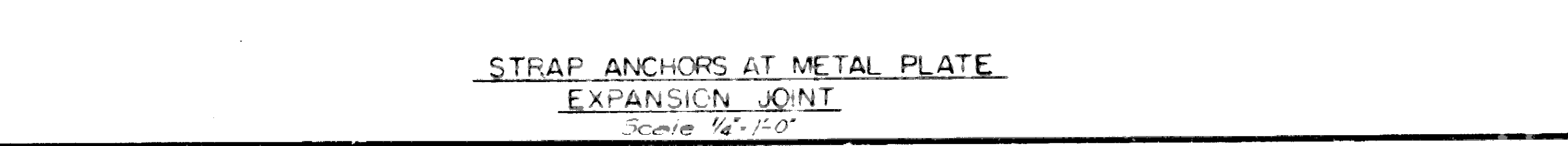
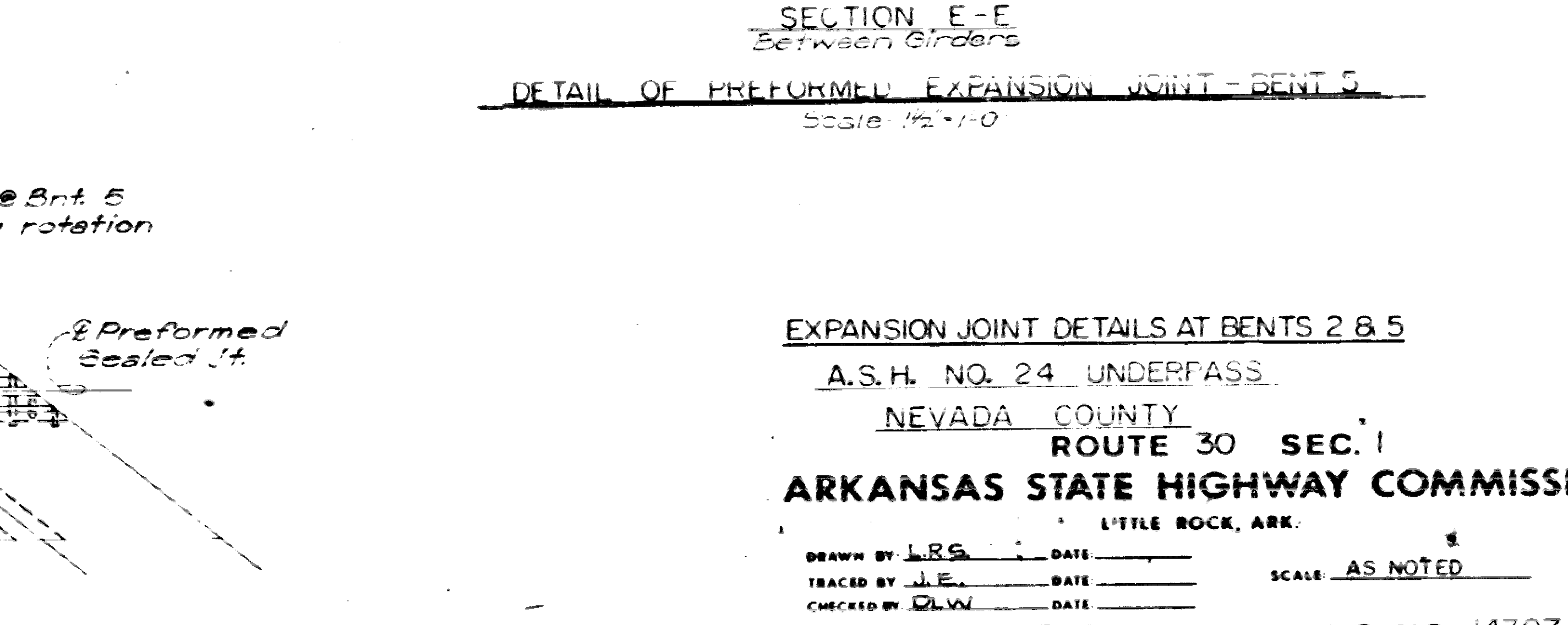
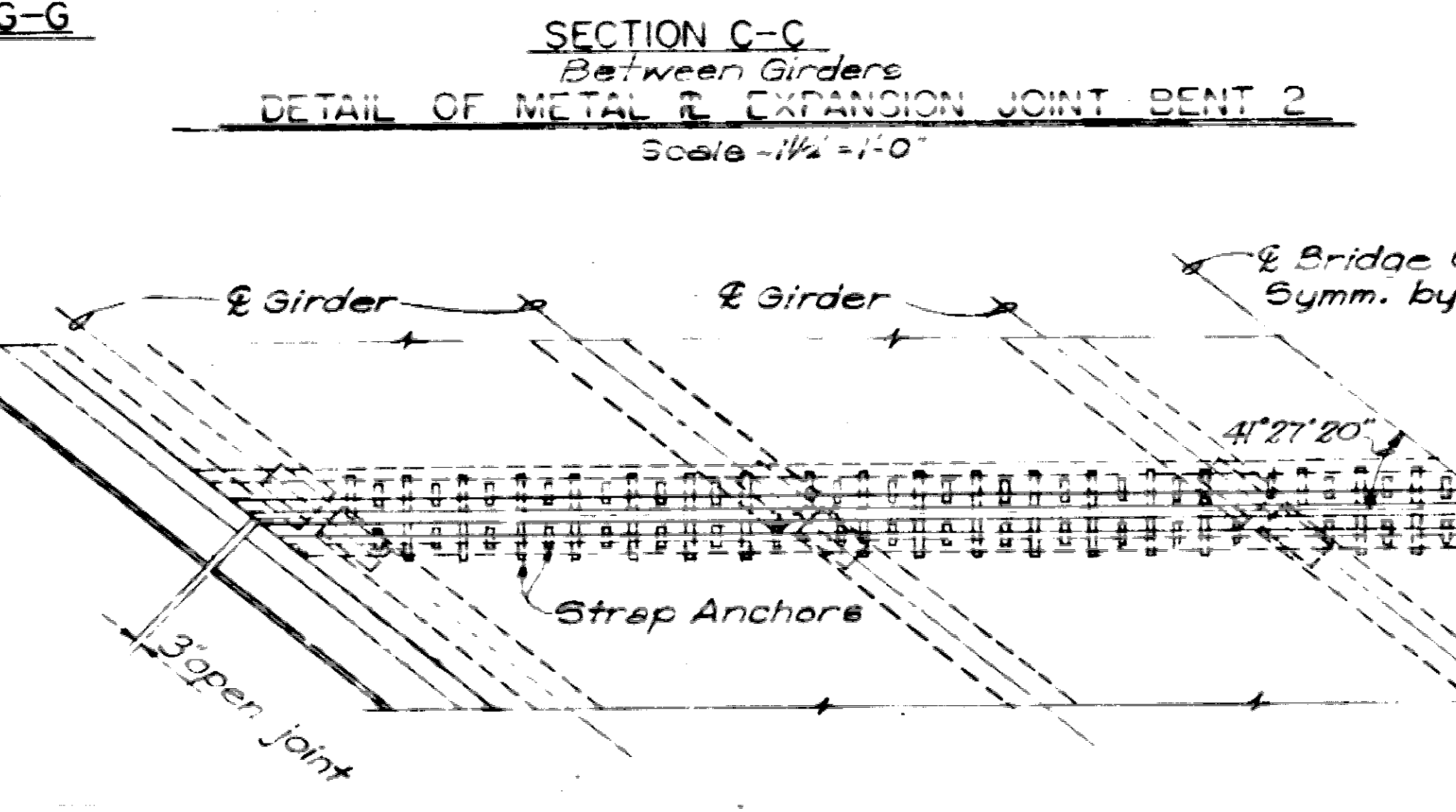
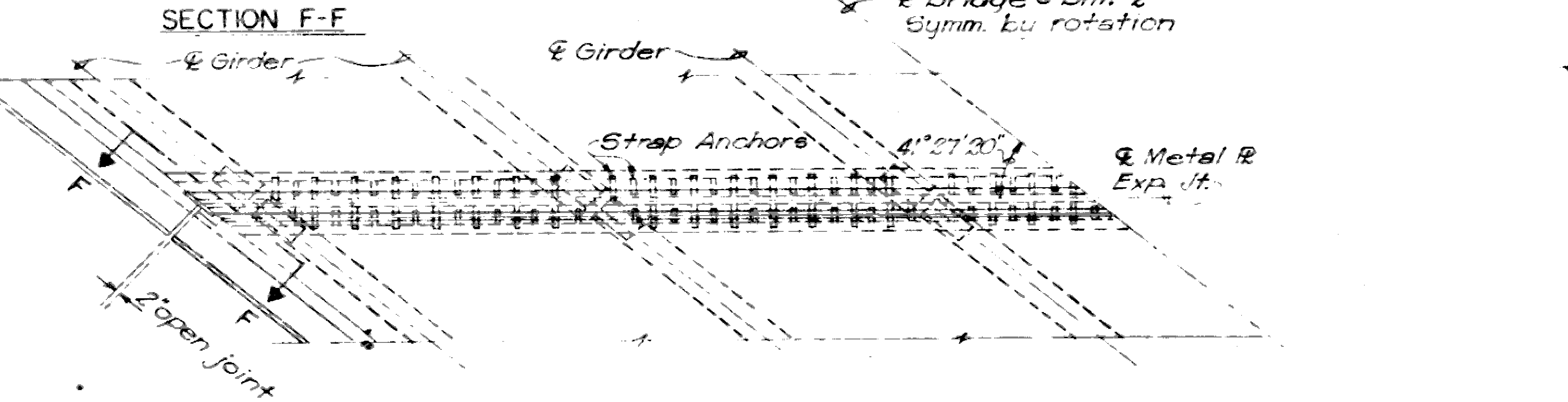
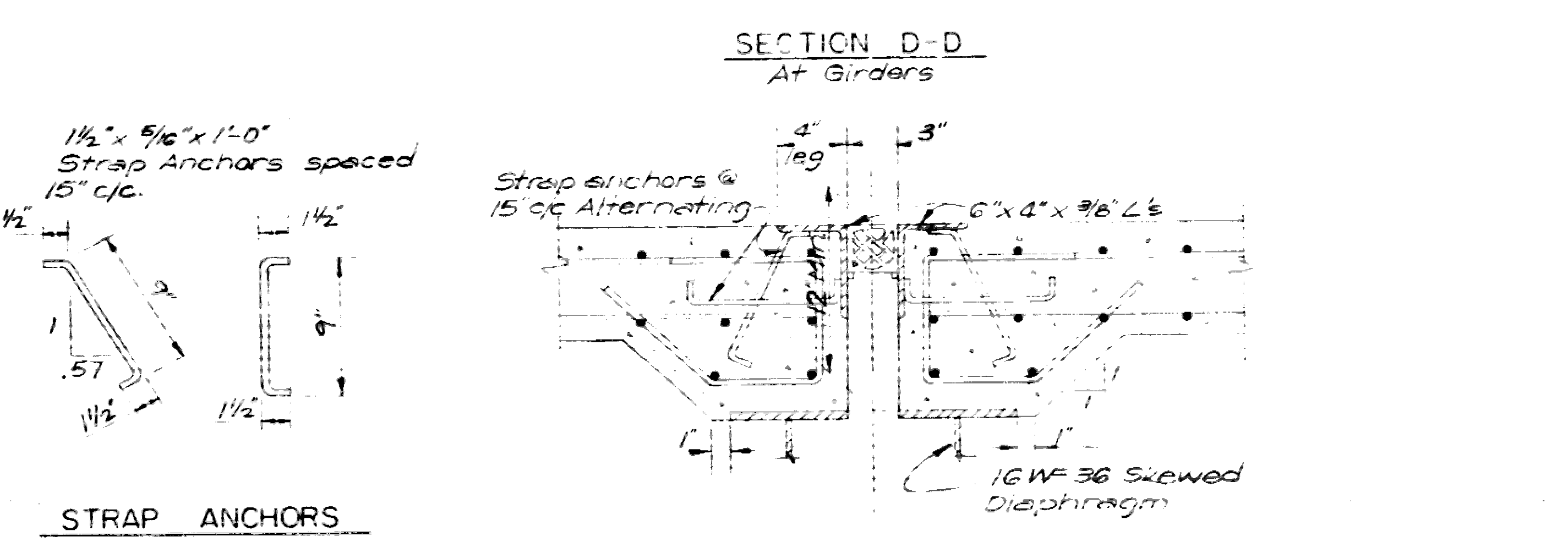
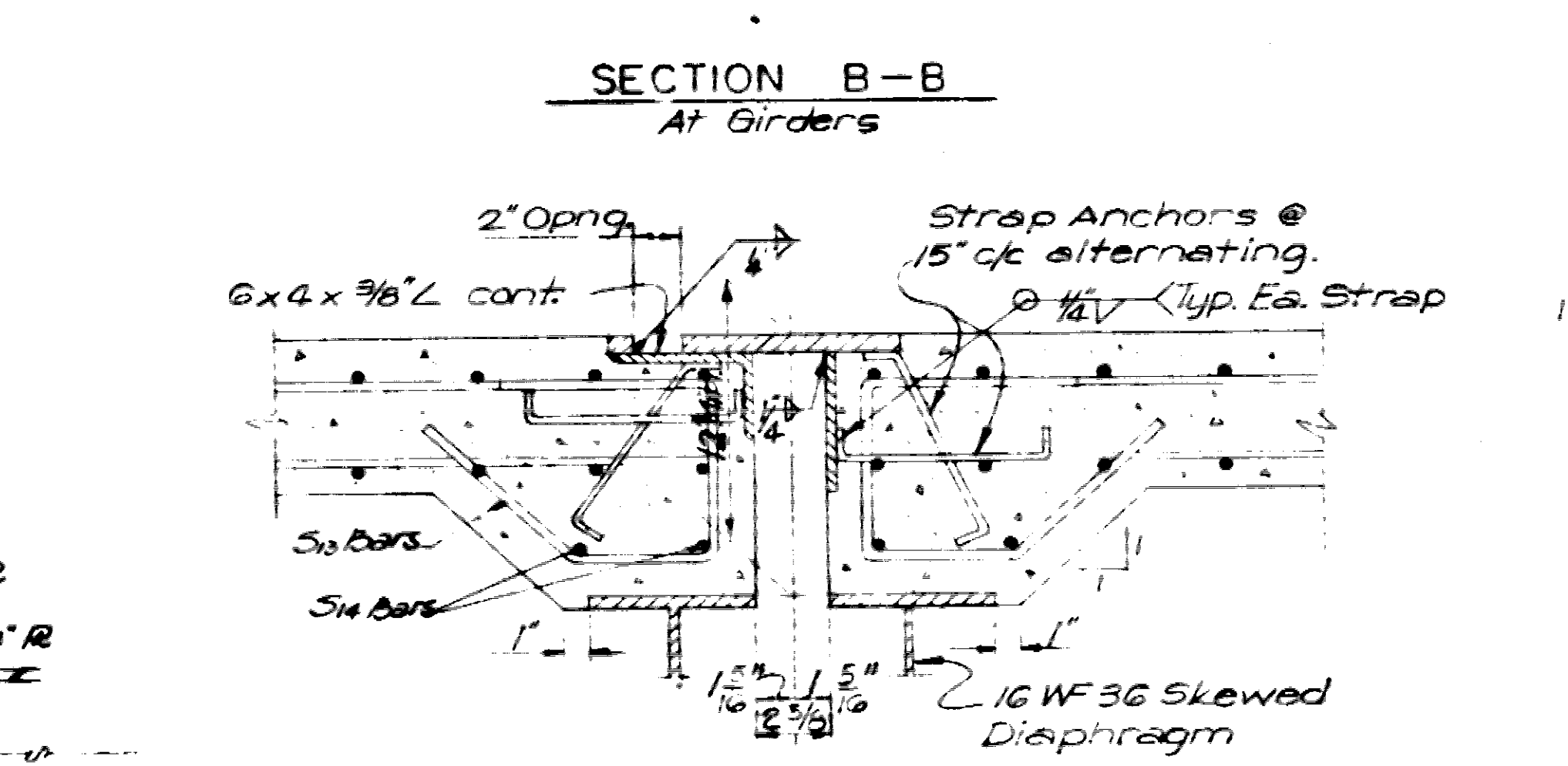
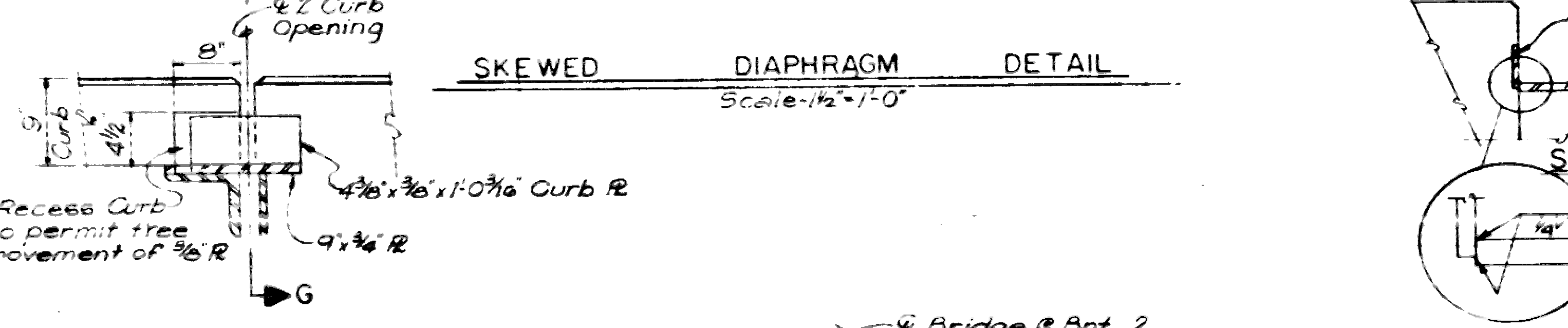
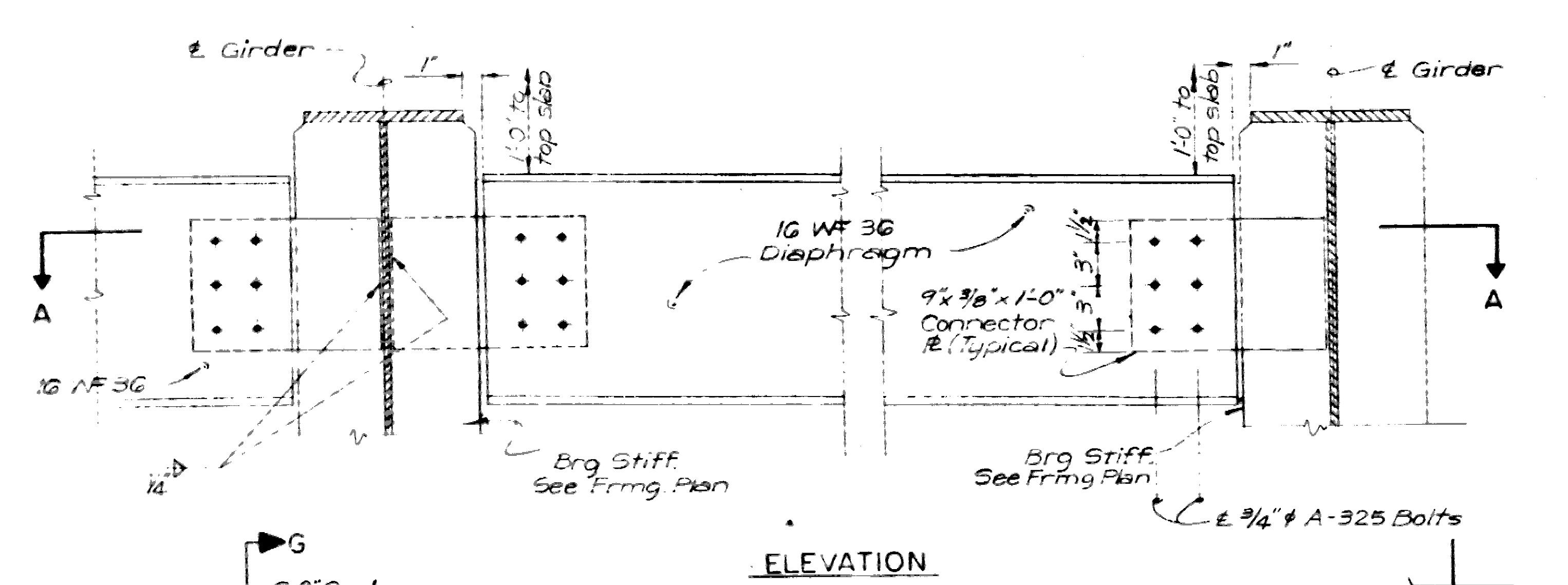
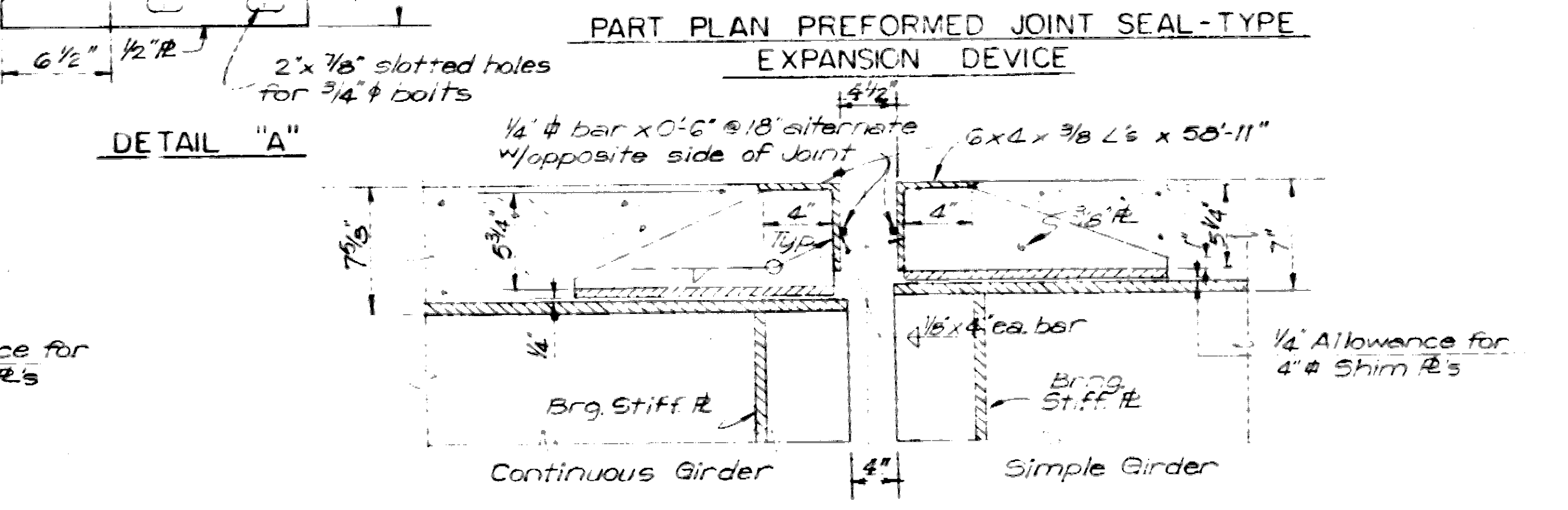
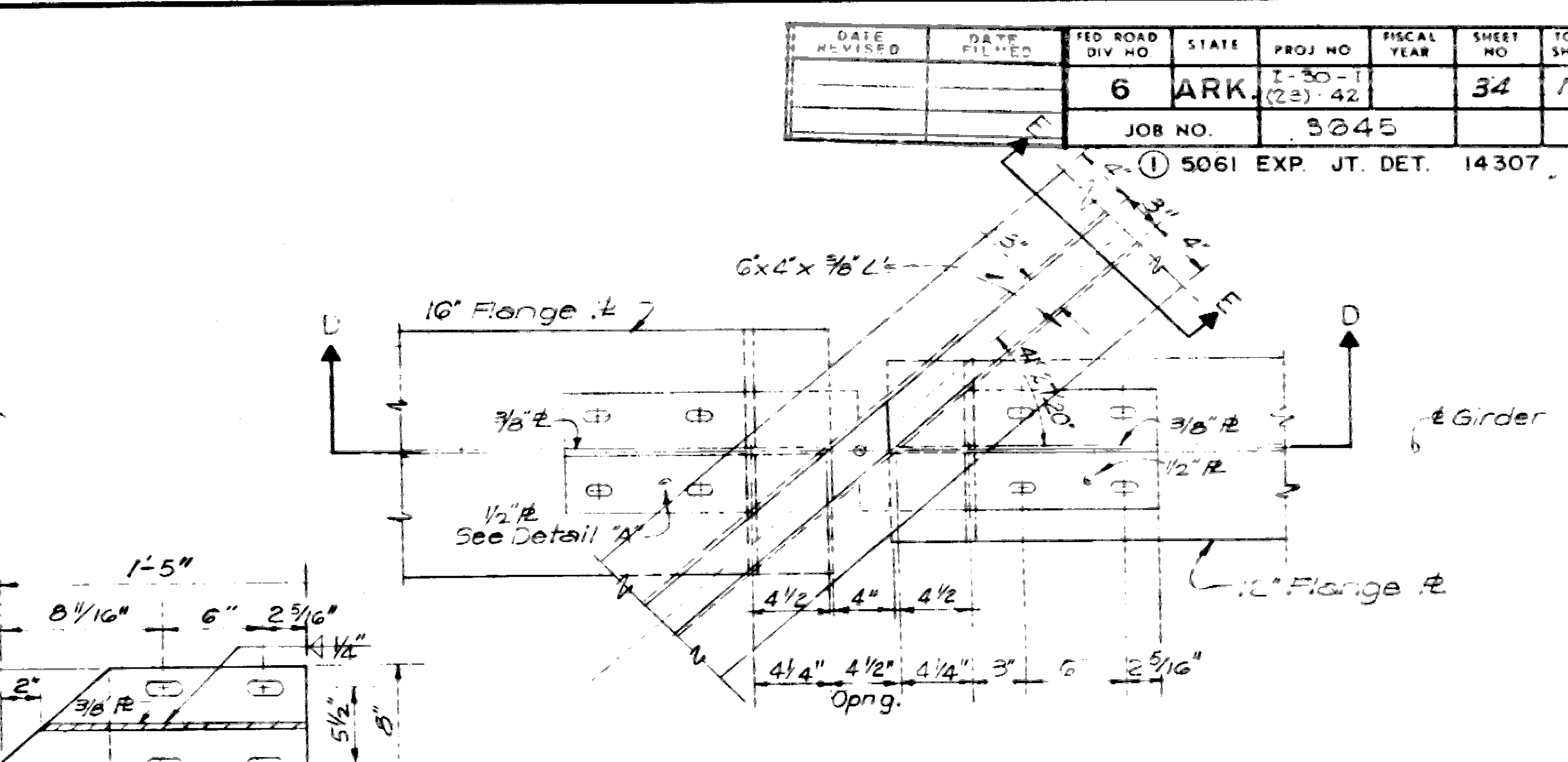
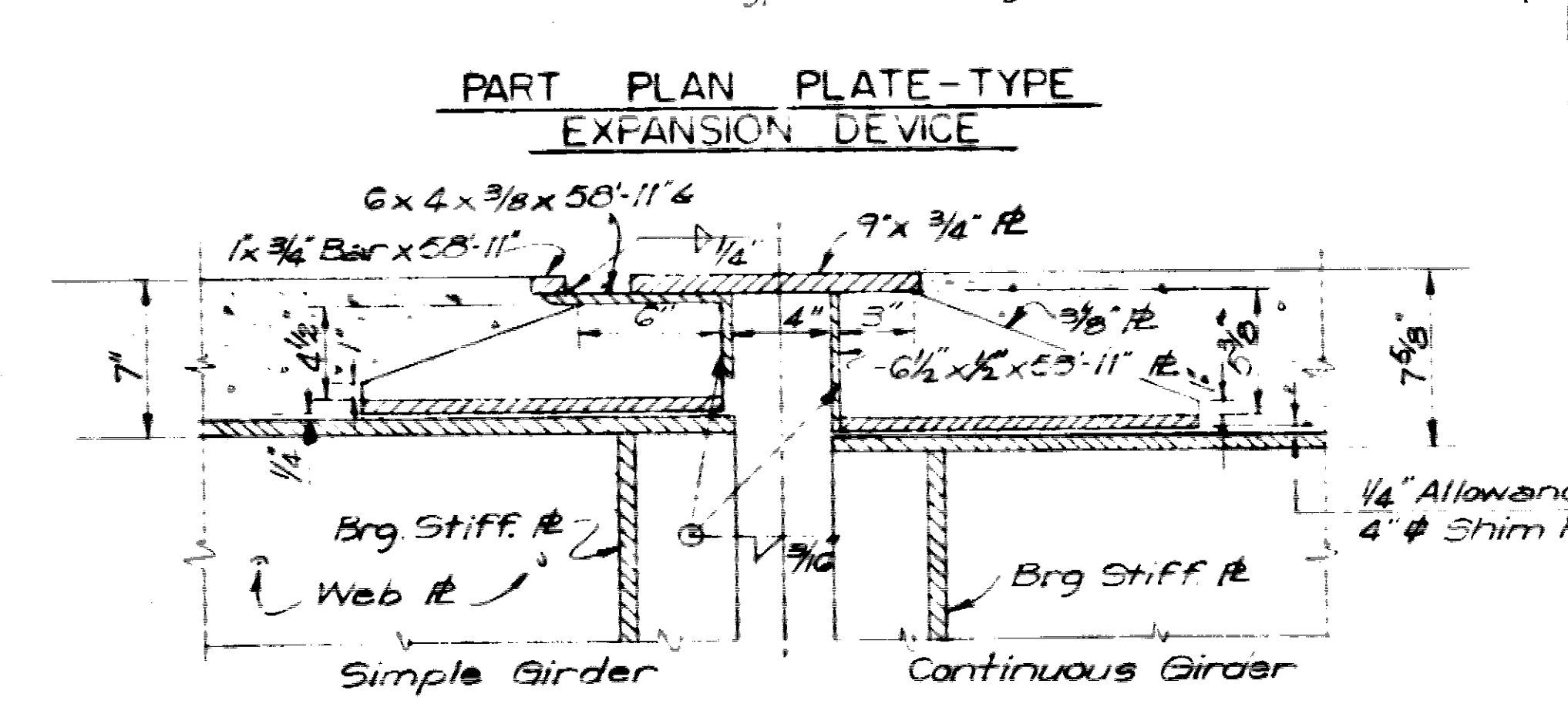
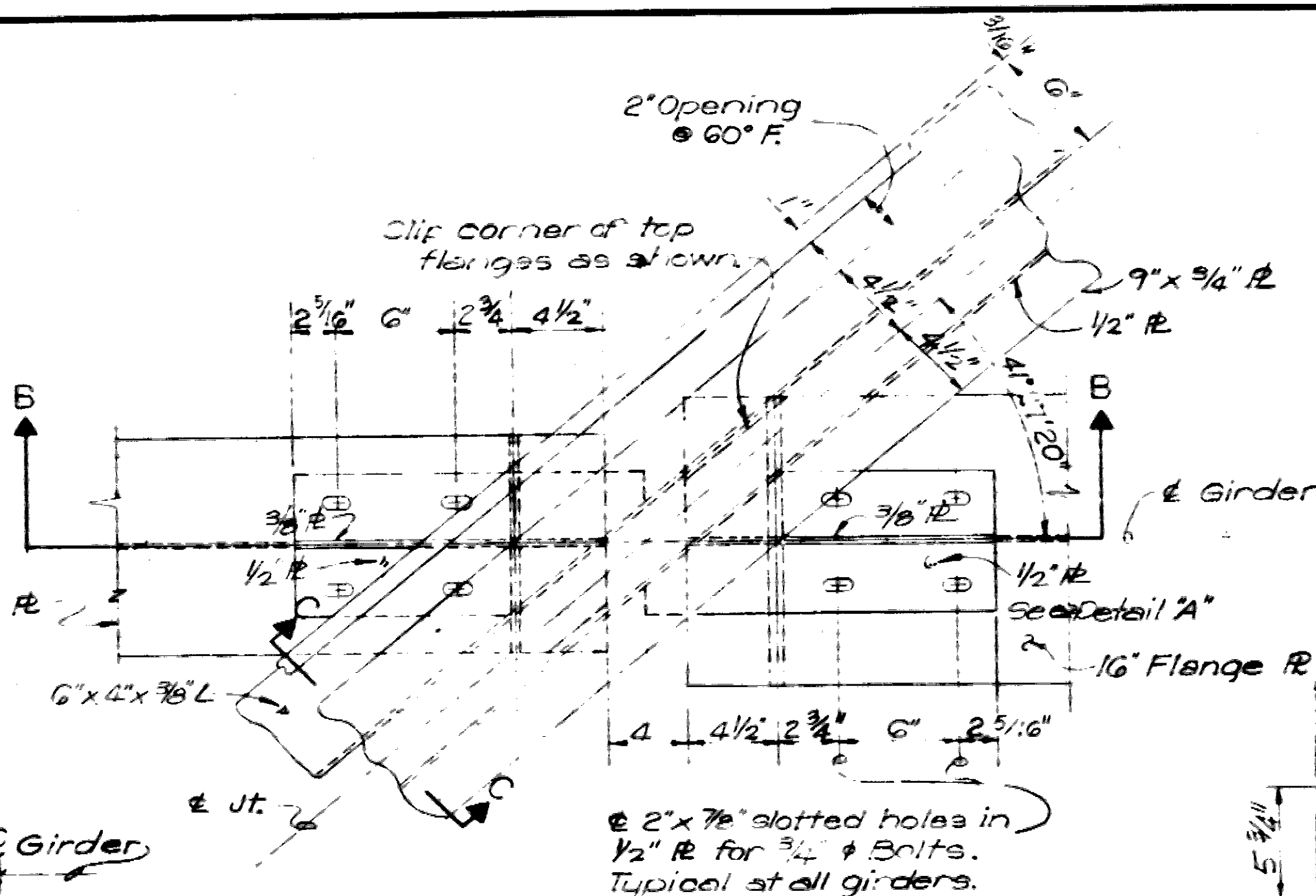
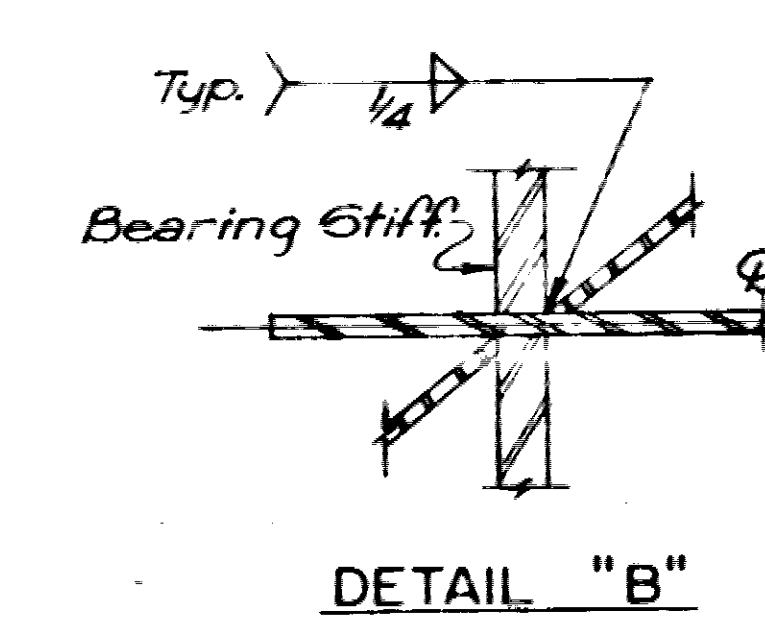
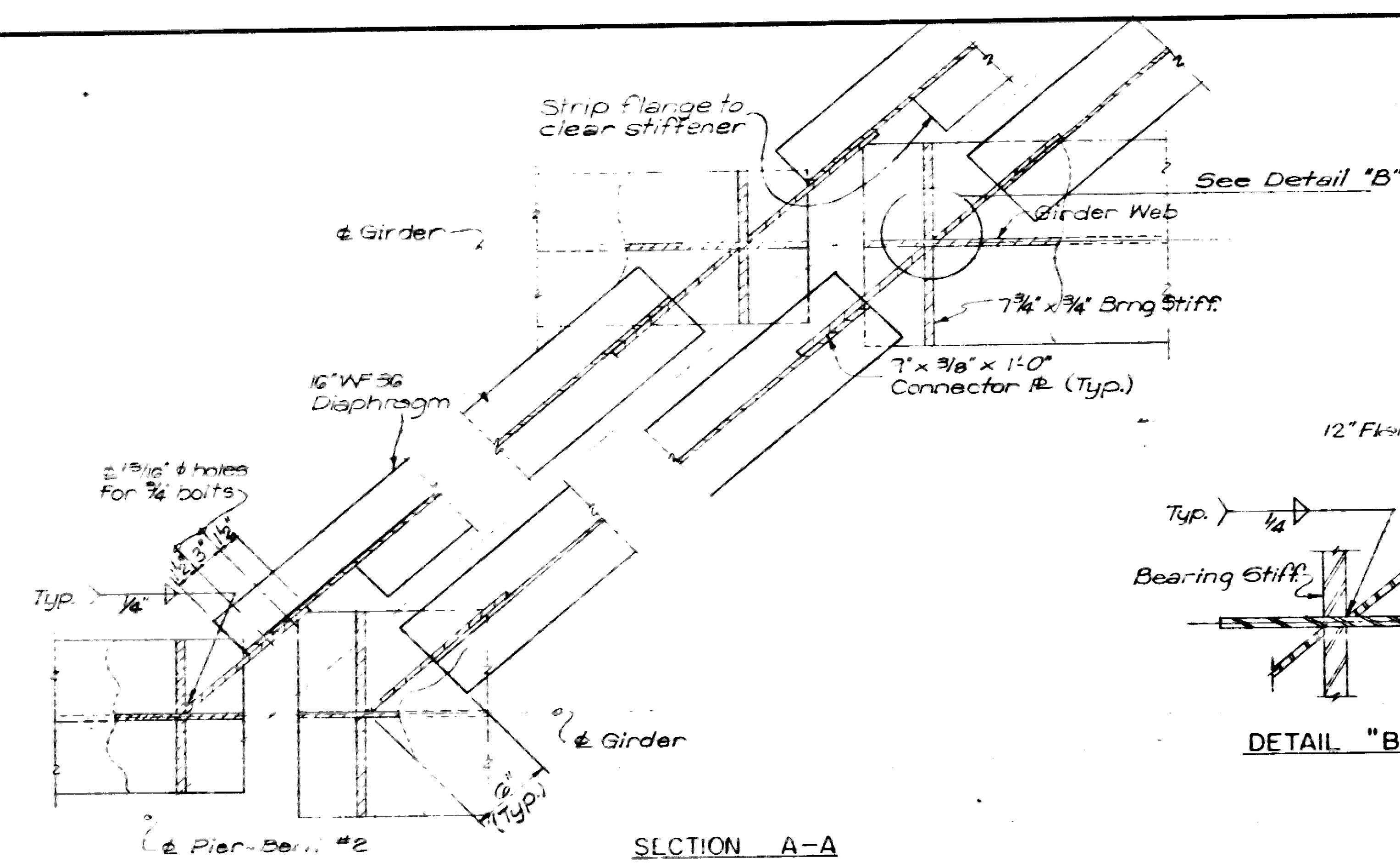
DETAIL OF BENTS NO. 2, 3, 4 & 5
A.S.H. NO. 24 UNDERPASS.
NEVADA COUNTY
ROUTE 30 SEC. 1
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.
DRAWN BY J.R.S. DATE _____
TRACED BY J.M. DATE _____
CHECKED BY D.M. DATE _____
BRIDGE NO. 5061 SCALE AS NOTED
DRAWING NO. 14303







