

| | | | | | | | | |
|-----------------|-----------------|-----------------|-----------------|--|---------------|--------------------|-----------------|---------------------|
| DATE 2-13-85 | DATE 2-28-85 | DATE 2-28-85 | DATE 2-28-85 | FED. ROAD NO. 6 | STATE ARK. | FED. AID PROJ. NO. | SHEET NO. 33 | TOTAL SHEETS 177 |
| JOB NO. 110065 | | | | 2671R 2671AR - QUANT. 27010 6102 6103 | | | | |

SCHEDULE OF BRIDGE QUANTITIES - JOB 110065

| BRIDGE NO. | CODE NO. | NAME PLATE TITLE | UNIT OF BRIDGE | ITEM NO. | 205 | 801 | SP & 802 | SP & 802 | 803 | 804 | SP JOB 110065 | * SP & 805 | * SP & 805 | * SP & 805 | SP & 807 | SP & 807 | SP & 807 |
|------------|----------|--------------------------|------------------------------------|----------|---------------------------------------|---|------------------|----------------------|--------------------|------------------------------|---|---|-----------------------------------|---|--------------------------------------|--|--|
| | | | | UNIT | REMOVAL OF EXISTING BRIDGE STRUCTURES | UNCLASSIFIED EXCAVATION FOR STRUCTURES - BRIDGE | CLASS S CONCRETE | CLASS S(AE) CONCRETE | BOILED LINSEED OIL | REINFORCING STEEL (GRADE 60) | EPOXY COATED REINFORCING STEEL (GRADE 60) | PRECAST CONCRETE PILING (16" Oct. Or 14" Sq.) | PRECAST CONCRETE PILING (14" Sq.) | PRECAST CONCRETE PILING (18" Oct. or Sq.) Δ | STRUCTURAL STEEL IN BEAM SPANS (A36) | STRUCTURAL STEEL IN PLATE GIRDER SPANS (A36) | STRUCTURAL STEEL IN PLATE GIRDER SPANS (A572 - GR. 50) |
| | | | | | LUMP SUM | CU. YD. | CU. YD. | CU. YD. | GAL. | LB. | LB. | LIN. FT. | LIN. FT. | LIN. FT. | LB. | LB. | LB. |
| 6102 | X271 | WEST MEMPHIS INTERCHANGE | BENT 1 | | | | 77.01 | | | 7441 | | 975 | | | | 1856 | |
| | | | BENT 2 | | | 118 | 102.73 | | | 14042 | | 1500 | | | | | |
| | | | BENT 3 | | | 122 | 109.69 | | | 13884 | | 1650 | | | | | |
| | | | BENT 4 | | | 132 | 105.60 | | | 13303 | | 1450 | | | | | |
| | | | BENT 5 | | | | 56.67 | | | 4981 | | 840 | | | | 1252 | |
| 6103 | X271 | WEST MEMPHIS INTERCHANGE | 532' CONT. PL. GIRDER UNIT | | | | | 653.90 | 56.0 | 61869 | 83300 | | | | | 449092 | 247100 |
| | | | TOTALS FOR BR. NO. 6102 | | 0.46 | 372 | 451.70 | 653.90 | 56.0 | 115520 | 83300 | 6415 | | | | 452200 | 247100 |
| | | | BENT 1 | | | | 54.34 | | | 5241 | | 770 630 Δ | | | | 2675 | |
| | | | BENT 2 | | | 142 | 92.91 | | | 11076 | | 1350 | | | | | |
| | | | BENT 3 | | | 146 | 98.19 | | | 11649 | | 1305 | | | | | |
| 2671R | X171 | RAILROAD OVERPASS | BENT 4 | | | | 62.26 | | | 6063 | | 840 700 Δ | | | | 3341 | |
| | | | 365' CONT. PL. GIRDER UNIT | | | | | 450.30 | 38.7 | 42491 | 59250 | | | | | 303404 | 192720 |