DATE REVISION	DATE	FILED	FED. ROAD DIV. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
			6	ARK.	23-42		34	114
			JOB NO.		5061 EXP. JT. DET. 14307			



EXPANSION JOINT DETAILS AT BENTS 2 & 5
A.S.H. NO. 24 UNDERPASS
NEVADA COUNTY
ROUTE 30 SEC. 1
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.
DRAWN BY L.R.G. DATE
CHECKED BY J.E. DATE
BRIDGE NO. 5061 DRAWING NO. 14307

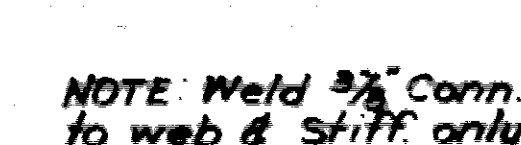
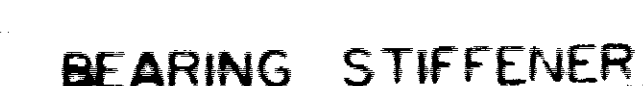


TABLE OF FILLET WELDS	
MAXIMUM THICKNESS OF THICKER PART	FILLET WELD SIZE
Over 1/2" to 3/4"	1/4"
Over 3/4" to 1 1/2"	5/16"
Over 1 1/2" to 2 1/2"	3/8"

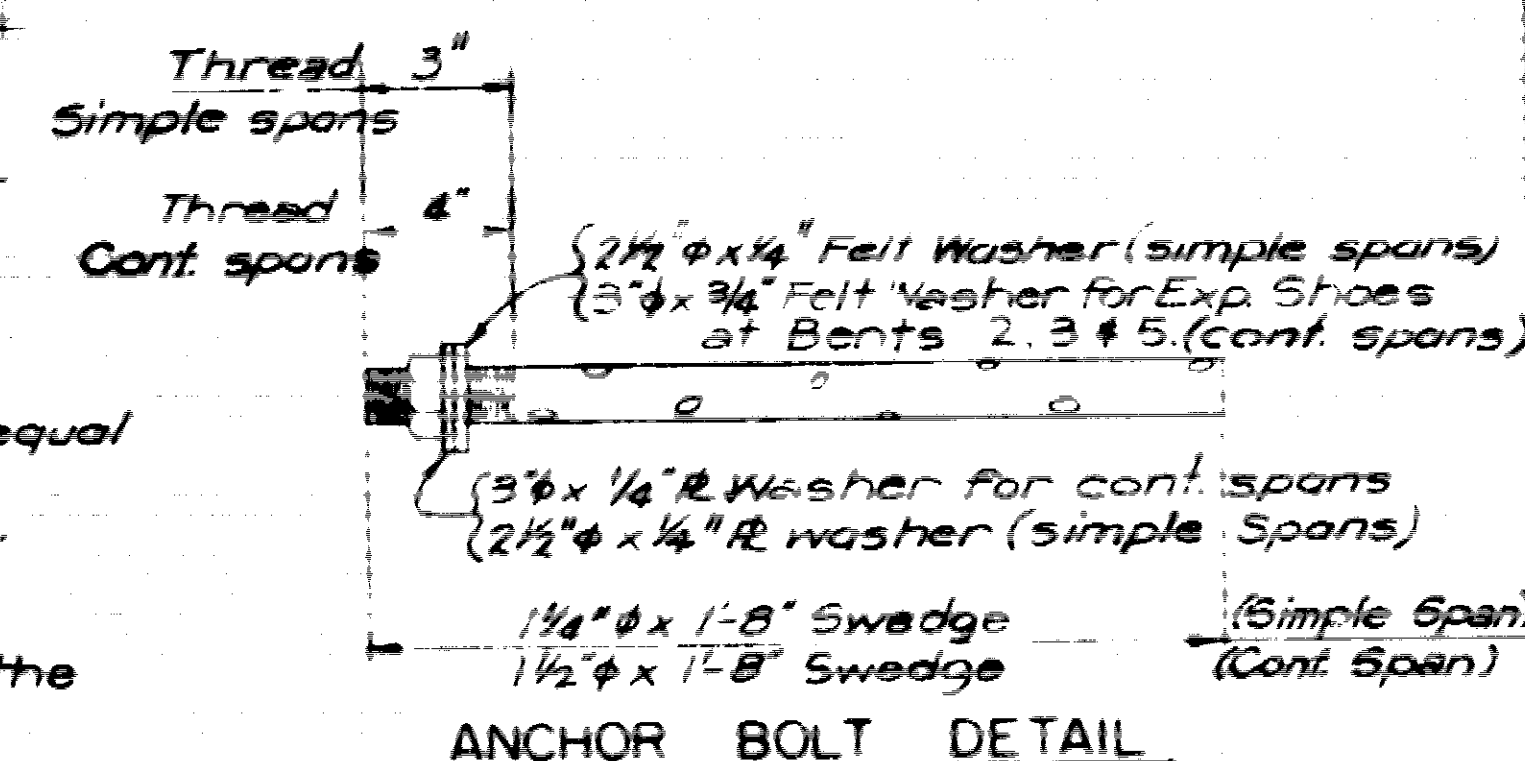


NOTE: Stud, Shear Connectors, Granular Flux Filled, Solid Fluxed, or equal shall be used at the following ratios:

$\frac{3}{4}$ " Diameter Stud spaced as shown on Framing Plan Drawings.

$\frac{1}{8}$ " Diameter Stud in place of $130 - \frac{3}{4}$ " ϕ Stud.

The Studs shall be 4' long and automatically end-welded to the Girder flanges in accordance with the recommendations of the manufacturer.



NOTE: Fillet welds shall be used if Shoes are welded.
See Table of Fillet welds For Size. Shoes
shall then be stress-relieved in accordance with
the provisions of the American Welding Society.
If Shoes are Fabricated from Welded Plates
all shoe material shall be of A.S.T.M. A-36.

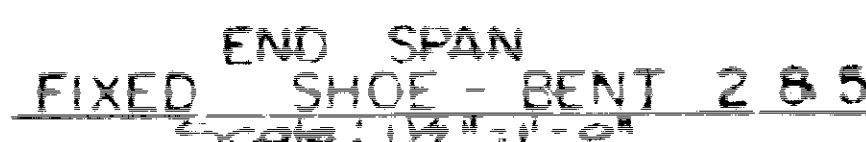
Masonry Plates for Expansion Shoes shall be ASTM A-36
If Shoes are Cast, Casting Material shall be of
ASTM A-27, Grade 25-35.

All pins shall be ASTM A-235, Class E; or ASTM A-108, Grade 1021 to 1030, inclusive, with a minimum Rockwell Scale B hardness of 85.

All shoes and anchor bolts shall be measured and paid for as "Structural Steel in Plate Girder Spans (A-36)".

Anchor Bolts shall be galvanized to conform to
ASTM Specifications A-153.

For General Notes on Structural Steel refer to
Dwg. No. 14309



SHOES, WELDED SPLICES, & X-FRAMING DETAILS

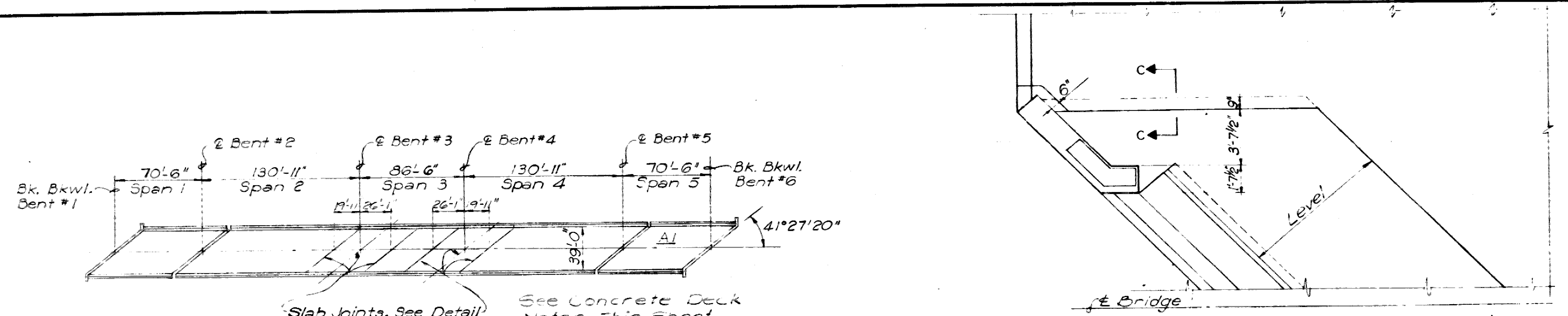
A.S.H. NO. 24 UNDERPASS
NEVADA COUNTY
ROUTE 30 SEC. 1

ARKANSAS STATE HIGHWAY COMMISSION

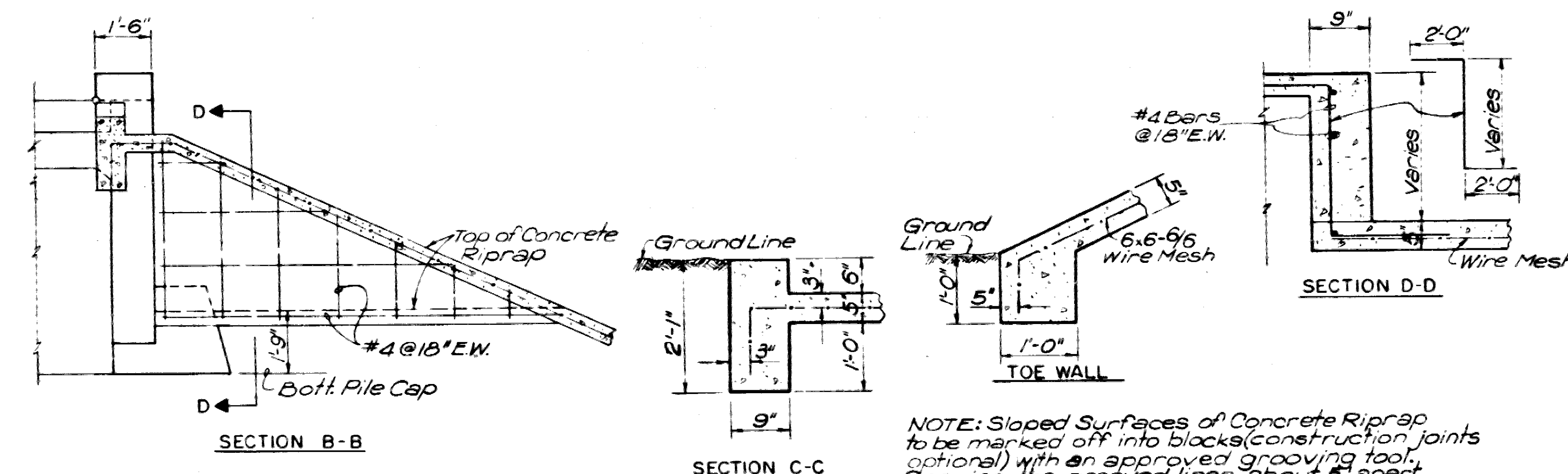
LITTLE ROCK, ARK.

DRAWN BY L. C. B. DATE _____
CHECKED BY J. E. DATE _____
SCALE **AS NOTED**

BRIDGE NO. 5061 **DRAWING NO. 14308**



CONCRETE DECK LAYOUT
Scale: 1" = 60'-0"



DETAIL OF CONCRETE RIPRAP

SHOP DRAWINGS SHALL INCLUDE:

1. METHOD OF ALIGNMENT AND CLAMPING GIRDERS DURING FABRICATION AND ERECTION.
2. PROGRAM FOR WELDING SEQUENCE AND DISTORTION CONTROL DURING FABRICATION AND ERECTION.

ERECTION:

THE OPTIONAL FIELD-SPLICE IN SPAN NO. 3 SHALL BE MADE ON THE GROUND, PRIOR TO ERECTION, WITH THE GIRDER WEBS HORIZONTAL AND BLOCKED ACCORDING TO THE FABRICATOR'S STRUCTURAL STEEL DETAILS.

STEEL MAY BE ERECTED WITHOUT THE USE OF FALSEWORK, PROVIDED ALL SPLICE POINTS ARE WITHIN 1/4" OF THE PLANNED ELEVATION ACCORDING TO THE SHOP DRAWINGS, WITH THE COMPLETE LINE OF GIRDER ERECTED. SPLICE POINTS THAT ARE OFF MORE THAN 1/4" SHALL BE CORRECTED BEFORE ANY WELDING IS STARTED OR ANY BOLTS TIGHTENED.

AFTER THE STRUCTURAL STEEL HAS BEEN ERECTED, A PROFILE OF THE TOP OF THE GIRDERS SHALL BE TAKEN TO DETERMINE THE THICKENING OF HAUNCHES REQUIRED FOR THE DEAD LOAD DEFLECTION.

DATE	REVISION	DATE	REVISION	DATE	REVISION	DATE	REVISION

REV. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	ARK.	(28) 42	96	114	
JOB NO.	5061	GEN. NOTES	14309		

CONCRETE DECK NOTES

ALL STRUCTURAL STEEL SHALL BE IN PLACE FOR A UNIT BEFORE ANY CONCRETE IS PLACED FOR THAT UNIT. A UNIT CONSISTS OF SIMPLE SPAN NO. 1, SIMPLE SPAN NO. 5 OR CONTINUOUS SPANS 2, 3 & 4.

THE CONCRETE DECK SHALL BE PLACED IN ACCORDANCE WITH ARKANSAS STATE HIGHWAY DEPARTMENT SUPPLEMENTAL SPECIFICATIONS.

EACH POUR SHALL BE A CONTINUOUS, UNINTERRUPTED OPERATION WHICH SHALL PLACE ALL THE CONCRETE IN THAT POUR, WHICH SHALL BE FROM EXPANSION JOINT TO EXPANSION JOINT.

MOVEMENT OF THE FINISHING MACHINE ACROSS NEW CONCRETE, WHEN PROTECTED BY MEANS OF PLANKING PLACED ON THE SURFACE, SHALL BE PROHIBITED FOR 72 HOURS AFTER FINISHING THE POUR.

CURB POURS SHALL BE POURED CONTINUOUSLY, SAME AS THE SLAB POURS. CURB POURS SHALL NOT BE MADE UNTIL AT LEAST 72 HOURS HAS ELAPSED AFTER COMPLETION OF ALL THE SLAB POURS.

CURB RISER POURS SHALL NOT BE MADE UNTIL AT LEAST 72 HOURS HAS ELAPSED AFTER COMPLETION OF ALL THE CURB POURS OR MONOLITHIC SLAB AND CURB POURS.

GENERAL NOTES

FIELD CONNECTIONS TO BE BOLTED WITH HIGH-STRENGTH BOLTS ASIM A-325.

HIGH-STRENGTH BOLTS: 3/4" Ø, OPEN HOLES 1 1/16" Ø EXCEPT WHERE NOTED OTHERWISE. BOLT SPACING SHALL BE 2 1/2" UNLESS OTHERWISE NOTED. MINIMUM EDGE DISTANCE SHALL BE 1 1/2" UNLESS OTHERWISE NOTED. BOLTS SHALL BE PLACED WITH HEADS ON THE OUTSIDE FACE OF EXTERIOR GIRDERS AND ON BOTTOM OF GIRDER FLANGES.

STRUCTURAL SHAPES OF EQUAL OR GREATER STRENGTH MAY BE SUBSTITUTED FOR SHAPES SHOWN, BUT PERMIT WILL BE MADE ON THE BASIS OF SHAPES SHOWN OR THOSE ACTUALLY USED, WHICHEVER IS LESS.

ALL WELDING SHALL CONFORM TO THE AMERICAN WELDING SOCIETY STANDARD SPECIFICATIONS FOR WELDING HIGHWAY AND RAILWAY BRIDGES, CURRENT EDITION, AND EXCEPT AS MODIFIED BY THE SPECIAL PROVISIONS. NO WELDS OTHER THAN THOSE REQUIRED BY THE PLANS AND SPECIFICATIONS MAY BE MADE WITHOUT PRIOR APPROVAL BY THE ENGINEER OF SIZE AND LOCATION.

SHOP PAINT: ALL STRUCTURAL STEEL, EXCEPT GALVANIZED MEMBERS, SURFACES IN CONTACT WITH CONCRETE, CONTACT SURFACES OF BOLTED CONNECTIONS, AND SURFACES WITHIN 3" OF HOLES AND FIELD WELDS, SHALL BE GIVEN ONE COAT OF RED LEAD AND RAW LINSEED OIL BEFORE SHIPMENT.

FIELD PAINT: AFTER ERECTION, ALL EXPOSED STEEL SURFACES WHICH DID NOT RECEIVE A COAT OF SHOP PAINT, EXCEPT SURFACES IN CONTACT WITH CONCRETE, SHALL BE GIVEN ONE COAT OF RED LEAD AND RAW LINSEED OIL. TWO ADDITIONAL COATS OF FIELD PAINT SHALL BE APPLIED TO ALL EXPOSED SURFACES. FIRST COAT: RED LEAD TINTED WITH LAMP BLACK; SECOND COAT: ALUMINUM PAINT.

BEARINGS SHALL BE FINALLY SEATED IN ACCORDANCE WITH SEC. 806.54, INCLUDING ALTERNATE, OF THE STANDARD SPECIFICATIONS. THIS WORK AND MATERIAL ARE TO BE CONSIDERED AS SUBSIDIARY TO THE ITEM OF "STRUCTURAL STEEL" AND WILL NOT BE PAID FOR DIRECTLY.

THESE DRAWINGS SHOW GENERAL FEATURES OF DESIGN ONLY. SHOP DRAWINGS SHALL BE MADE IN ACCORDANCE WITH THE SPECIFICATIONS, SUBMITTED, AND APPROVAL SECURED BEFORE FABRICATION IS BEGUN.

ANCHOR BOLTS SHALL BE GALVANIZED TO CONFORM TO ASTM SPECIFICATION, DESIGNATION A-153.

REINFORCING STEEL TO BE DEFORMED BARS OF INTERMEDIATE OR HARD GRADE. THE REINFORCING STEEL IS TO BE ACCURATELY LOCATED IN THE FORMS AND FIRMLY HELD IN PLACE BY STEEL WIRE SUPPORTS, SUFFICIENT IN NUMBER AND SIZE TO PREVENT DISPLACEMENT DURING THE COURSE OF CONSTRUCTION. THE WIRE SUPPORTS WILL NOT BE PAID FOR DIRECTLY BUT WILL BE CONSIDERED SUBSIDIARY TO THE ITEM OF "REINFORCING STEEL".

SHOP LISTS AND BENDING DIAGRAMS OF REINFORCING STEEL, INCLUDING WIRE SUPPORTS, SHALL BE SUBMITTED AND APPROVAL SECURED BEFORE FABRICATION IS BEGUN.

ALL CHAMFERS ON CONCRETE RISERS ARE TO BE 1/4".

SHOP DRAWINGS FOR BRIDGE RAILING SHALL BE SUBMITTED AND APPROVED BEFORE FABRICATION IS BEGUN.

FABRICATION:

EXTENSION BARS SHALL BE USED IN MAKING BUTT WELDS IN THE FLANGES ACCORDING TO THE A.W.S. SPECIFICATION SECTION 505(C-2).

ENDS OF GIRDERS TO BE SPliced SHALL BE PREPARED IN THE SHOP, TAKING INTO ACCOUNT THE RELATIVE POSITIONS OF ADJACENT SECTIONS AND THE HIGHWAY GRADE AND ALIGNMENT.

ALL GIRDERS SHALL BE SHOP-ASSEMBLED WITH GIRDER WEBS HORIZONTAL, FIELD CONNECTIONS REVEALED (FOR ALTERNATE SPLICE) AND ALL PARTS MATCHED. THE SHOP ASSEMBLY SHALL HAVE A MINIMUM OF 3 SECTIONS. A BLOCKING DIAGRAM OF EACH GIRDER WITH MATCH LINES INDICATED SHALL BE A PART OF THE STRUCTURAL STEEL DETAILS.

ALL GIRDER WEBS SHALL BE CAMBERED SUCH THAT, UNDER TOTAL DEAD LOAD, THE TOP OF THE GIRDER WEBS WILL PARALLEL THE FINISH HIGHWAY GRADE WITH ALLOWABLE DEVIATION OF 1/4".

CONCRETE RIP RAP & GENERAL NOTES
A.S.H. NO. 24 UNDERPASS

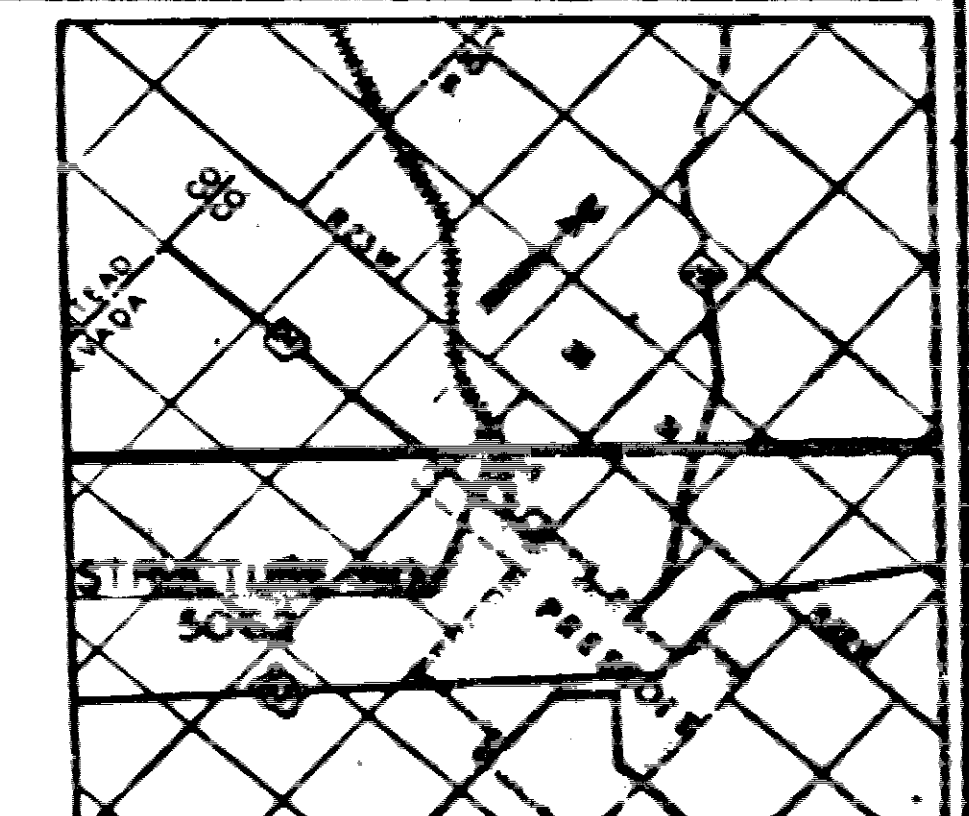
NEVADA COUNTY
ROUTE 30 SEC. 1
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

DRAWN BY LRS DATE _____
CHECKED BY MAC DATE _____
BRIDGE ENGINEER SCALE AS NOTED
BRIDGE NO. 5061 DRAWING NO. 14309

288

5062 LAYOUT 14310

DATE	REVISION	BY	NO.	DATE	REVISION	BY	NO.	TOTAL SHEETS
								57
								114



KEY PLAN

GENERAL NOTES

ALL CONCRETE TO BE PLACED IN THE DRY. EXPOSED CORNERS TO BE CHAMFERED 3/4" UNLESS OTHERWISE NOTED.

IN GENERAL, ALL CONSTRUCTION JOINTS IN ABUTMENTS AND PIERS SHALL BE HORIZONTAL AND SHALL BE PROVIDED WITH KEYS NOT LESS THAN 4" HIGH.

ALL PILING SHALL BE 16" OCTAGONAL PRECAST CONCRETE OR 16" CONCRETE FILLED METAL SHELLS AND 30' L. BE DRIVEN WITH AN APPROVED AIR, STEAM, OR DIESEL HAMMER TO A MINIMUM BEARING CAPACITY OF 40 TONS PER PILE WITH A MINIMUM LENGTH OF 15'. LIMITS OF PILING SHOWN ARE ASSUMED FOR ESTIMATING QUANTITIES ONLY. ACTUAL LENGTHS TO BE DETERMINED IN THE FIELD. HAVE THE 30' TEST PILE IN BENT NO. 2.

PILES IN END BENTS SHALL BE DRIVEN, AFTER SETTLEMENT IS IN PLACE, TO SUBGRADE ELEVATION.

SPECIFICATIONS: ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 1939, AND 1960 SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS - AMERICAN RAILWAY ENGINEERING ASSOCIATION, MANUAL OF RECOMMENDED PRACTICE, 1967 EDITION.

UNIT STRESSES: CLASS A CONCRETE (A-15) 3000 PSI
CLASS B CONCRETE (A-10) 2500 PSI
REINFORCING STEEL (A-36) 20,000 PSI
STRUCTURAL STEEL (A-36) 20,000 PSI

SPECIAL NOTE: FIELD INSPECTION WILL NOT REQUIRE TYPING ACCEPTANCE OR REJECTION WILL BE MADE BY VISUAL INSPECTION BY THE RESIDENT ENGINEER.

REFERENCE LIST OF DRAWINGS

FOR DETAILS OF BENT NOS. 1 & 6, SEE DWG. NO. 14310.
FOR DETAILS OF BENT NOS. 2, 3, 4 & 5, SEE DWG. NO. 14310.
FOR FRAMING PLAN & STRUCTURAL DETAILS, SEE DWG. NO. 14310.
FOR DETAILS OF GIRDERS & FRAMING, SEE DWG. NO. 14310.
FOR WATERPROOFING DETAILS, SEE DWG. NO. 14310.
FOR DETAILS OF SHOES, SEE DWG. NO. 14310.
FOR DETAILS OF PILING, SEE DWG. NO. 2382.
FOR CONCRETE RIP RAP DETAILS SEE STD. DWG. NO. 1001A.