| | | | TOTALS FOR BR. NO. 6103 | | 0.54 | 288 | 307.70 | 450.30 | 38.7 | 76520 | 59250 | 4265 3985 Δ | | | | 309420 | 192720 |
| | | | END BENTS 1 & 22 | | | | 22.52 | | | 2072 | | | | 396 | 1172 | | |
| | | | INT. BENTS 2, 3 & 4 | | | | 29.89 | | | 4751 | | | | 660 | | | |
| 2671R | X171 | RAILROAD OVERPASS | INT. BENTS 5, 6, 7 & 8 | | | | 60.77 | | | 9426 | | | | 800 | | | |
| | | | INT. BENTS 9, 10, 11 & 12 | | | | 67.47 | | | 10277 | | | | 800 | | | |
| | | | INT. BENTS 13, 14, 15, 16, 17 & 18 | | | | 93.41 | | | 14423 | | | | 1200 | | | |
| | | | INT. BENTS 19, 20, & 21 | | | | 29.70 | | | 4743 | | | | 660 | | | |
| | | | PIERS 1 & 2 | | 80 | 100.04 | | | | 7097 | | | 1000 | | | | |
| | | | 45'-0" W-BM. SPANS | | | | | 645.33 | 55.9 | 63670 | 68891 | | | | 182121 | | |
| | | | 47'-3 1/2" TRANSITION SPAN 1 | | | | | 59.29 | 5.1 | 6232 | 6319 | | | | 18592 | | |
| | | | 66'-6" TRANSITION SPAN 2 | | | | | 79.75 | 6.9 | 8535 | 9073 | | | | 23120 | | |
| | | | 42'-8 1/2" TRANSITION SPAN 3 | | | | | 49.31 | 4.2 | 5304 | 5404 | | | | 15368 | | |
| | | | 126'-6" CONT. W-BM. UNITS | | | | | 302.12 | 26.2 | 27350 | 34943 | | | | 72527 | | |
| | | | TOTALS FOR BR. NO. 2671R | | | 80 | 403.80 | 1135.80 | 98.3 | 163880 | 124630 | | 1000 | 4516 | 312900 | | |
| | | | END BENTS 1 & 20 | | | | 37.62 | | | 3378 | | | | 792 | 1648 | | |
| | | | INT. BENTS 2 & 19 | | | | 33.52 | | | 4656 | | | | 720 | | | |
| | | | INT. BENTS 3, 4, 5 & 6 | | | | 118.90 | | | 16923 | | | | 1200 | | | |
| | | | INT. BENTS 7, 8, 9 & 10 | | | | 129.38 | | | 18238 | | | | 1200 | | | |
| | | | INT. BENTS 11, 12, 13 & 14 | | | | 130.50 | | | 18348 | | | | 1200 | | | |
| | | | INT. BENTS 15, 16, 17 & 18 | | | | 117.44 | | | 16621 | | | | 1200 | | | |
| 2671R | X171 | RAILROAD OVERPASS | PIERS 1 & 2 | | 159 | 221.34 | | | | 13199 | | | 2000 | | | | |
| | | | 45'-0" W-BM. SPANS | | | | | 1117.60 | 100.0 | 107901 | 129018 | | | | 463413 | | |
| | | | 36'-6" W-BM. SPANS | | | | | 114.26 | 10.1 | 10149 | 10653 | | | | 46970 | | |
| | | | 45'-0" TRANSITION SPAN 1 | | | | | 78.10 | 7.0 | 8776 | 8973 | | | | 40884 | | |
| | | | 66'-6" TRANSITION SPAN 2 | | | | | 103.32 | 9.3 | 12164 | 12662 | | | | 44402 | | |
| | | | 45'-0" TRANSITION SPAN 3 | | | | | 62.92 | 5.7 | 6077 | 6194 | | | | 25763 | | |
| | | | TOTALS FOR BR. NO. 2671R | | | 159 | 788.70 | 1476.20 | 132.1 | 236430 | 167500 | | 2000 | 6312 | 623080 | | |
| | | | TOTALS FOR JOB 110065 | | | 899 | 1951.90 | 3716.20 | 325.1 | 592350 | 434680 | 10680 10400 Δ | 3000 | 10828 | 935980 | 761620 | 439820 |

* Refers to SP 802-5

SHEET 1 OF 2

SCHEDULE OF BRIDGE QUANTITIES

I-55/I-40 INTERCHANGE ALTERATIONS

CRITTENDEN, COUNTY

ROUTE SEC.

ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

DRAWN BY: D.H.P. DATE: 12-10-84

CHECKED BY: HJD DATE: 1-22-85 SCALE: None

DESIGNED BY: DATE:

2671R 2671AR

BRIDGE NO. 6102 6103 DRAWING NO. 27010

DALE F. LOE
DESIGN SECTION SUPERVISOR

Δ Revised Piling and Piling Quant., 2-28-85 D.H.P.

David Pinkerton
BRIDGE ENGINEER

SCHEDULE OF BRIDGE QUANTITIES - JOB 110065

| BRIDGE NO. | CODE NO. | NAME PLATE TITLE | UNIT OF BRIDGE | ITEM NO | SP # 808 | SP # 809 | 812 | 816 | SPJOB 110065 | SPJOB 110065 | SPJOB 110065 | SPJOB 110065 | ** SP # 805 | | |
|----------------------------|----------|--------------------------|------------------------------------|---------|----------------------|------------------------|-----------------------------|-----------------|--|----------------------------|---|--------------------------------|-------------|--|--|
| | | | | ITEM | ELASTOMERIC BEARINGS | PREFORMED JOINT SEALER | BRIDGE NAME PLATES (TYPE C) | CONCRETE RIPRAP | PAINTING EXISTING STRUCTURAL STEEL (TYPE II) | REMODELING EXISTING BRIDGE | FURNISHING AND REPLACING HANGER PLATES AND PINS | TEST PILES (16" OCT. OR 14SQ.) | | | |
| | | | | UNIT | LUMP SUM | LIN. FT. | EACH | CU. YD. | LUMP SUM | LUMP SUM | EACH | LIN. FT. | | | |
| G102 | X271 | WEST MEMPHIS INTERCHANGE | BENT 1 | | | | | 190 | | | | 70 | | | |
| | | | BENT 2 | | | | | | | | | | | | |
| | | | BENT 3 | | | | | | | | | | | | |
| | | | BENT 4 | | | | | | | | | | | | |
| | | | BENT 5 | | | | | 336 | | | | 55 | | | |
| | | | 532' CONT. PL. GIRDER UNIT | | | | | | | | | | | | |
| TOTALS FOR BR. NO. G102 | | | | 0.56 | | 1 | 526 | | | | 125 | | | | |
| G103 | X271 | WEST MEMPHIS INTERCHANGE | BENT 1 | | | | | 275 | | | | 75 | | | |
| | | | BENT 2 | | | | | | | | | | | | |
| | | | BENT 3 | | | | | | | | | 50 | | | |
| | | | BENT 4 | | | | | 243 | | | | | | | |
| | | | 365' CONT. PL. GIRDER UNIT | | | | | | | | | | | | |
| | | | TOTALS FOR BR. NO. G103 | 0.44 | | 1 | 518 | | | 125 | | | | | |
| 2671 AR | X171 | RAILROAD OVERPASS | END BENTS 1 & 22 | | | | | | | | | | | | |
| | | | INT. BENTS 2, 3 & 4 | | | | | | | | | | | | |
| | | | INT. BENTS 5, 6, 7 & 8 | | | | | | | | | | | | |
| | | | INT. BENTS 9, 10, 11 & 12 | | | | | | | | | | | | |
| | | | INT. BENTS 13, 14, 15, 16, 17 & 18 | | | | | | | | | | | | |
| | | | INT. BENTS 19, 20, & 21 | | | | | | | | | | | | |
| | | | PIERS 1 & 2 | | | | | | | | | | | | |
| | | | 45'-0" W-BM. SPANS | | | | | | | | | | | | |
| | | | 47'-3 1/2" TRANSITION SPAN 1 | | | | | | | | | | | | |
| | | | 66'-6" TRANSITION SPAN 2 | | | | | | | | | | | | |
| | | | 42'-8 1/2" TRANSITION SPAN 3 | | | | | | | | | | | | |
| | | | 126'-6" CONT. W-BM. UNITS | | | | | | | | | | | | |
| TOTALS FOR BR. NO. 2671 AR | | | | | 764 | 1 | | 0.5 | 0.5 | 1 | | | | | |
| 2671 R | X171 | MO-PAC OVERPASS | END BENTS 1 & 20 | | | | | | | | | | | | |
| | | | INT. BENTS 2 & 19 | | | | | | | | | | | | |
| | | | INT. BENTS 3, 4, 5 & 6 | | | | | | | | | | | | |
| | | | INT. BENTS 7, 8, 9 & 10 | | | | | | | | | | | | |
| | | | INT. BENTS 11, 12, 13 & 14 | | | | | | | | | | | | |
| | | | INT. BENTS 15, 16, 17 & 18 | | | | | | | | | | | | |
| | | | PIERS 1 & 2 | | | | | | | | | | | | |
| | | | 45'-0" W-BM. SPANS | | | | | | | | | | | | |
| | | | 36'-6" W-BM. SPANS | | | | | | | | | | | | |
| | | | 45'-0" TRANSITION SPAN 1 | | | | | | | | | | | | |
| | | | 66'-6" TRANSITION SPAN 2 | | | | | | | | | | | | |
| | | | 45'-0" TRANSITION SPAN 3 | | | | | | | | | | | | |
| TOTALS FOR BR. NO. 2671 R | | | | | 1292 | 1 | | 0.5 | 0.5 | 2 | | | | | |
| TOTALS FOR JOB 110065 | | | | 1 | 2056 | 4 | 1044 | 1 | 1 | 3 | 250 | | | | |