SOIL CLASSIFICATION

BR.	- BROWN	MULTICO.	- MULTICOLORED
CL.	- CLAY	PLST.	- PLASTIC
CLY.	- CLAYEY	SD.	- SAND
DK.	- DARK	SDY.	- SANDY
FR.	- FIRM	SD LNS.	- SAND LENSES
FRAG.	- FRAGMENTS	SGTST.	- SANDSTONE
GR.	- GRAVEL	SGT.	- SILTY
GRV.	- GRAVELS	TH.	- TAN
HD.	- HARD	YLLW.	- YELLOW
LMSN.	- LIMESTONE	MTR. BRG.	- WATER BEARING
MST.	- MOIST		

LAYOUT OF UNDERPASS

E 1-30 & PRESCOTT-NORTHWESTERN RR
NEVADA COUNTY
ROUTE 30 SEC. 1

ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

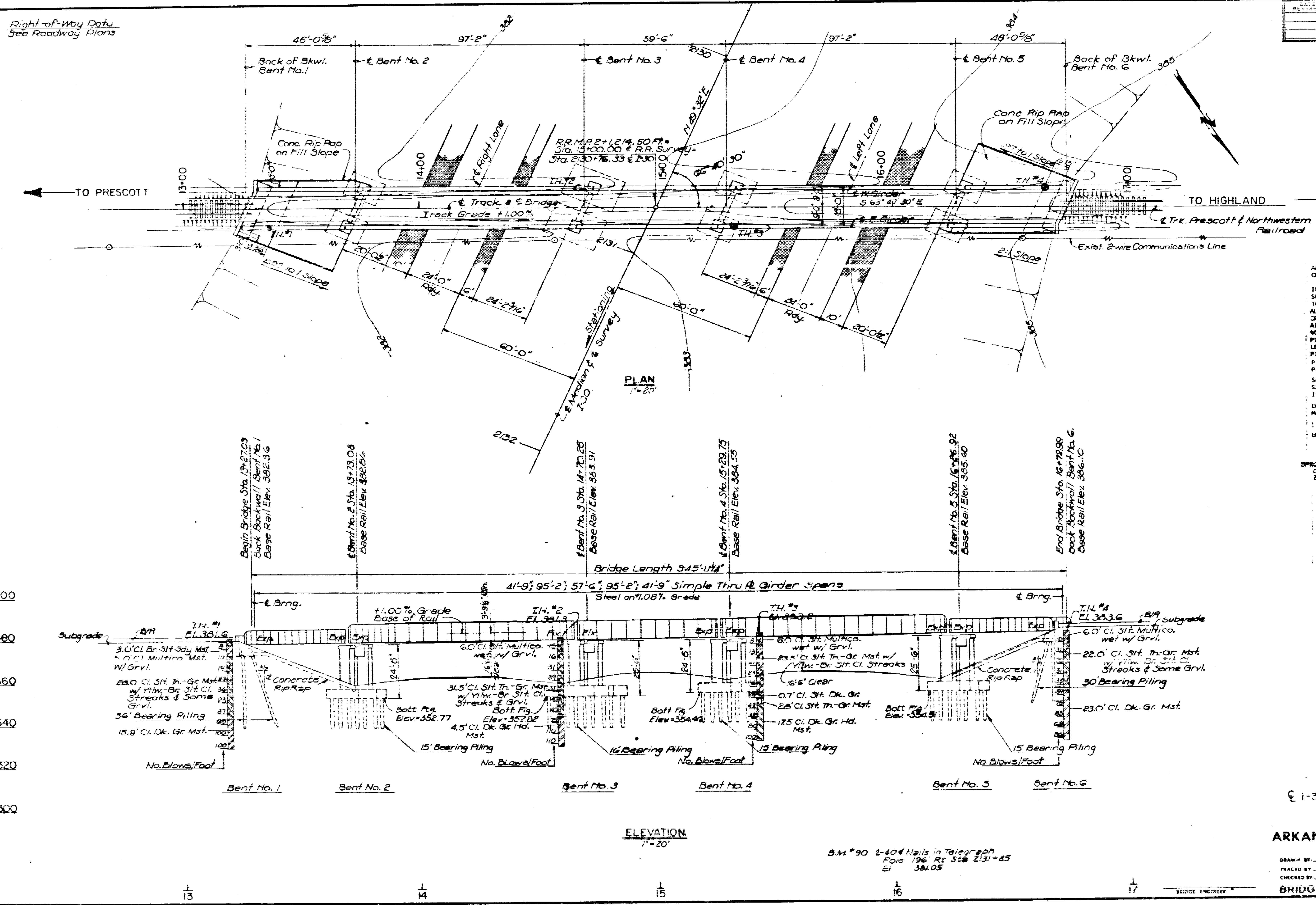
DRAWN BY LDM DATE SCALE 1"=20' 0"

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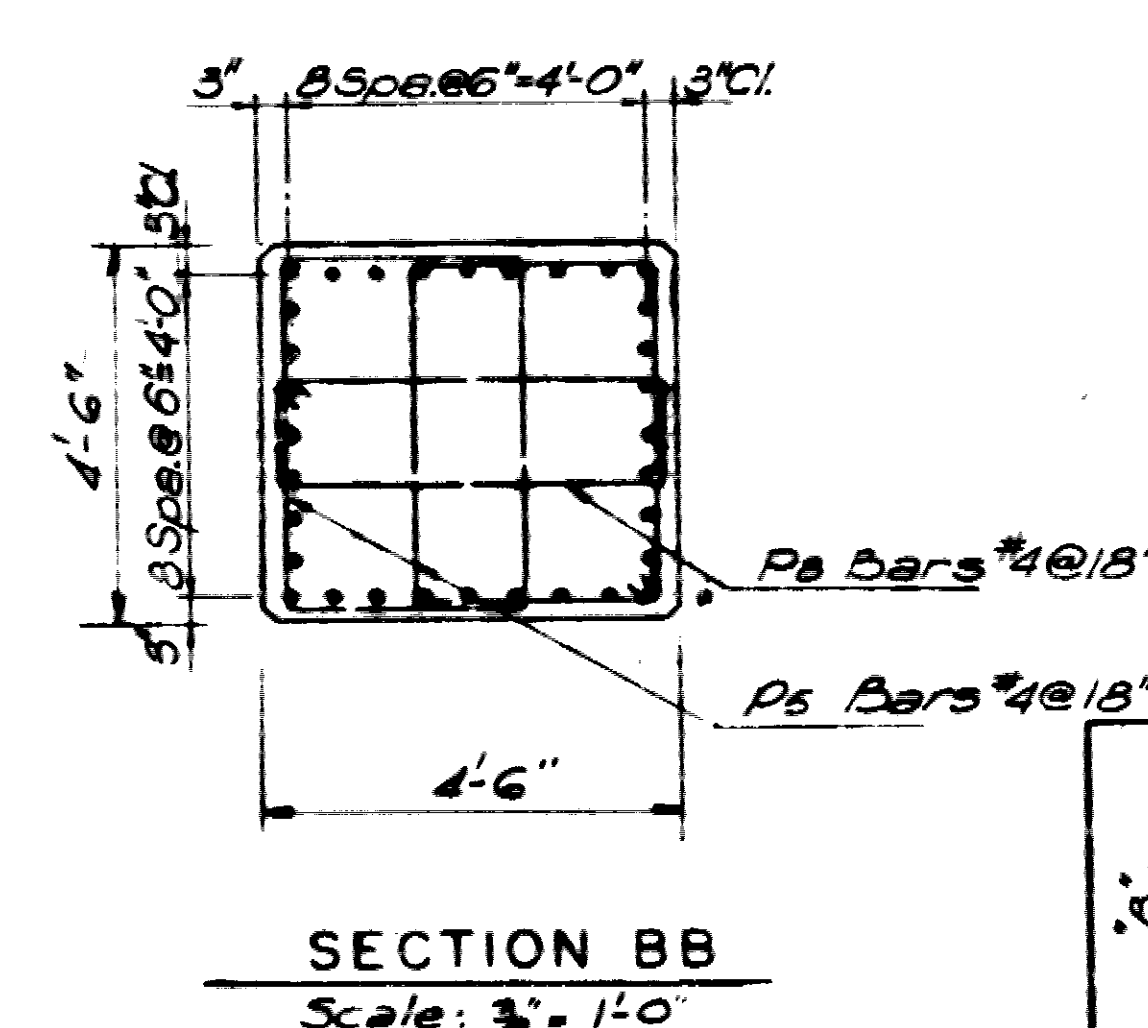
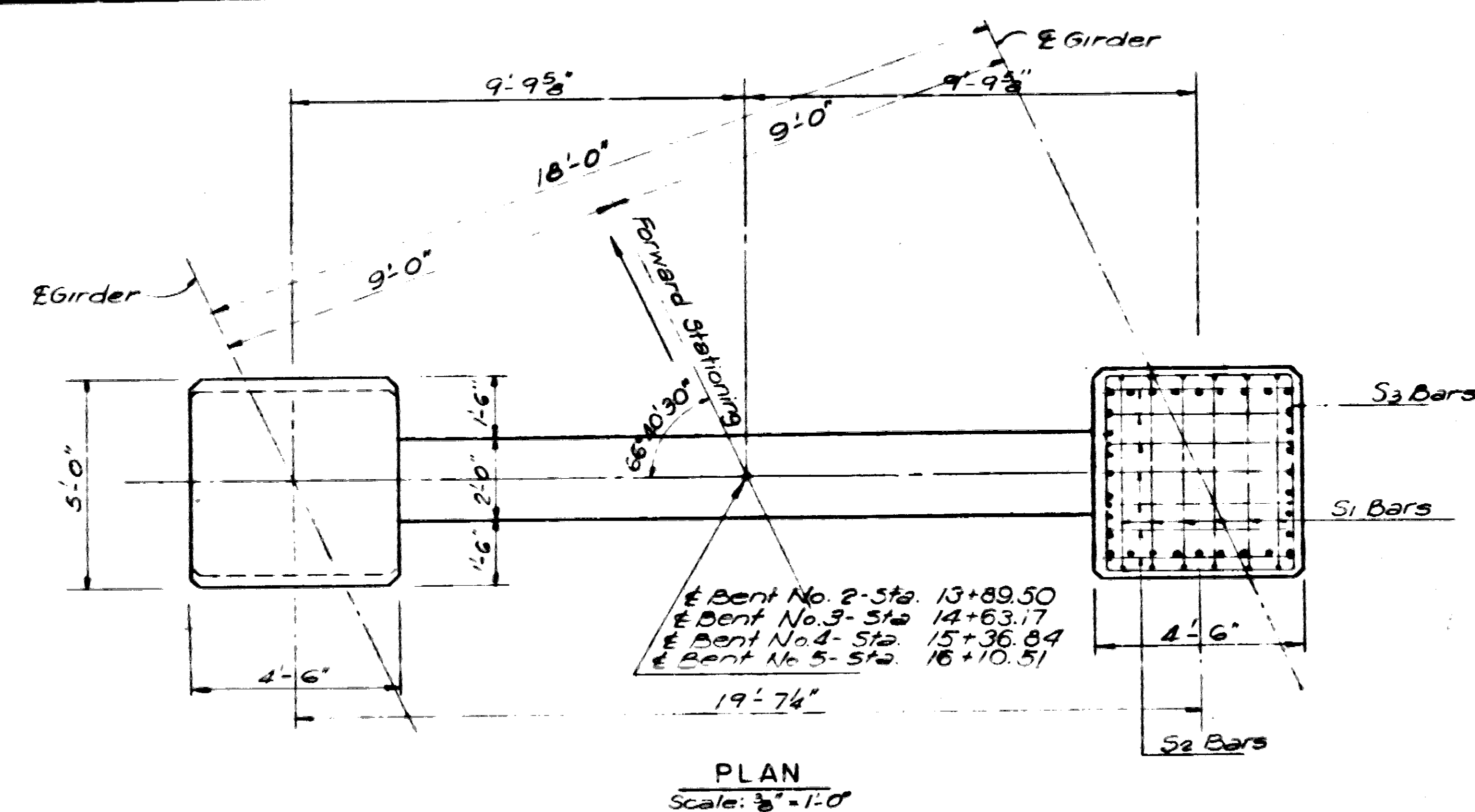
BRIDGE NO. 5062 DRAWING NO. 14310

Right-of-Way Data
See Roadway Plans



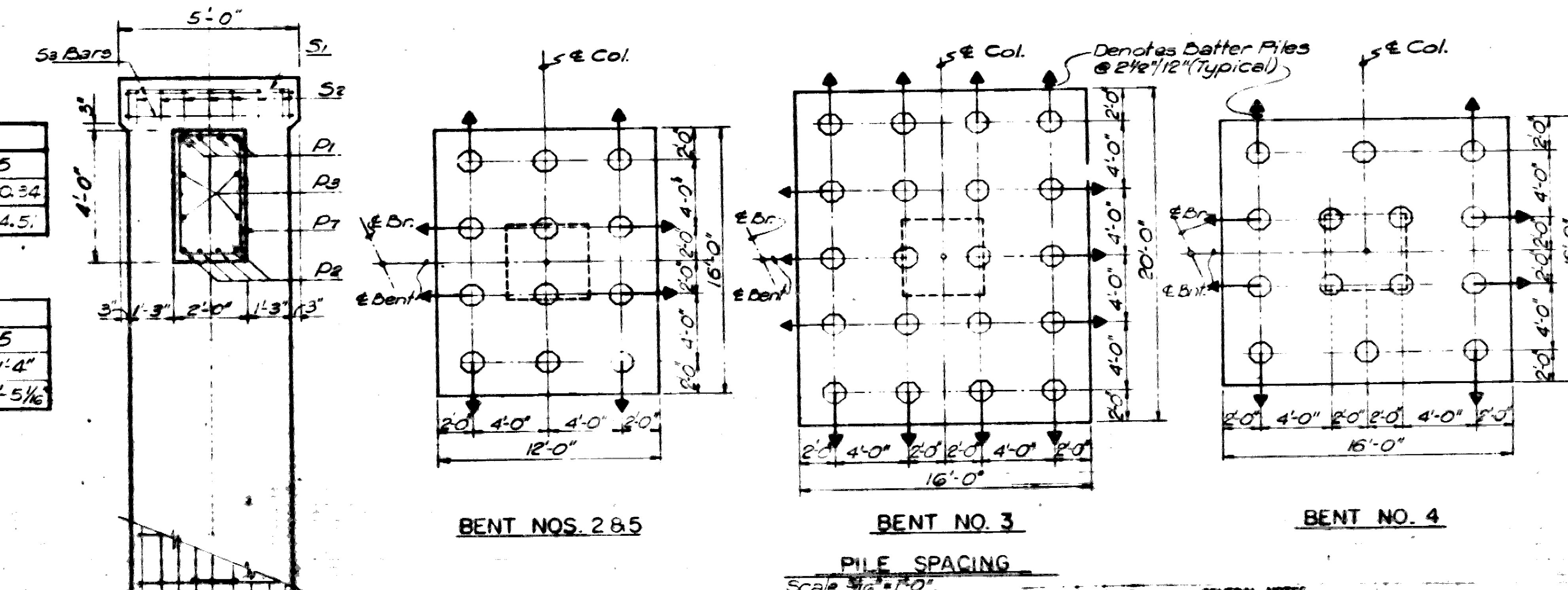
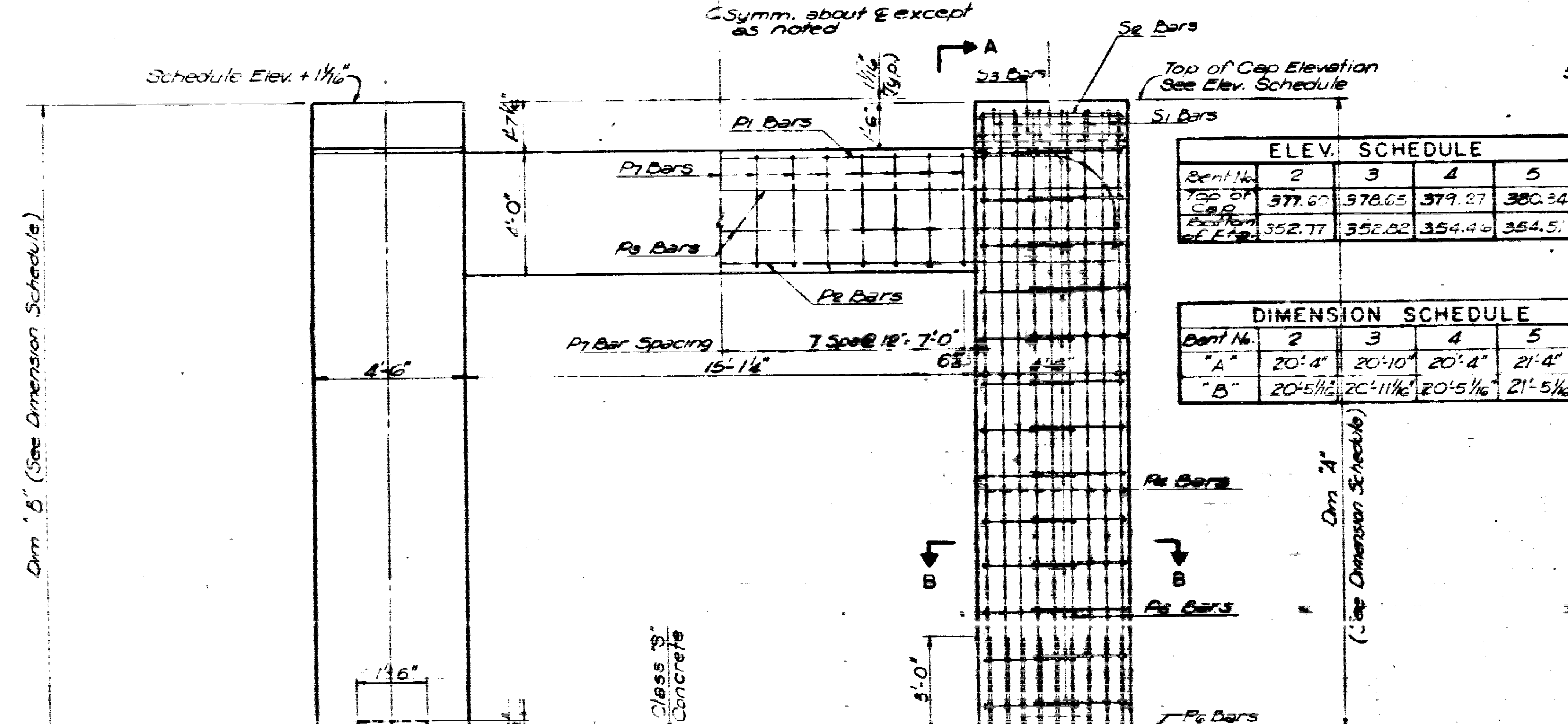
① 5062 DET. OF INT. BENTS 14312

DATE REVISED	BY	DATE	PROJECT NO.	STATE	PROJ. NO.	YEAR	SHEET NO.	TOTAL SHEETS
			6	ARK	23	42	39	114
			JOB NO.		3545			

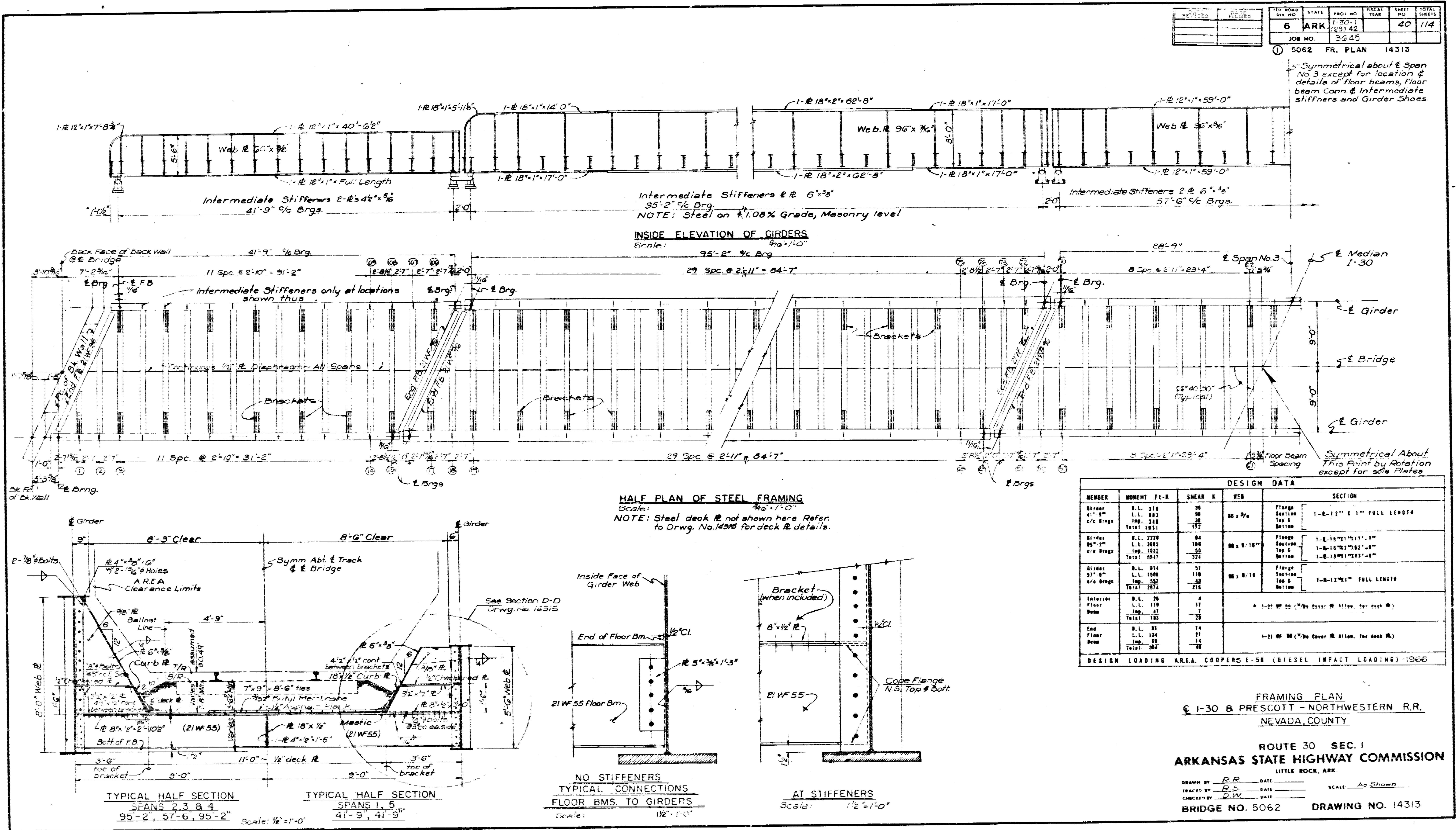


		BAR LIST															
		BENT NO. 2				BENT NO. 3				BENT NO. 4				BENT NO. 5			
MARK	SIZE	NO.	LENGTH	A"	B"	NO.	LENGTH	A"	B"	NO.	LENGTH	A"	B"	NO.	LENGTH	A"	B"
D1	9	4	28'-10"	23'-7"	3'-6"	4	28'-10"	23'-7"	3'-6"	4	28'-10"	23'-7"	3'-6"	4	28'-10"	23'-7"	3'-6"
P1	9	4	23'-9"	-	-	4	23'-9"	-	-	4	23'-9"	-	-	4	23'-9"	-	-
P2	5	4	23'-9"	-	-	4	23'-9"	-	-	4	23'-9"	-	-	4	23'-9"	-	-
P3	6	4	20'-0"	-	-	4	20'-0"	-	-	4	20'-0"	-	-	4	20'-0"	-	-
P4	5	52	14'-3 1/2"	2'-7 1/2"	4'-7 1/2"	52	14'-3 1/2"	2'-7 1/2"	4'-7 1/2"	52	14'-3 1/2"	2'-7 1/2"	4'-7 1/2"	52	14'-3 1/2"	2'-7 1/2"	4'-7 1/2"
P5	9	64	6'-3"	-	-	64	6'-3"	-	-	64	6'-3"	-	-	64	6'-3"	-	-
P6	4	15	10'-11"	1'-7"	3'-7"	15	10'-11"	1'-7"	3'-7"	15	10'-11"	1'-7"	3'-7"	15	10'-11"	1'-7"	3'-7"
P7	4	38	12'-10"	1'-5"	0'-6"	38	12'-10"	1'-5"	0'-6"	38	12'-10"	1'-5"	0'-6"	38	12'-10"	1'-5"	0'-6"
P8	6	40	16'-10"	15'-5"	0'-6"	40	16'-10"	15'-5"	0'-6"	40	16'-10"	15'-5"	0'-6"	40	16'-10"	15'-5"	0'-6"
P9	8	5	21'-8"	9'-5"	9"	5	21'-8"	9'-5"	9"	5	21'-8"	9'-5"	9"	5	21'-8"	9'-5"	9"
P10	5	26	11'-3 1/2"	1'-5"	4'-7 1/2"	26	11'-3 1/2"	1'-5"	4'-7 1/2"	26	11'-3 1/2"	1'-5"	4'-7 1/2"	26	11'-3 1/2"	1'-5"	4'-7 1/2"
S1	4	12	7'-0"	4'-6"	1'-3"	12	7'-0"	4'-6"	1'-3"	12	7'-0"	4'-6"	1'-3"	12	7'-0"	4'-6"	1'-3"
S2	4	14	6'-6"	4'-0"	1'-3"	14	6'-6"	4'-0"	1'-3"	14	6'-6"	4'-0"	1'-3"	14	6'-6"	4'-0"	1'-3"
S3	4	4	8'-1 1/2"	4'-3 1/2"	4'-7 1/2"	4	8'-1 1/2"	4'-3 1/2"	4'-7 1/2"	4	8'-1 1/2"	4'-3 1/2"	4'-7 1/2"	4	8'-1 1/2"	4'-3 1/2"	4'-7 1/2"