** Refers to SP 802-5 & SP 805-2

SHEET 2 OF 2

SCHEDULE OF BRIDGE QUANTITIES

1-55/1-40 INTERCHANGE ALTERATIONS

CRITTENDEN. COUNTY

ROUTE SEC. ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

DRAWN BY: D.H.P. DATE: 12-10-84

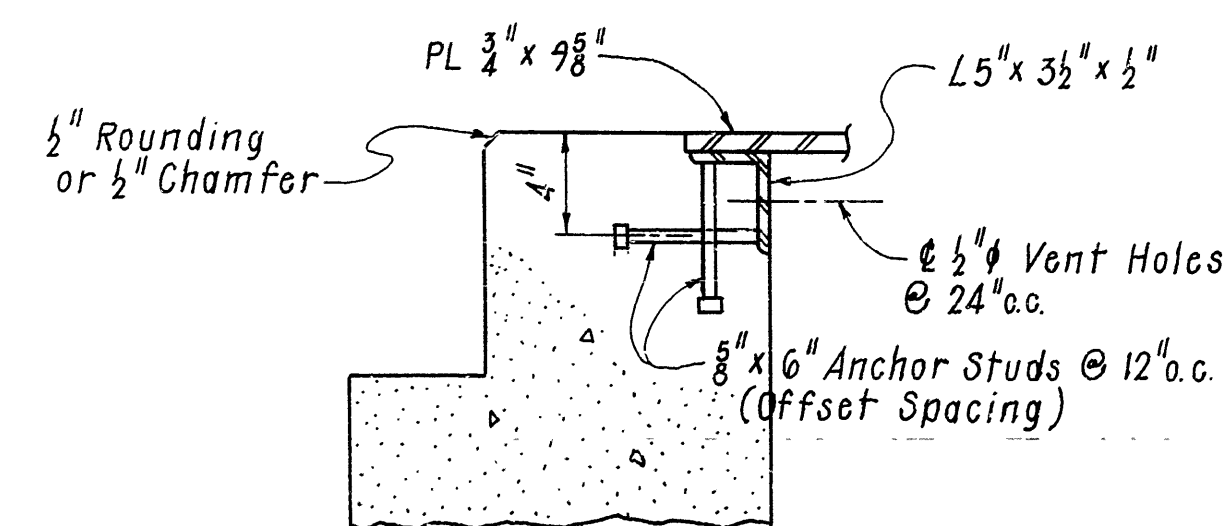
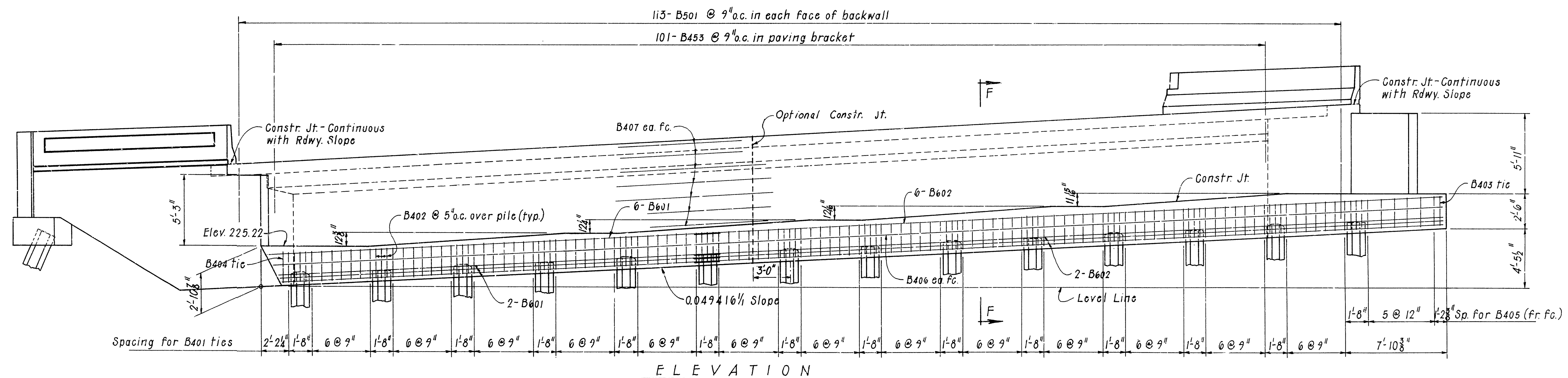
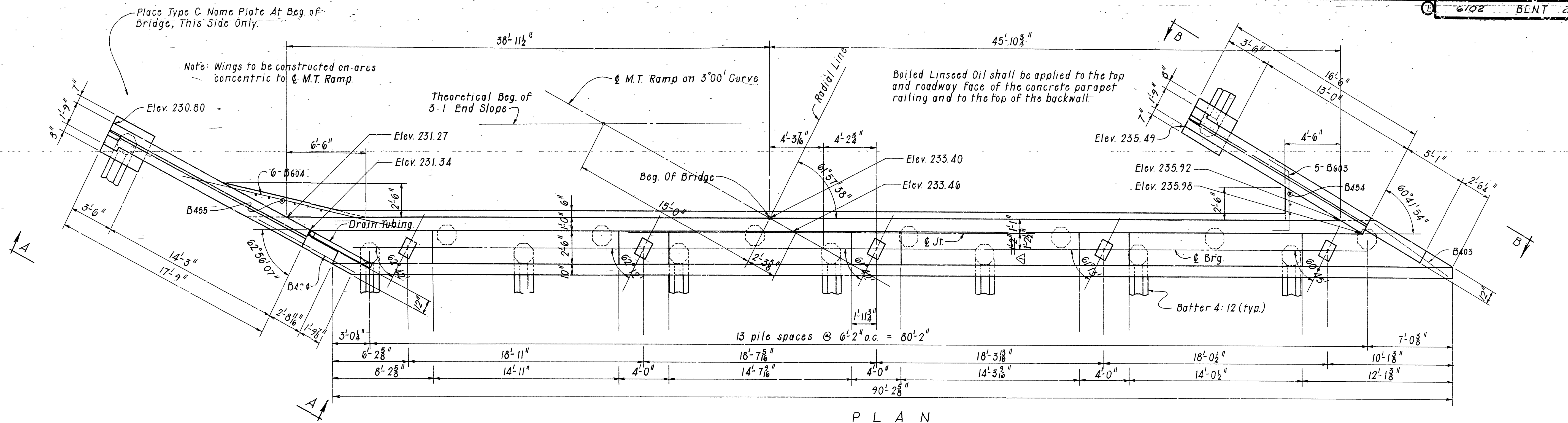
CHECKED BY: HJD DATE: 1-22-85 SCALE: None

DESIGNED BY: _____ DATE: _____
BRIDGE NO. 2671R 2671AR 6102 6103 DRAWING NO. 2701!

DALE F. LOE
DESIGN SECTION SUPERVISOR

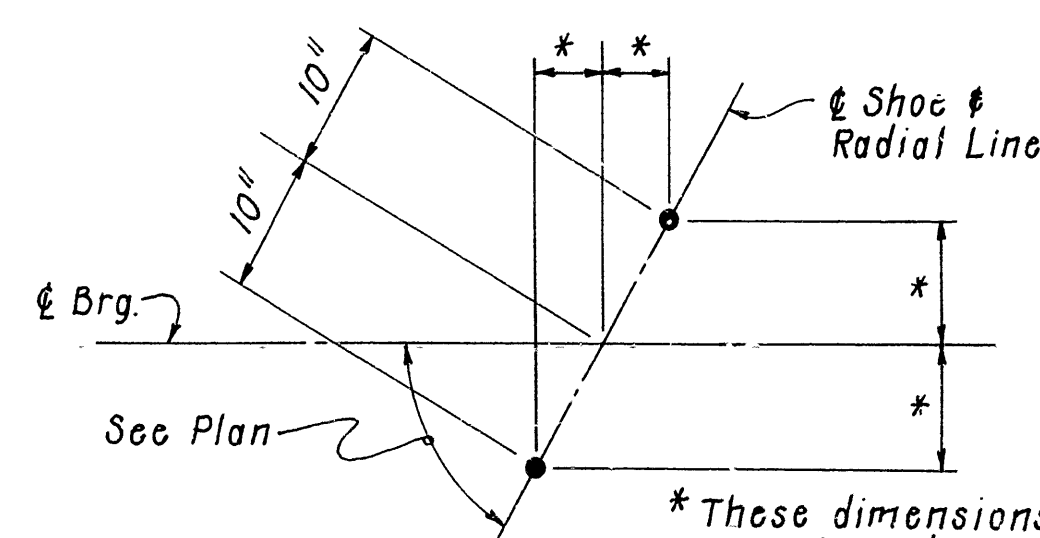
Beryl Pinkerton
BRIDGE ENGINEER

4/3



Note: For Joint & Exp. Device Details, see
dwg. no. 27025, 27027

DETAIL G
N.T.S.



* These dimensions vary with respect to the given angles shown on Plan View.

ANCHOR BOLT LOCATION (TYP.)
N.T.S.

GENERAL NOTES

ALL CONCRETE TO BE CLASS "S" AND TO BE POURED IN THE DRY.

ALL CONCRETE SHALL HAVE A MINIMUM 28 DAY COMPRESSIVE STRENGTH
 $f'_c = 3500$ PSI. ALL EXPOSED CORNERS TO BE CHAMFERED 3/4 INCH.

REINFORCING STEEL TO BE ASTM A615 OR A617, GRADE 60. SEE LAYOUT FOR PILE DATA AND GENERAL NOTES.

IN GENERAL, ALL CONSTRUCTION JOINTS IN BENTS SHALL BE PROVIDED WITH KEYS NOT LESS THAN 1-1/2 INCH HIGH COVERING THE MIDDLE THIRD OF BOTH DIMENSIONS.

LIVE LOAD: HS 20 & MILITARY

DESIGN SPECIFICATIONS: AASHTO 1983 WITH CURRENT INTERIM SPECIFICATIONS.

METHOD OF DESIGN: LOAD FACTOR

ALL STRUCTURAL STEEL SHALL BE ASTM A36 AND SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER POUND B/L FOR "STRUCTURAL STEEL IN GATE GIRDER SPANS (A36)."

FOR END BENT CONSTRUCTION PROCEDURE, SEE JOB SPECIAL PROVISIONS.

⚠ Rev. 4-18-85 Brq. Dimen.