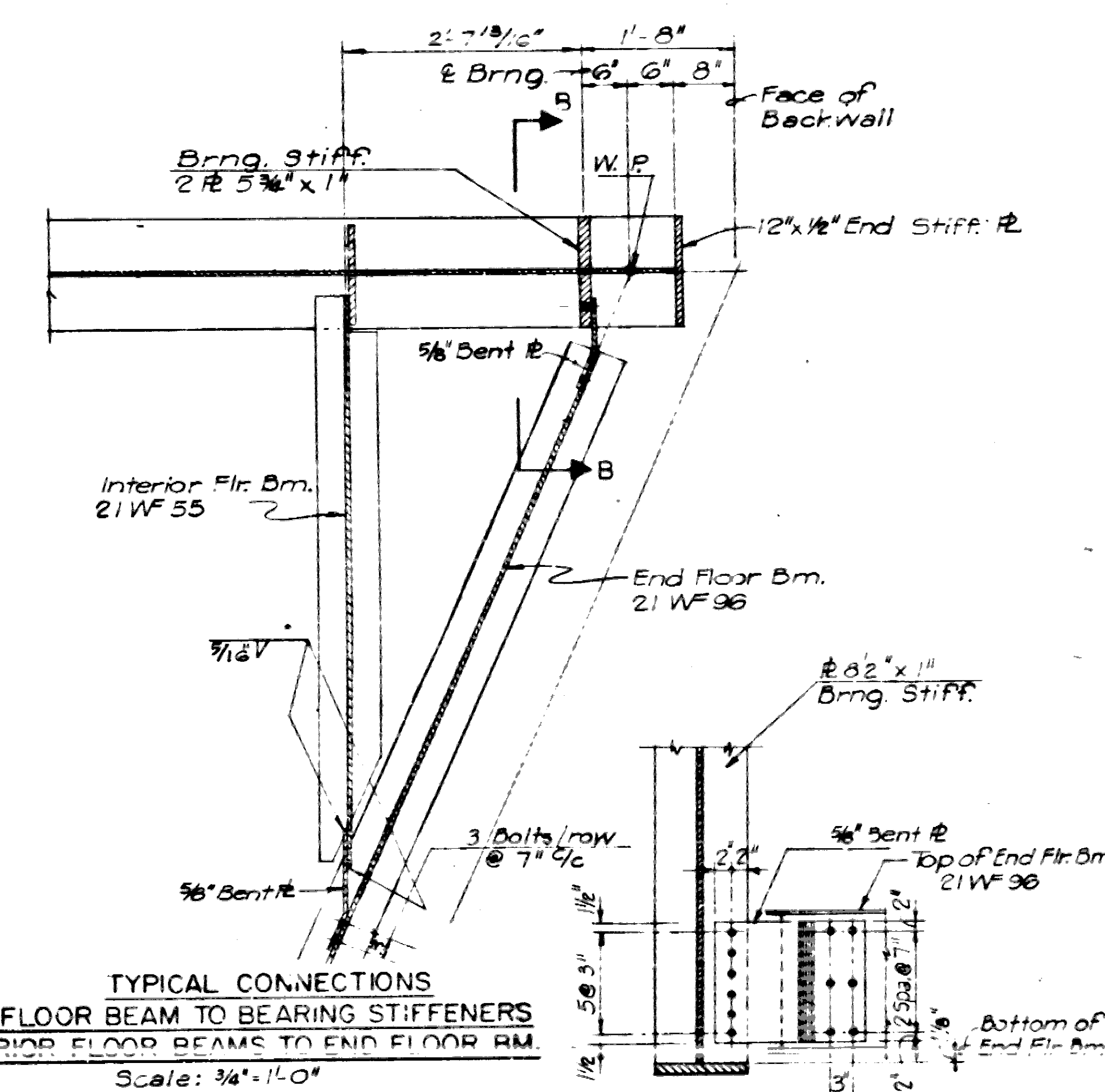
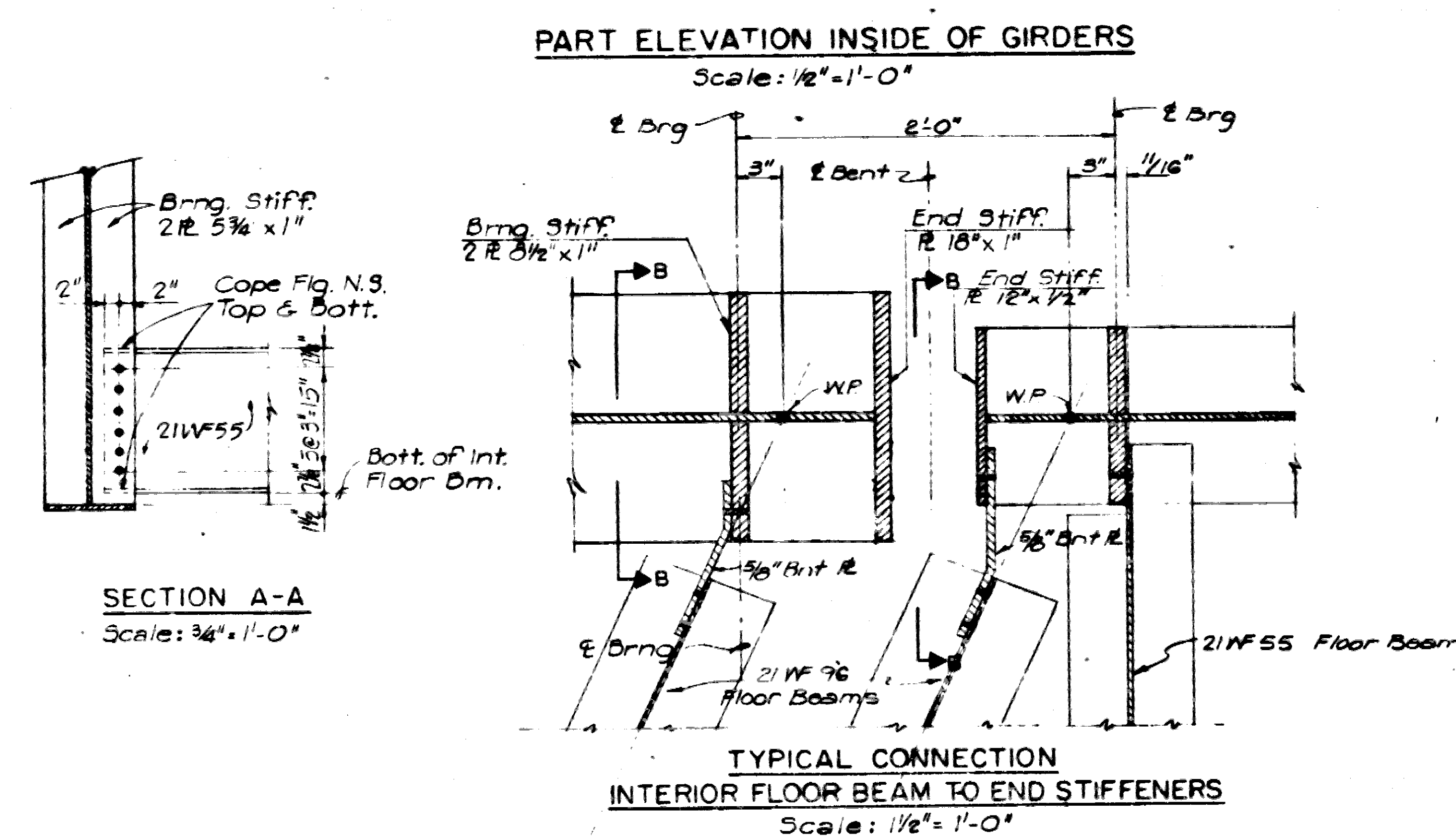
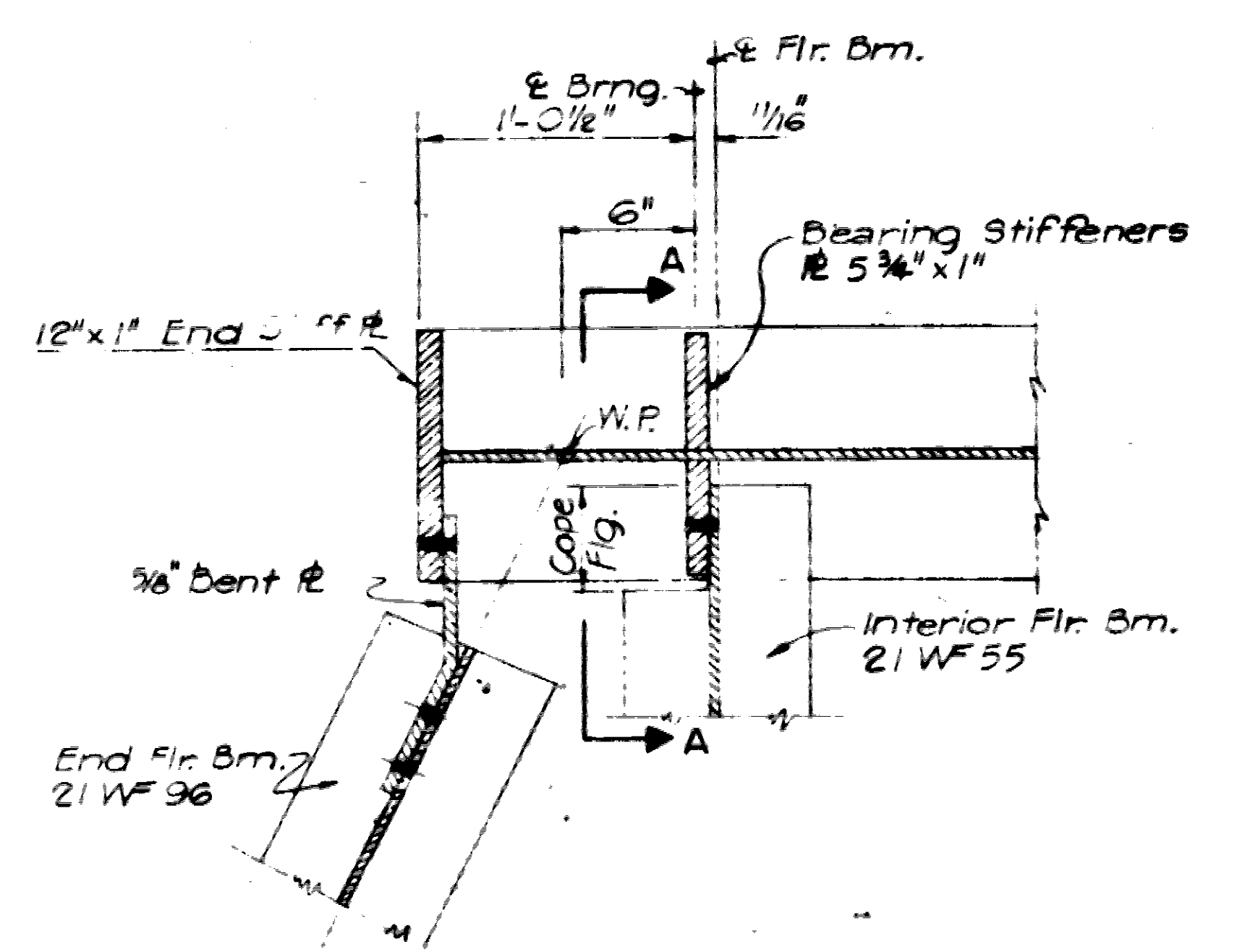
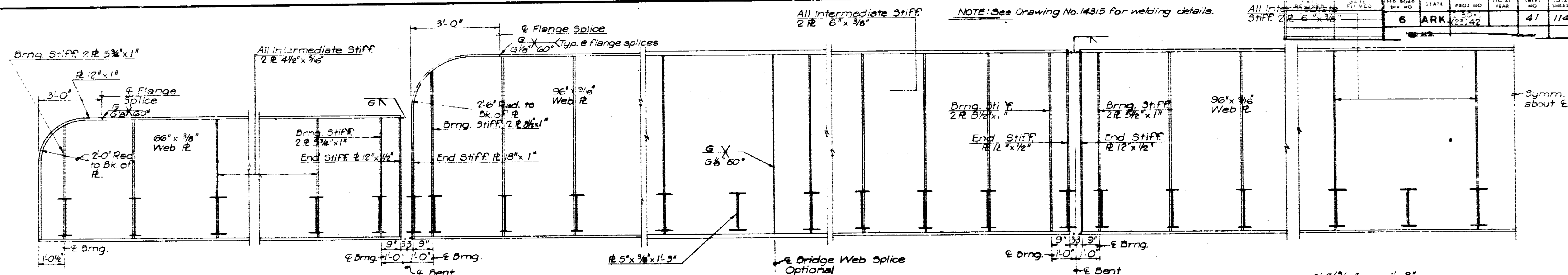
Bar Dim are measured center to center



- GENERAL NOTES
- CONCRETE IN FOOTINGS TO BE CLASS "A", AND CONCRETE IN COLLARS AND DEATH TO BE CLASS "B". ALL CASTED CORNERS TO BE CHAMFERED 3/4" UNLESS OTHERWISE NOTED.
 - REINFORCING STEEL SHALL BE DEFORMED BARS OF INTERMEDIATE OR HIGHER GRADE. SHAP, LIST AND SPACING DIMENSIONS MUST BE SUBMITTED FOR APPROVAL BEFORE FABRICATION IS BEGUN. STEEL WIRE SUPPORTS USED TO SUPPORT AND SPACE THE BARS SHALL NOT BE PAID FOR DIRECTLY BUT WILL BE CONSIDERED SUBSIDIARY TO THE ITEM "REINFORCING STEEL".



DATE	REV.	BY	CHKD.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
				6	ARK	22-42	41	114

SECTION B-B
Scale: 3/4" = 1'-0"

GENERAL NOTES FOR STRUCTURAL STEEL

- GENERAL DATA**
- DESIGN SHALL CONFORM TO REQUIREMENTS OF THE A.R.E.A. SPECIFICATIONS FOR STEEL RAILWAY BRIDGES - 1967 EDITION.
 - CONSTRUCTION SHALL CONFORM TO THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION OF THE ARKANSAS STATE HIGHWAY COMMISSION - EDITION OF 1959.
- MATERIALS**
- MATERIALS SHALL CONFORM TO THE REQUIREMENTS OF THE CURRENT STANDARDS OF THE A.S.T.M. AND TO THE 1959 EDITION OF ARKANSAS HIGHWAY COMMISSION SPECIFICATIONS.
 - ALL STEEL SHALL CONFORM TO THE REQUIREMENTS AS STATED ON THE DRAWINGS FOR THE PARTICULAR ITEMS INVOLVED.
 - IN GENERAL, STRUCTURAL STEEL SHALL CONFORM TO REQUIREMENTS OF A.S.T.M. A-36.
 - ALL SHIP MATERIAL SHALL CONFORM TO A.S.T.M. A-80.
 - ALL PINS SHALL CONFORM TO A.S.T.M. A-235, CLASS E, OR TO A.S.T.M. A-108, GRADES 1020 TO 1010, INCLUSIVE, WITH A MINIMUM ROCKWELL SCALE B HARDNESS OF 85.
 - ALL HIGH-STRENGTH BOLTS SHALL CONFORM TO A.S.T.M. A-325.
 - WELDING MATERIALS SHALL CONFORM TO REQUIREMENTS OF A.W.S.
 - ANCHOR-BOLT GALVANIZING SHALL CONFORM TO A.S.T.M. A-153.
- FABRICATION**
- IN GENERAL, FABRICATION DETAILS AND WORKMANSHIP SHALL CONFORM TO THE REQUIREMENTS OF THE A.R.E.A. SPECIFICATIONS, EDITION OF 1967, WHICH SPECS. SHALL GOVERN IN THE CASE OF ANY CONFLICT WITH REQUIREMENTS OF THE ARKANSAS HIGHWAY COMMISSION SPECS. IN MATTERS OF FABRICATION.
 - SHOP FABRICATION DETAILS SHALL CONFORM IN ALL RESPECTS WITH THE DETAILS OF THESE PLANS. SHOP DRAWINGS SHALL BE SUBMITTED TO THE HIGHWAY COMMISSION AND APPROVED BY THE HIGHWAY COMMISSION BEFORE ANY FABRICATION IS BEGUN. SHOP DRAWINGS SHALL INCLUDE THE PROVIDED METHOD OF HOLDING DECK PLATES AGAINST FLOOR BEAMS DURING WELDING.
 - ALL SHOP WELDING SHALL CONFORM TO ARKANSAS STANDARD SPECIFICATIONS SEC. 806.24.

ERECTOR

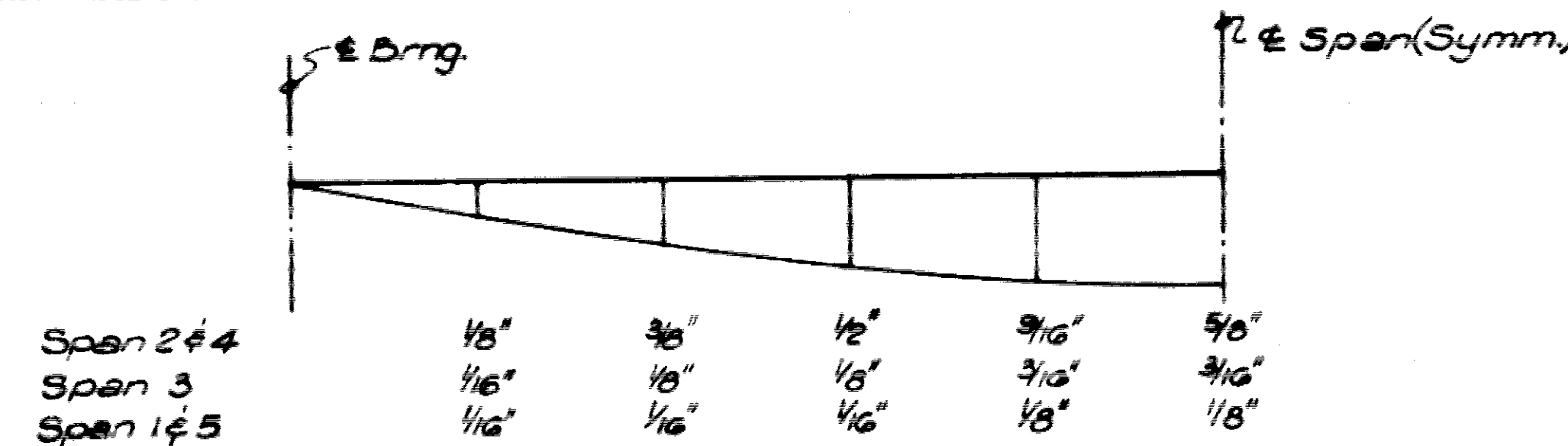
- THE CONTRACTOR SHALL SUBMIT AN OUTLINE AND DRAWINGS ADEQUATELY DESCRIBING HIS PROPOSED METHOD AND SEQUENCE OF ERECTION FOR ALL STRUCTURAL STEEL. THE PROPOSED ERECTION PROCEDURE SHALL BE SUBJECT TO APPROVAL BY THE HIGHWAY COMMISSION.
- ALL FIELD CONNECTIONS SHALL BE MADE WITH 7/8" H HIGH-STRENGTH BOLTS IN ACCORDANCE WITH THE REQUIREMENTS AND PROCEDURES OF SEC. 806.21(4) OF THE ARKANSAS STANDARD SPECIFICATIONS. BOLTS SHALL BE PLACED WITH HEADS ON THE OUTSIDE FACE OF MEMBERS OR GIRDERS AND ON BOTTOM OF GIRDER BOTTOM FLANGES.
- ALL FIELD WELDING SHALL CONFORM TO ARKANSAS STANDARD SPECIFICATIONS SEC. 806.24.

PAINTING

- PAINTING METHODS AND PROCEDURES SHALL CONFORM TO THE REQUIREMENTS OF THE ARKANSAS STANDARD SPECIFICATIONS, 1959.
- SHOP PAINT: ALL STRUCTURAL STEEL, EXCEPT GALVANIZED MEMBERS, SURFACES IN CONTACT WITH CONCRETE, CONTACT SURFACES OF BOLTED CONNECTIONS AND SURFACES WITHIN 3 INCHES OF HOLES AND FIELD WELDS, SHALL BE GIVEN A SHOP COAT OF PAINT BEFORE SHIPMENT.
- AFTER COMPLETION OF ERECTION, ALL ABRASIONS OF THE SHOP COAT AND ALL WELDS, BOLTS AND LOCATION MARKS SHALL BE THOROUGHLY CLEANED AND GIVEN ONE COAT OF SHOP PAINT IN THE FIELD BEFORE THE APPLICATION OF FIELD COATS. THE TOP SURFACES OF DECK PLATES AND BALLAST CURB PLATES SHALL BE THOROUGHLY CLEANED AND GIVEN ONE COAT OF NON-OIL BASE PRIMER IN ACCORDANCE WITH ASTM DESIGNATION D41-41.
- FIELD PAINT: AFTER ERECTION, ALL EXPOSED STEEL SURFACES WHICH DID NOT RECEIVE A COAT OF SHOP PAINT, EXCEPT SURFACES IN CONTACT WITH CONCRETE, SHALL BE GIVEN ONE COAT OF SHOP PAINT. ALL STEEL SURFACES, EXCEPT DECK PLATE SURFACES, SHALL RECEIVE TWO COATS OF FIELD PAINT AS SPECIFIED IN SEC. 806.2(1).

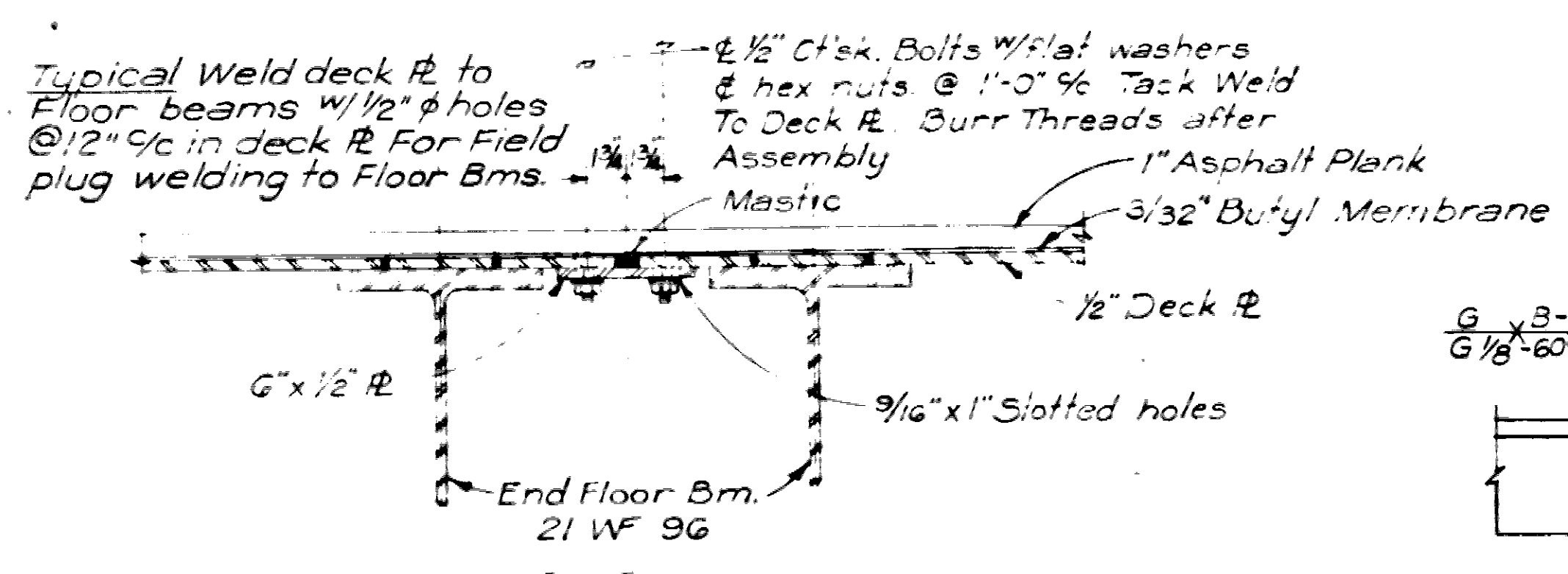
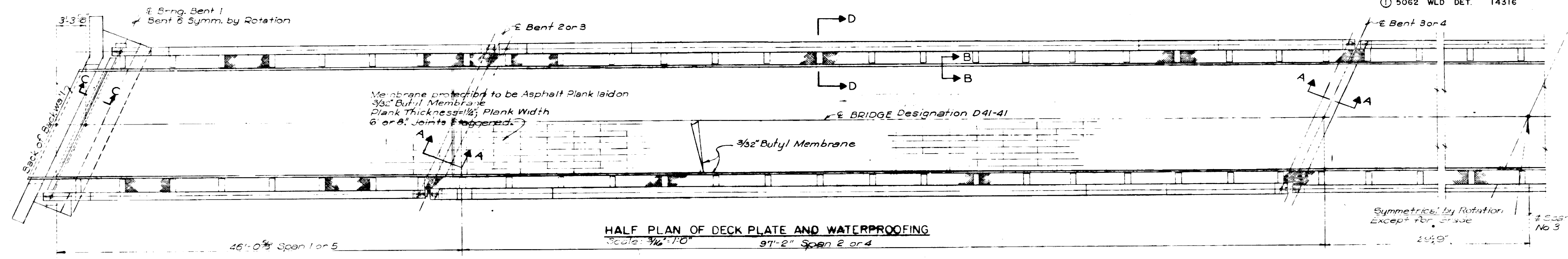
PAYMENT

- METHOD OF MEASUREMENT FOR BASIS OF PAYMENT FOR STRUCTURAL STEEL SHALL BE IN ACCORDANCE WITH SEC. 806.73 OF THE ARKANSAS STANDARD SPECIFICATIONS, 1959, UNLESS OTHERWISE STATED.
- ALL COSTS OF BOLTING OPERATIONS SHALL BE INCLUDED IN THE PRICE BID PER POUND FOR STRUCTURAL STEEL.
- PAYMENT FOR HIGH-STRENGTH BOLTS, MACHINE BOLTS, WASHERS, NUTS, CLIPS, BEARING PLATES, INCLUDING LEAD PLATES AND ANCHOR BOLTS, WILL BE MADE BY UNIT PRICE BID PER POUND OF STRUCTURAL STEEL. PAYMENT FOR SHOP AND FIELD WELDING WILL BE BY THE POUND OF DEPOSITED WELD METAL AT THE UNIT PRICE BID PER POUND FOR STRUCTURAL STEEL. MEASUREMENT WILL BE MADE ACCORDING TO SEC. 806.73(c).
- ALL COSTS OF PAINTING AND OTHER COMPOUNDS AND COATINGS SHALL BE INCLUDED IN THE UNIT PRICE BID PER POUND FOR STRUCTURAL STEEL.



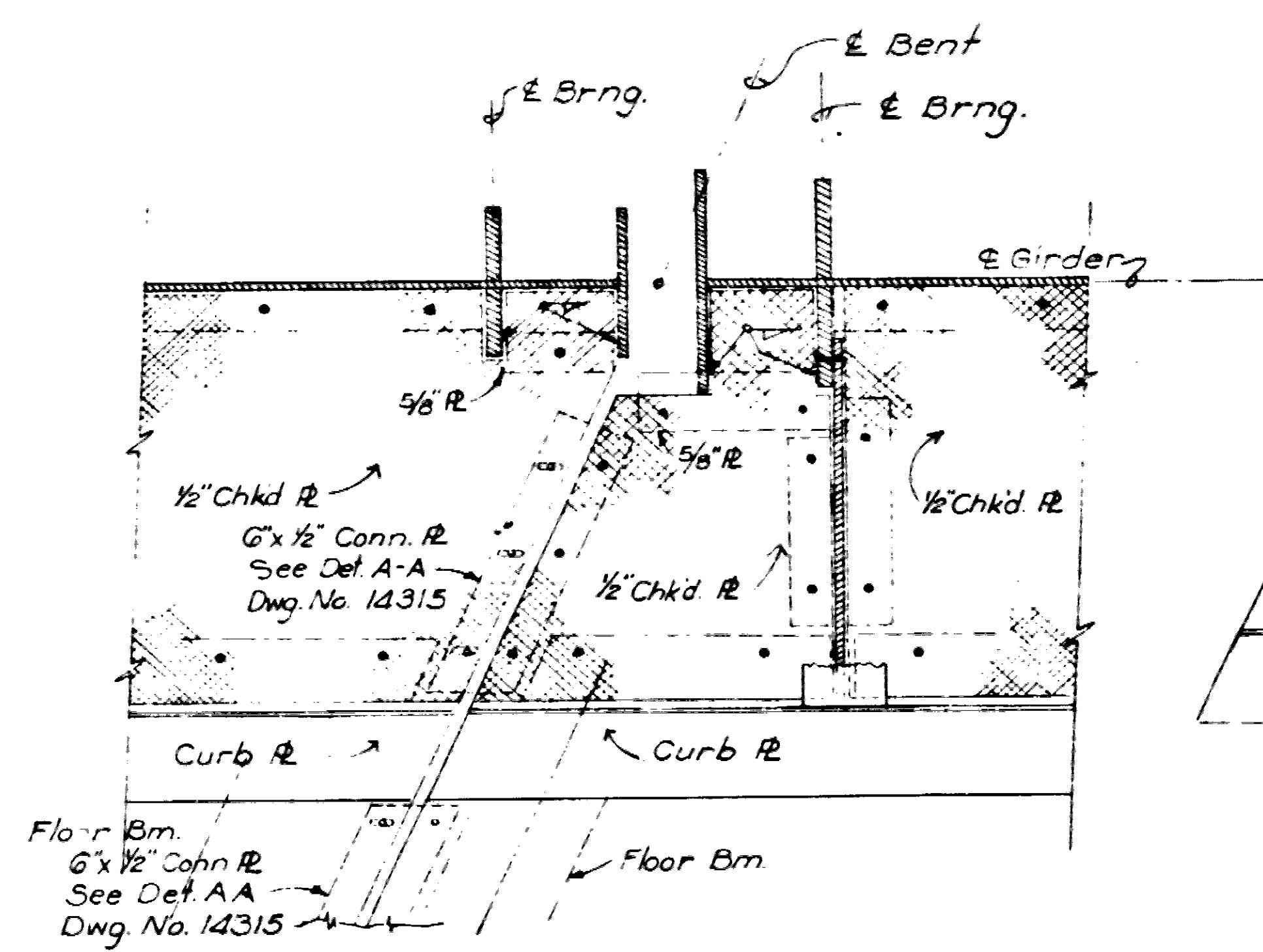
DETAILS OF FRAMING & GIRDERS
 I-30 & PRESCOTT-NORTHWESTERN R.R.
 NEVADA, COUNTY
 ROUTE 30 SEC. I
 ARKANSAS STATE HIGHWAY COMMISSION
 LITTLE ROCK, ARK.
 DRAWN BY: R.A.R. DATE: _____
 TRACED BY: H.L.H. DATE: _____
 CHECKED BY: D.L.W. DATE: _____
 BRIDGE NO. 5062 SCALE: AS NOTED
 DRAWING NO. 14314

REV.	BY	DATE	TED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
1	30-67	5-16-67	6	ARK.	1-30-1	123142	42	114
			JOB NO.	5062	WLD. DET.	14316		

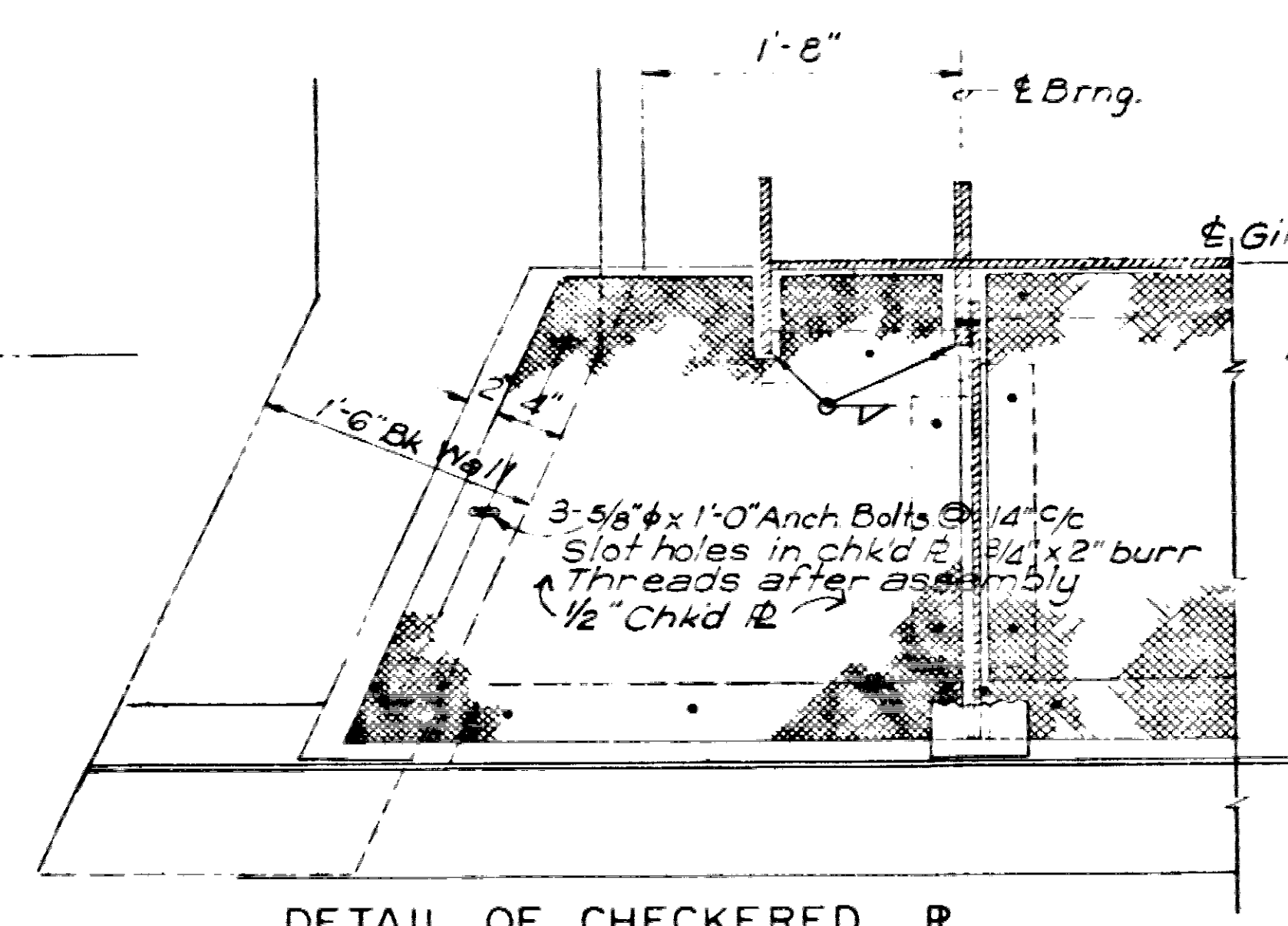


FLANGE CHAMFER DETAIL

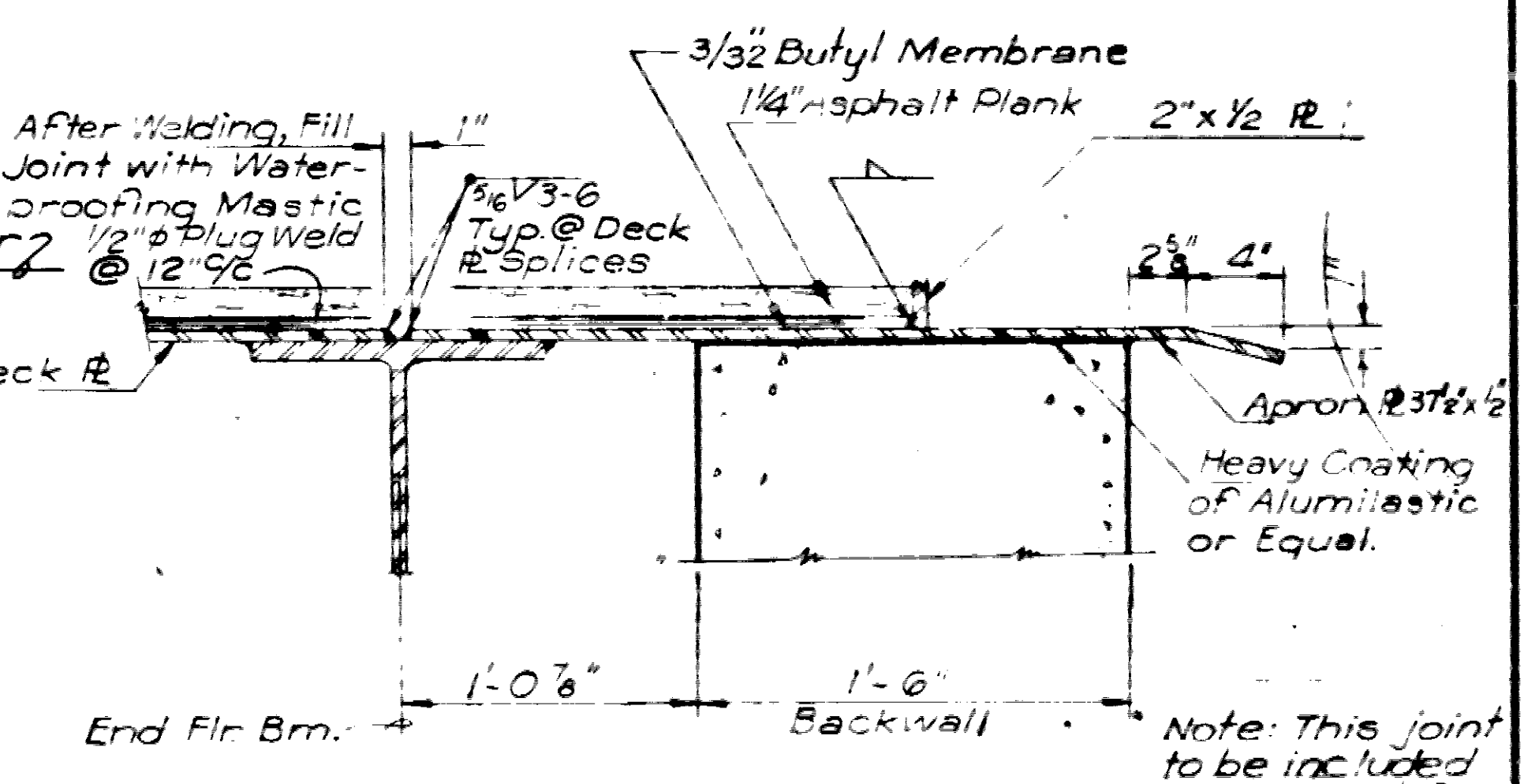
MAXIMUM THICKNESS OF THICKER PART	FILLET WELD SIZE
LESS THAN 3/4"	1/4"
OVER 3/4" TO 1 1/4"	5/16"
OVER 1 1/4" TO 2 1/2"	3/8"



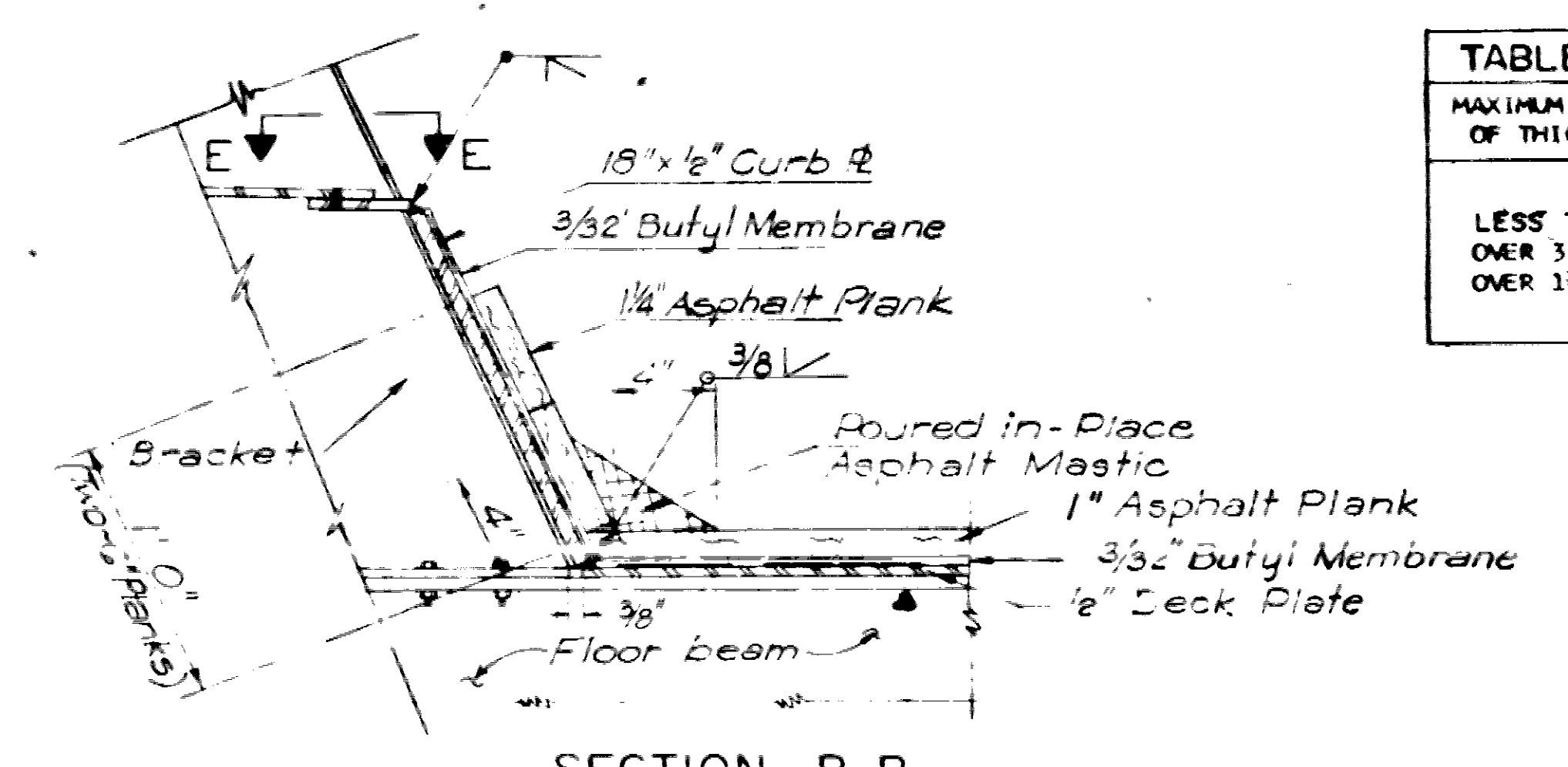
DETAIL OF CHECKERED R. AT INTERIOR BENTS



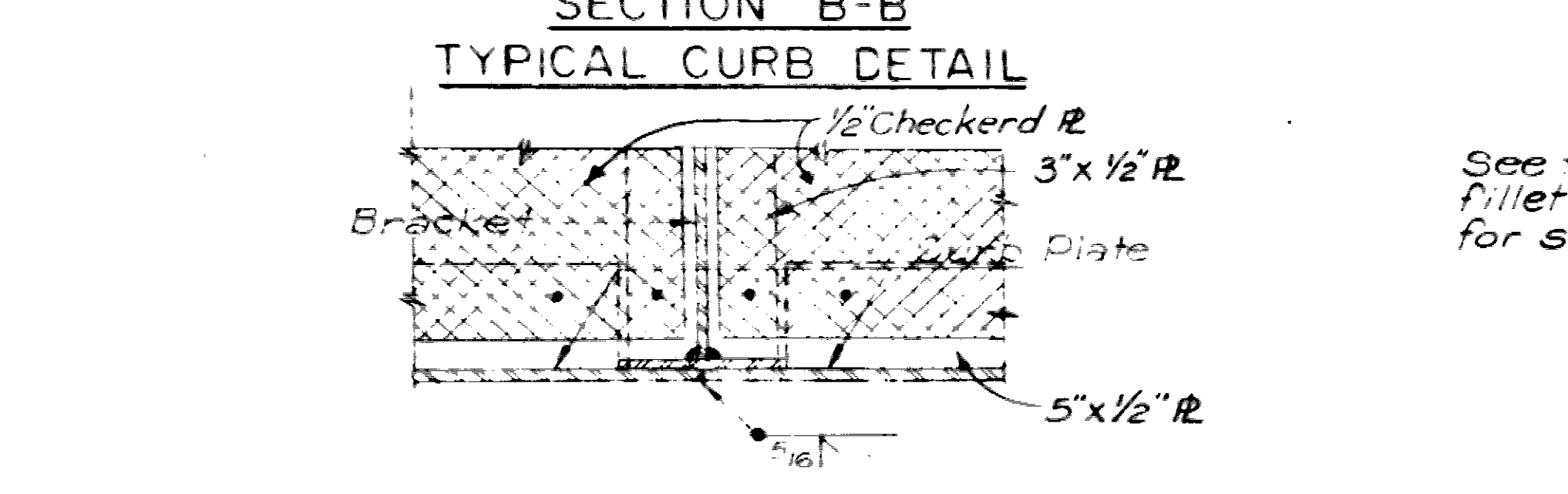
DETAIL OF CHECKERED R. AT END BENTS



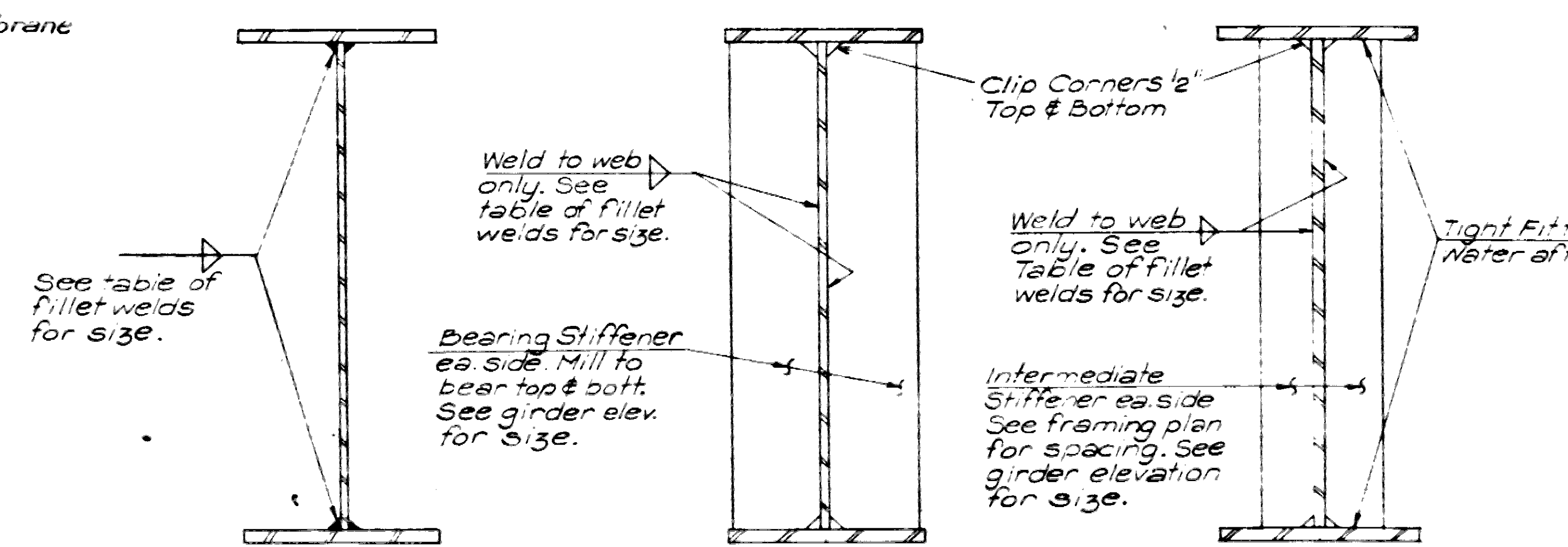
SECTION C-C. APRON PLATE DETAIL



SECTION B-B. TYPICAL CURB DETAIL

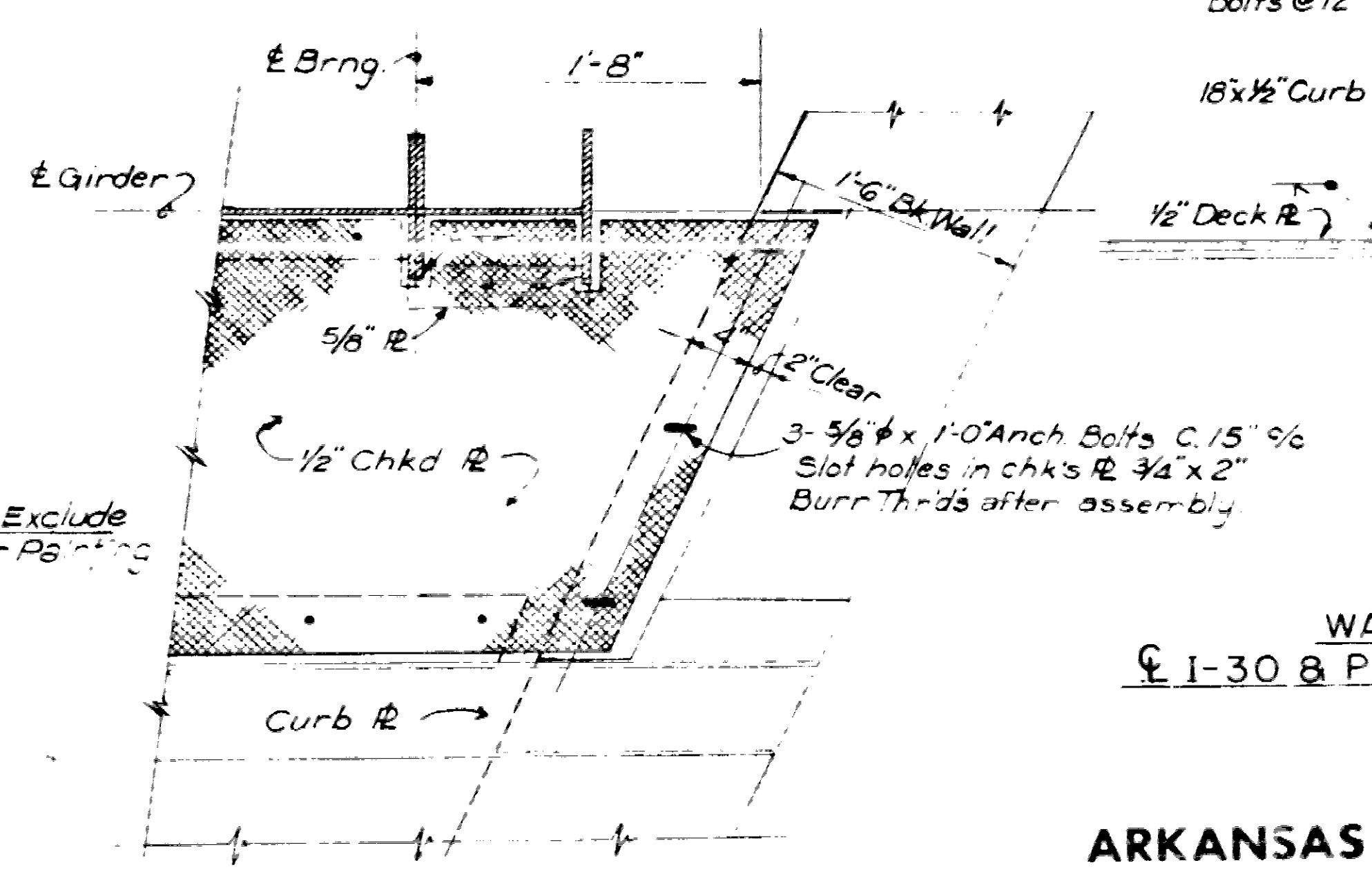


SECTION E-E. CURB PLATE JOINT DETAIL

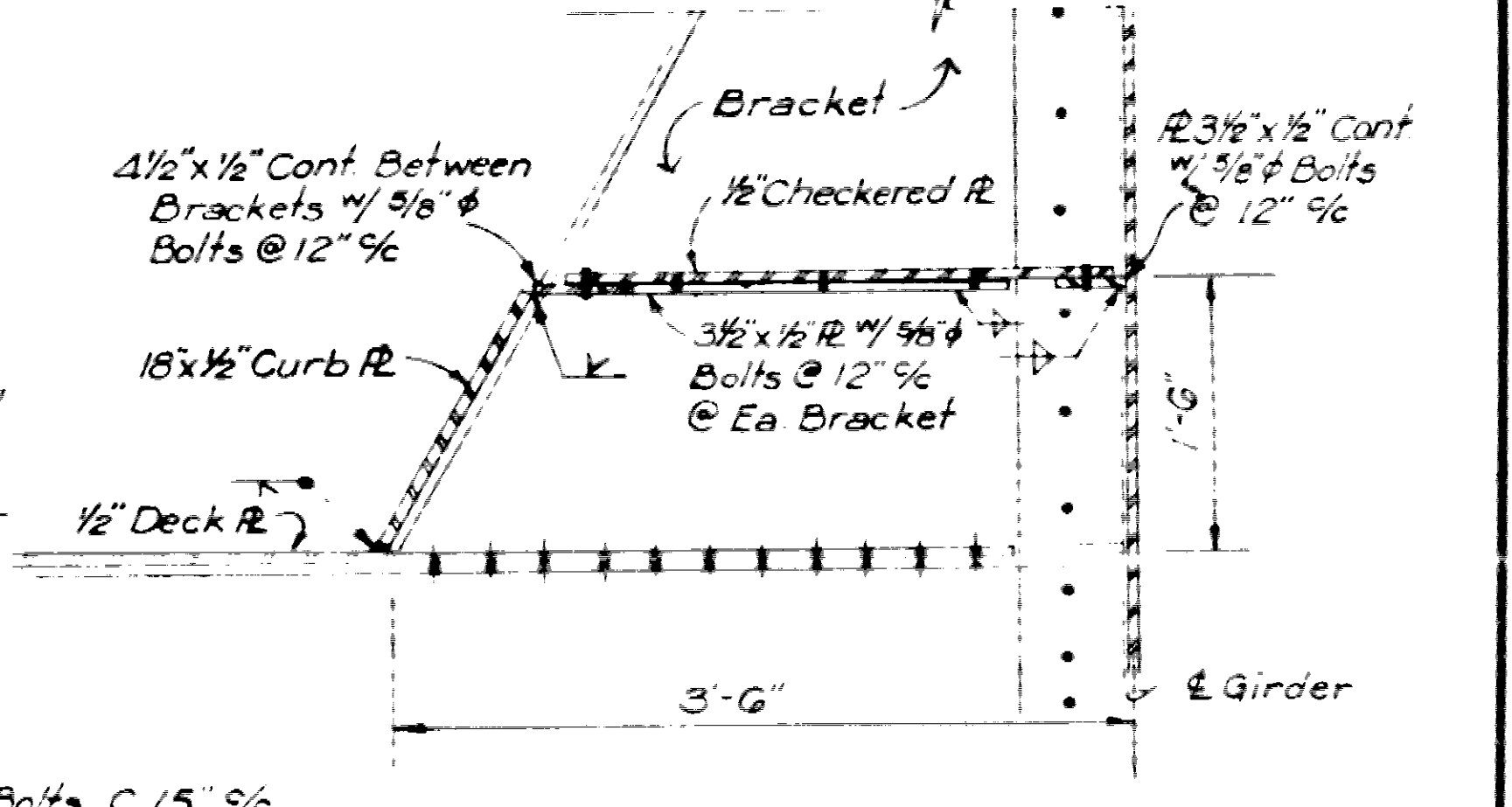


FLANGE TO WEB CONNECTION, BEARING STIFFENERS, INTERMEDIATE STIFFENERS

WELDING DETAILS. No Scale



DETAIL OF CHECKERED R. AT END BENTS



SECTION D-D. SCALE: 1" = 1'-0"

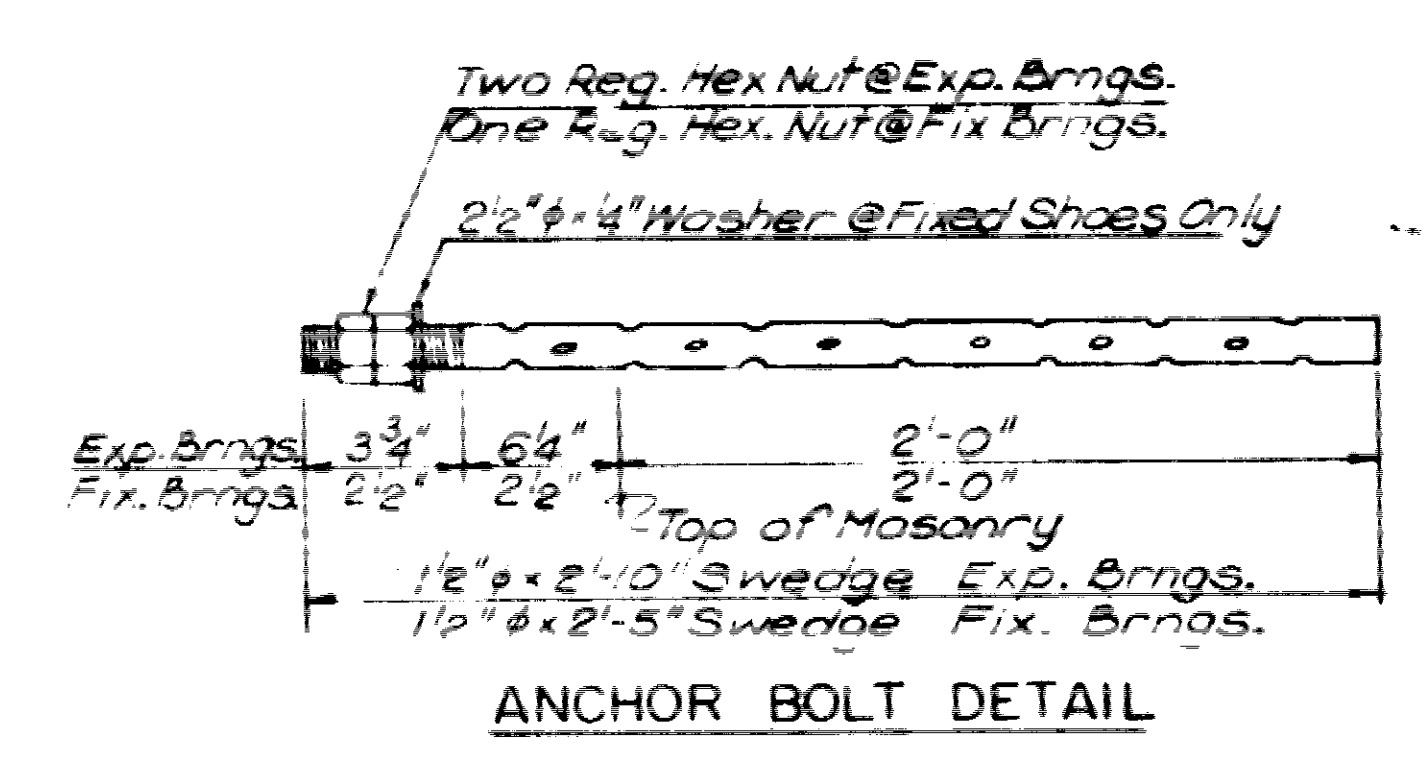
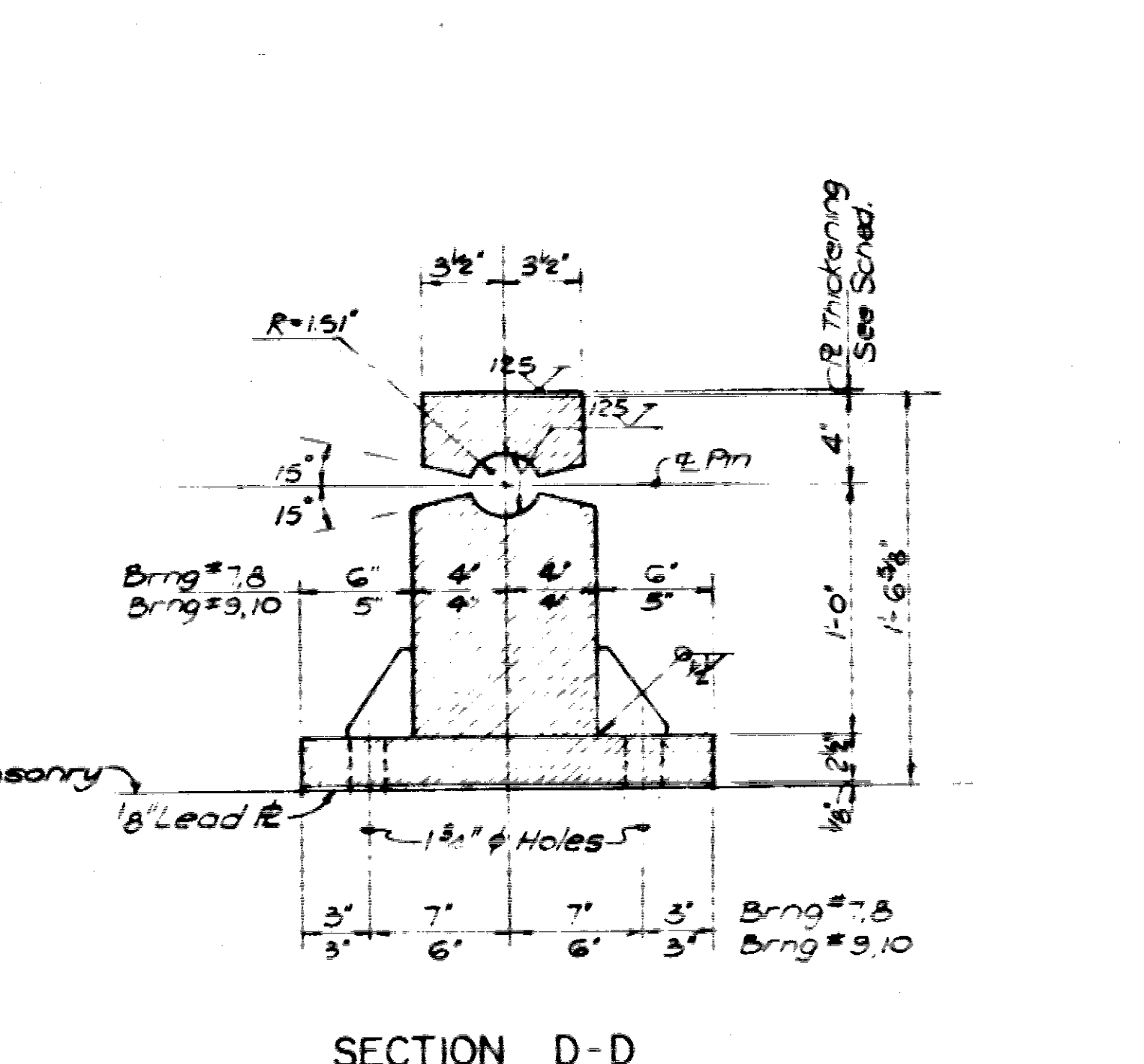
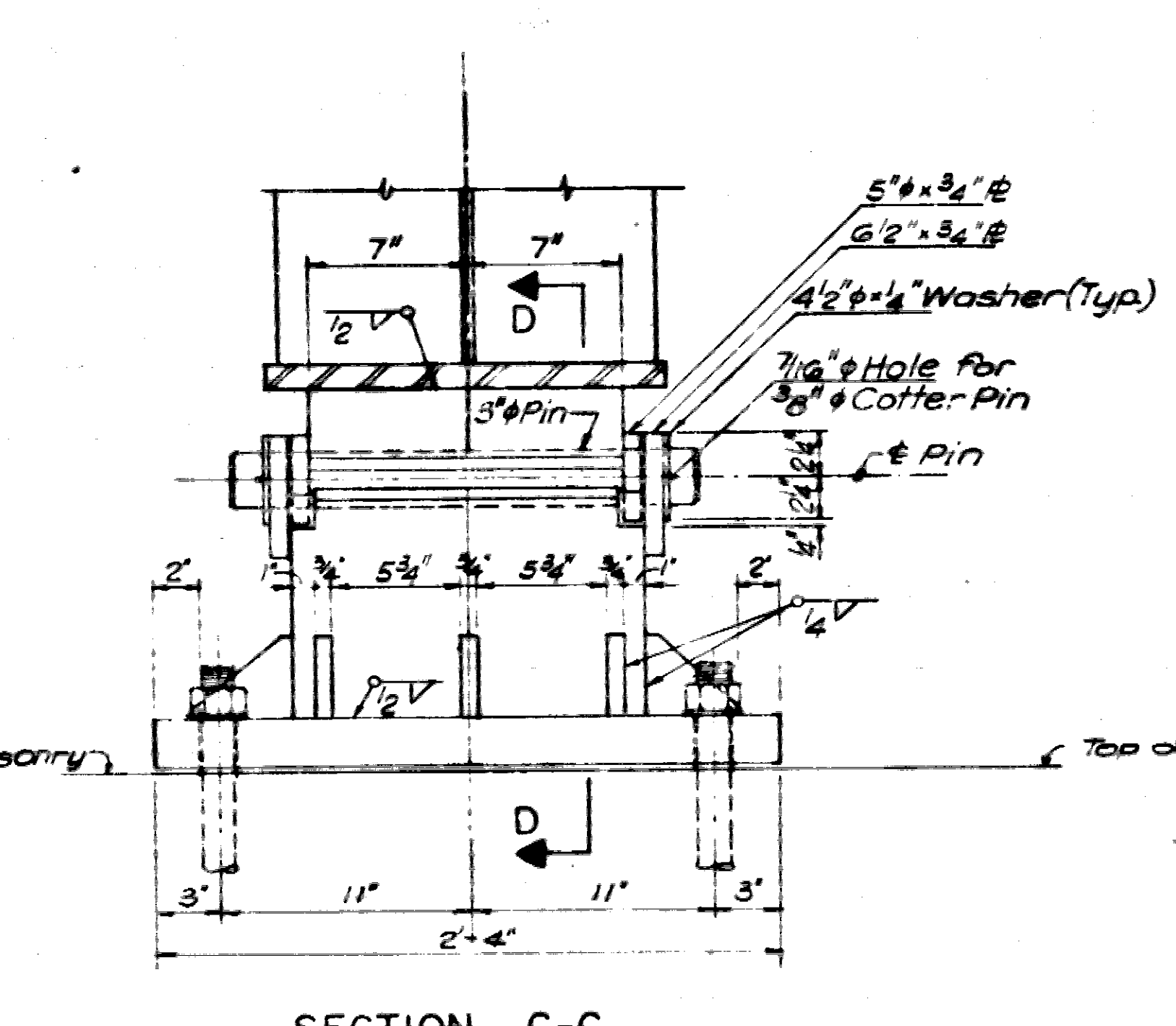
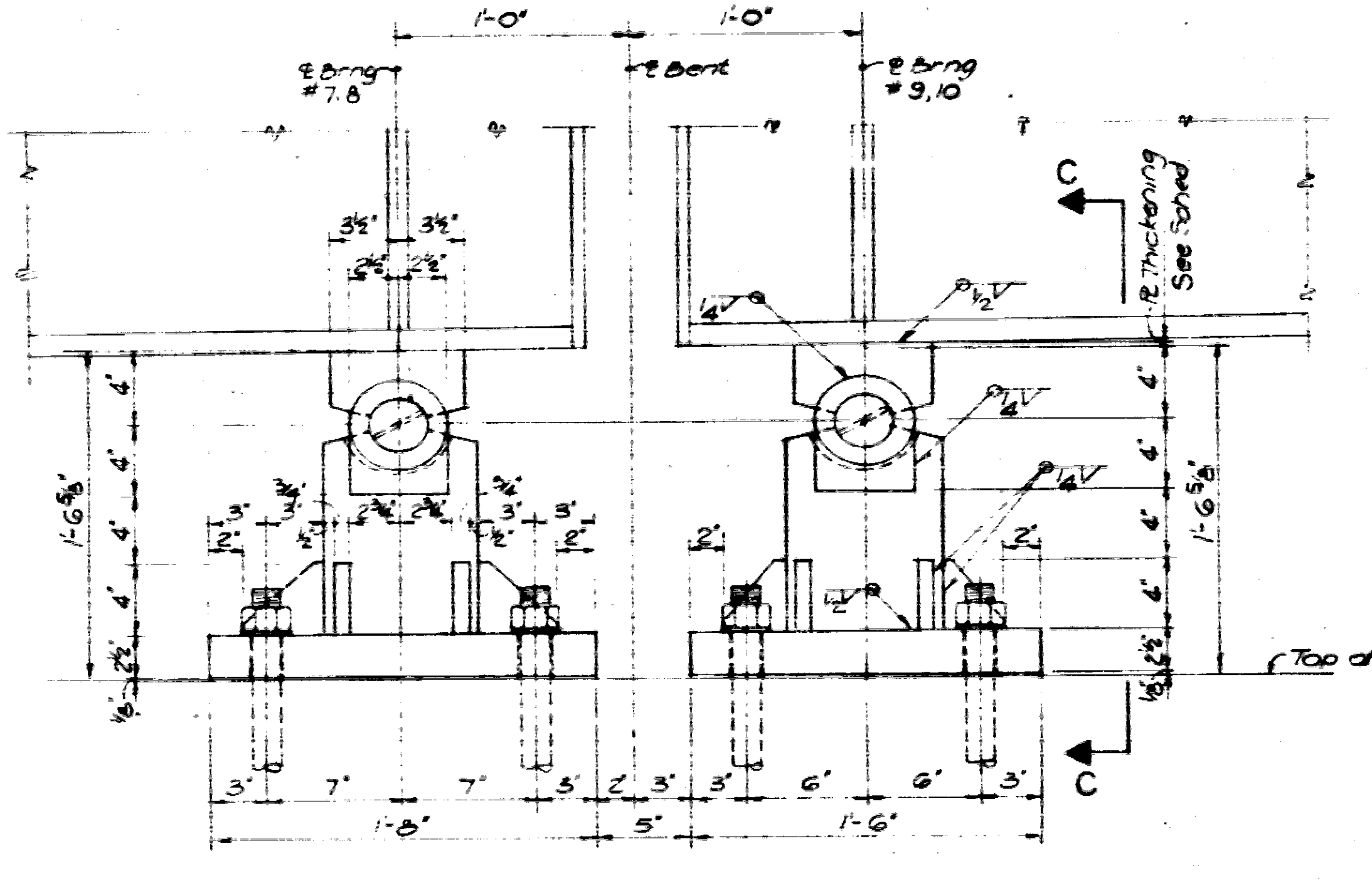
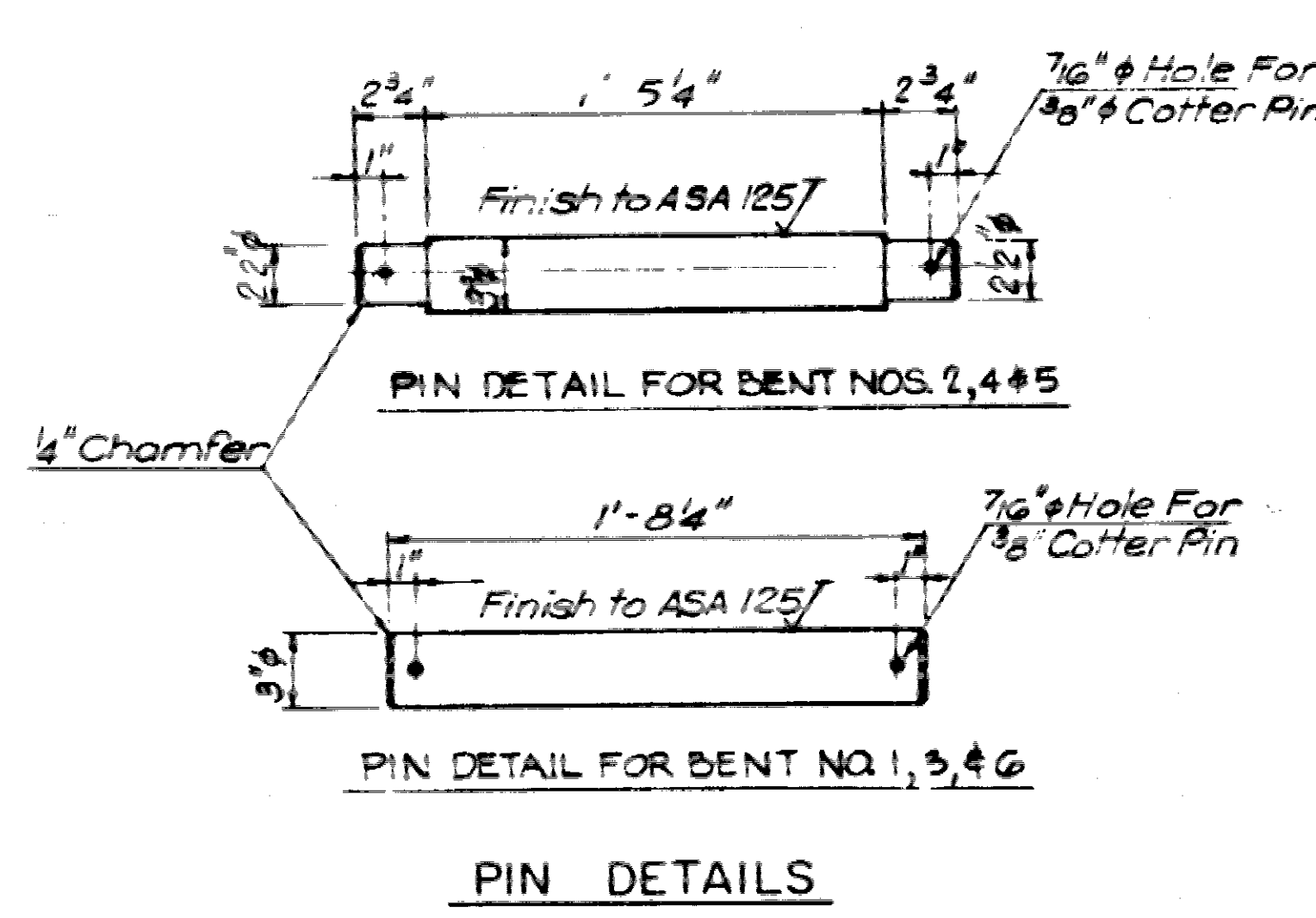
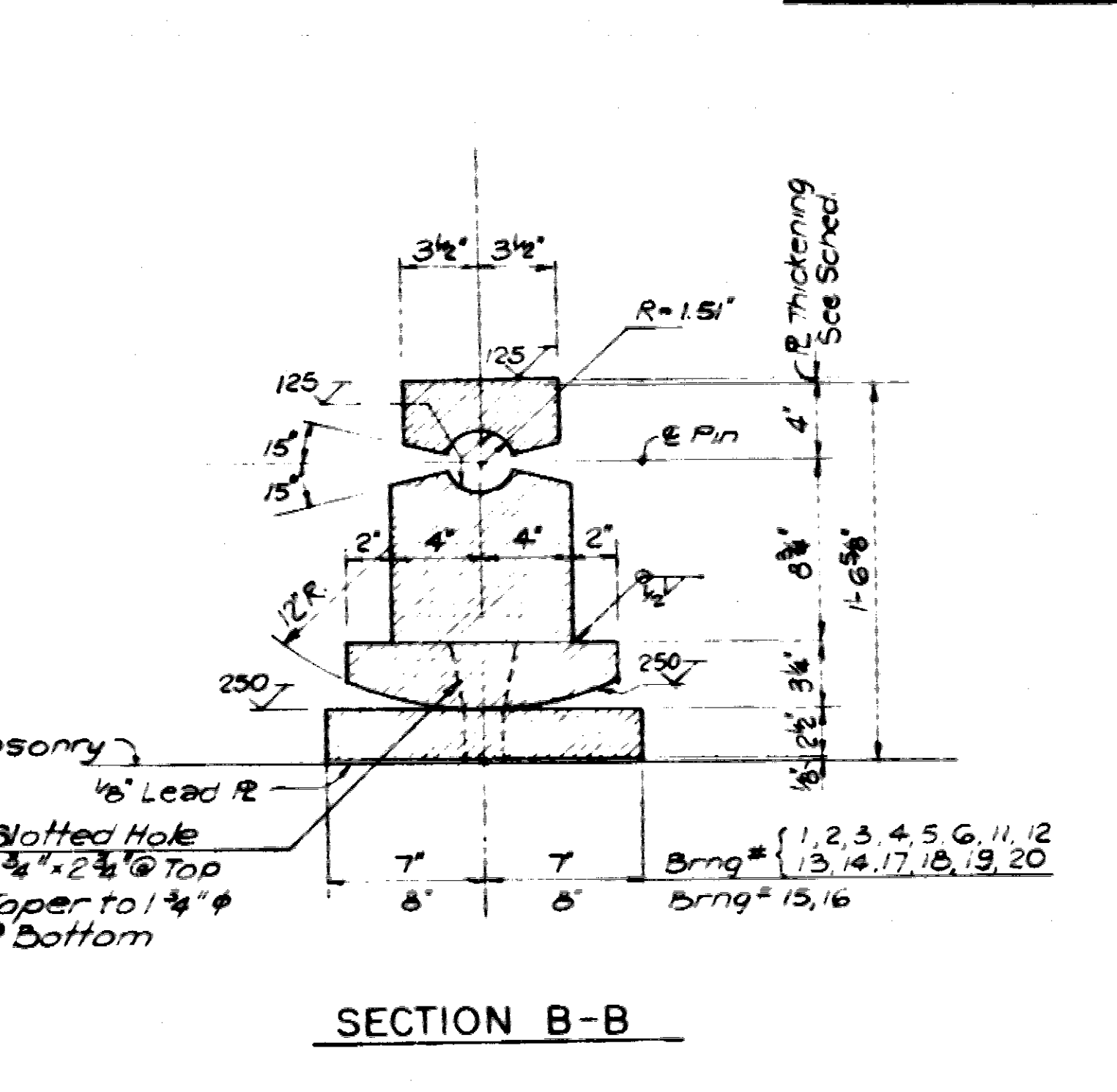
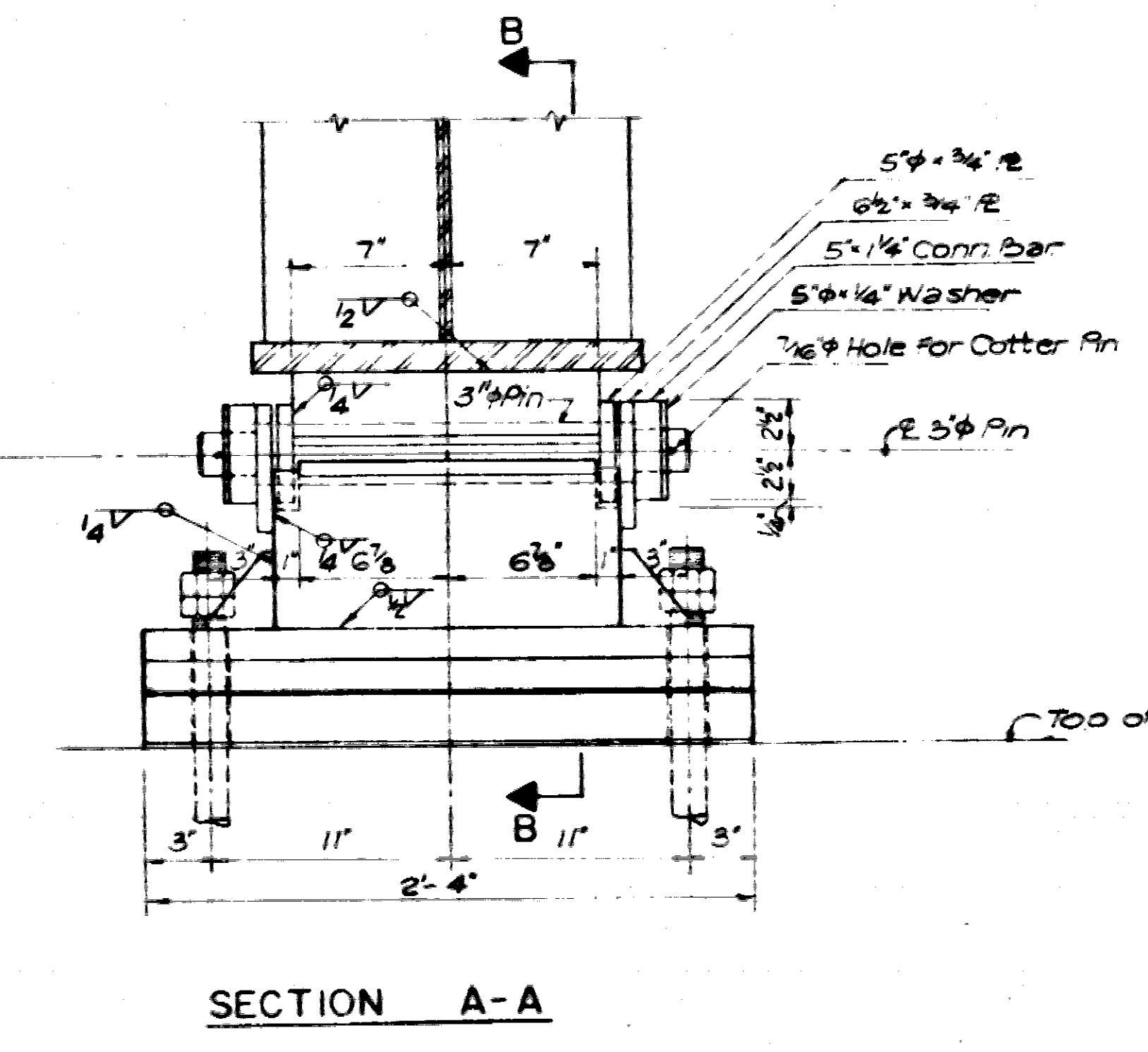
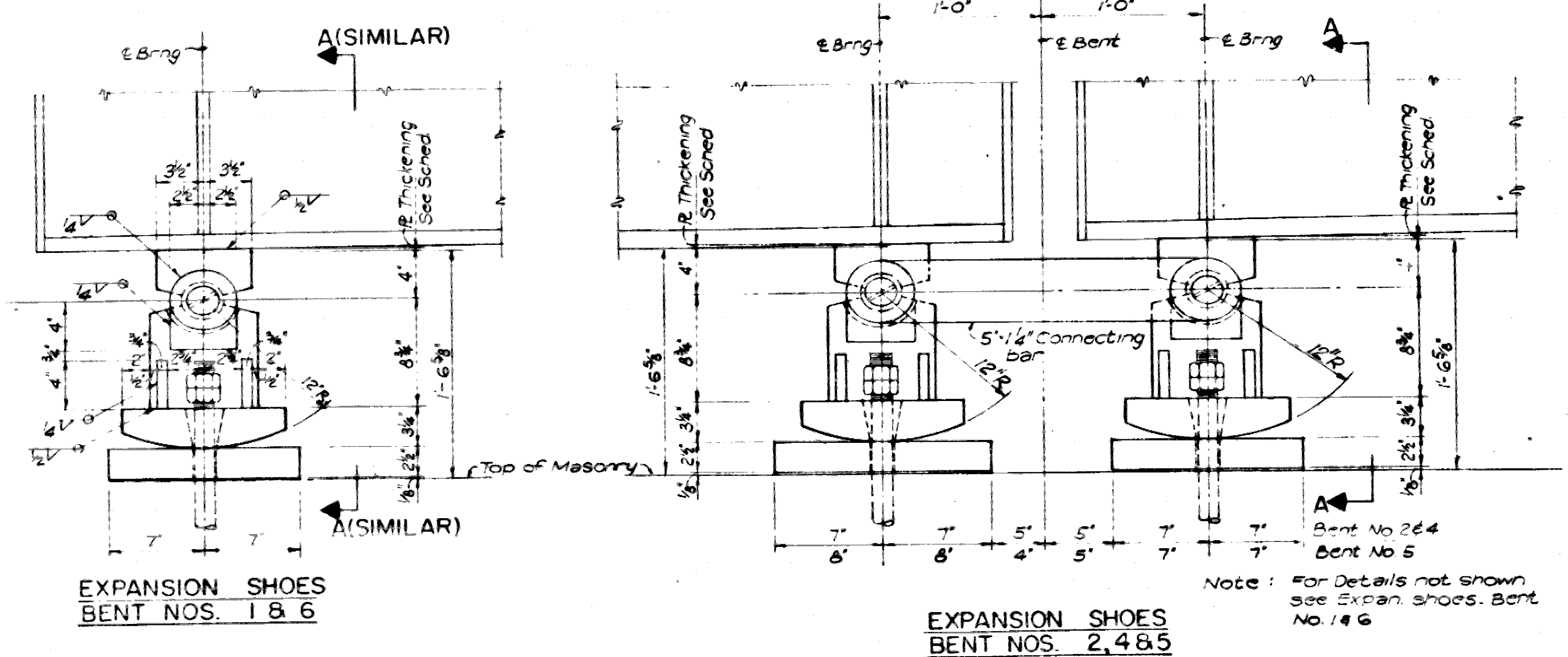
WELDING DETAILS & WATER PROOFING DETAILS
I-30 & PRESCOTT-NORTHWESTERN RR.
NEVADA COUNTY

ROUTE 30 SEC. 1
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

DRAWN BY: RAB DATE: _____
TRACED BY: R.S. DATE: _____
CHECKED BY: D.W. DATE: _____
BRIDGE NO. 5062 DRAWING NO. 14315

Removed Asphalt Primer Note: 4-30-67 www

REVISED	DATE	BY	APPROVED	DATE	BY
6	ARK	1-30-71			
JOB NO.	5062	DET. OF SH.	14316		



MAXIMUM THICKNESS OF THICKER PART OF THICKENING	FILLET WELD SIZE
To 3/4" incl.	1/4"
Over 3/4" to 1 1/2"	5/16"
Over 1 1/2" to 2 1/2"	3/8"
Over 2 1/2"	1/2"

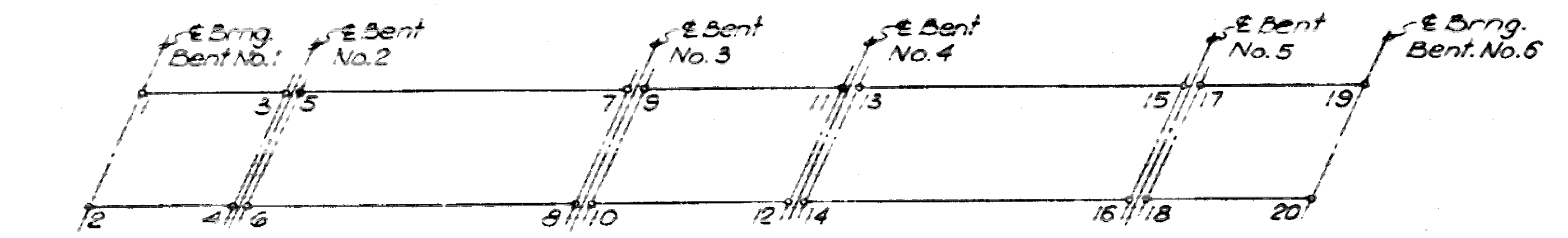
NOTES: SHOES SHALL BE FABRICATED FROM WELDED PLATES CONFORMING TO ASTM A-36. SHOES SHALL THEN BE STRESS-RELIEVED IN ACCORDANCE WITH THE PROVISIONS OF THE AMERICAN WELDING SOCIETY.

MASONRY PLATES FOR EXPANSION SHOES SHALL CONFORM TO ASTM A-36.

PINS SHALL CONFORM TO ASTM A-235 CLASS E, OR ASTM A-108 GRADE 1021 TO 1030, INCLUSIVE, WITH A MINIMUM ROCKWELL SCALE B HARDNESS OF 85.

ANCHOR BOLTS SHALL BE 1 1/2" #, SLEDGED BOLTS WITH HEX NUTS WITH PLAIN WASHERS FOR FIXED BEARINGS, AND WITH TWO HEX NUTS WITH NO WASHERS FOR EXPANSION BEARINGS. ANCHOR BOLTS SHALL BE GALVANIZED TO CONFORM TO ASTM A153.

SHOES, MASONRY PLATES, PINS AND ANCHOR BOLTS SHALL BE PAID FOR AS STRUCTURAL STEEL IN PLATE GIRDER SPANS (A-36).



BEARING NO.	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20
THICKENING	0"	0"	0"	0"	0"	0"	0"	0"	0"	0"	0"	0"	0"	0"	0"	0"	0"	0"	0"	0"

DETAILS OF SHOES
CL 1-30 & PRESCOTT-NORTHWESTERN RR
NEVADA COUNTY
ROUTE 30 SEC. 1
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

BRIDGE NO. 5062 DRAWING NO. 14316