(SHEET 1 OF 2)

DETAILS OF END BENT 1
M.T. RAMP BRIDGE OVER I-40

ROUTE SEC.
ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

Mai 11-21-84

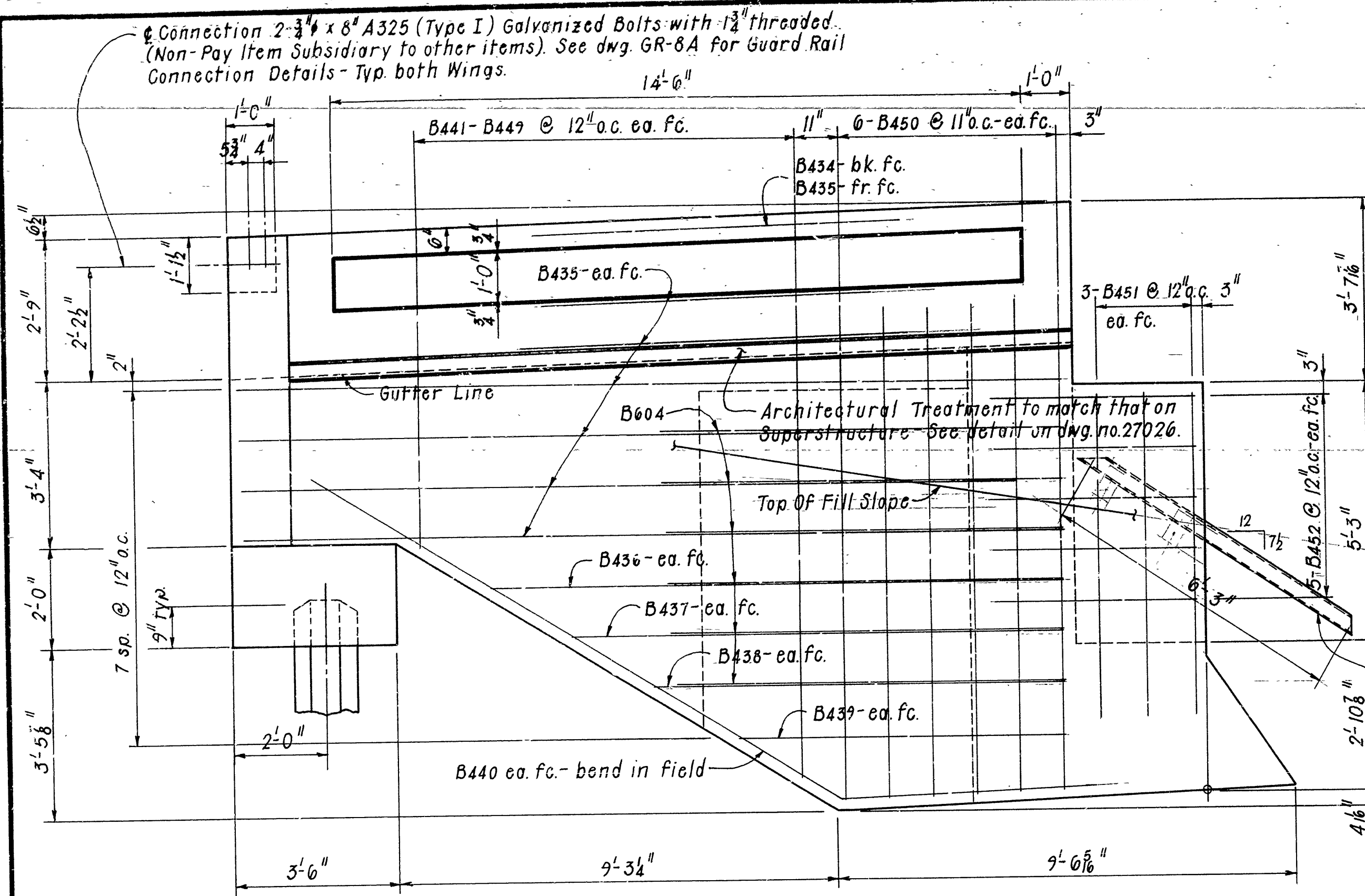
DRAWN BY: H. M. J. DATE: 11-21-84
CHECKED BY: H. M. J. DATE: 1-15-85

SCALE: $1'' = 1'-0''$ or as shown

BRIDGE NO.6102

DRAWING NO. 27013

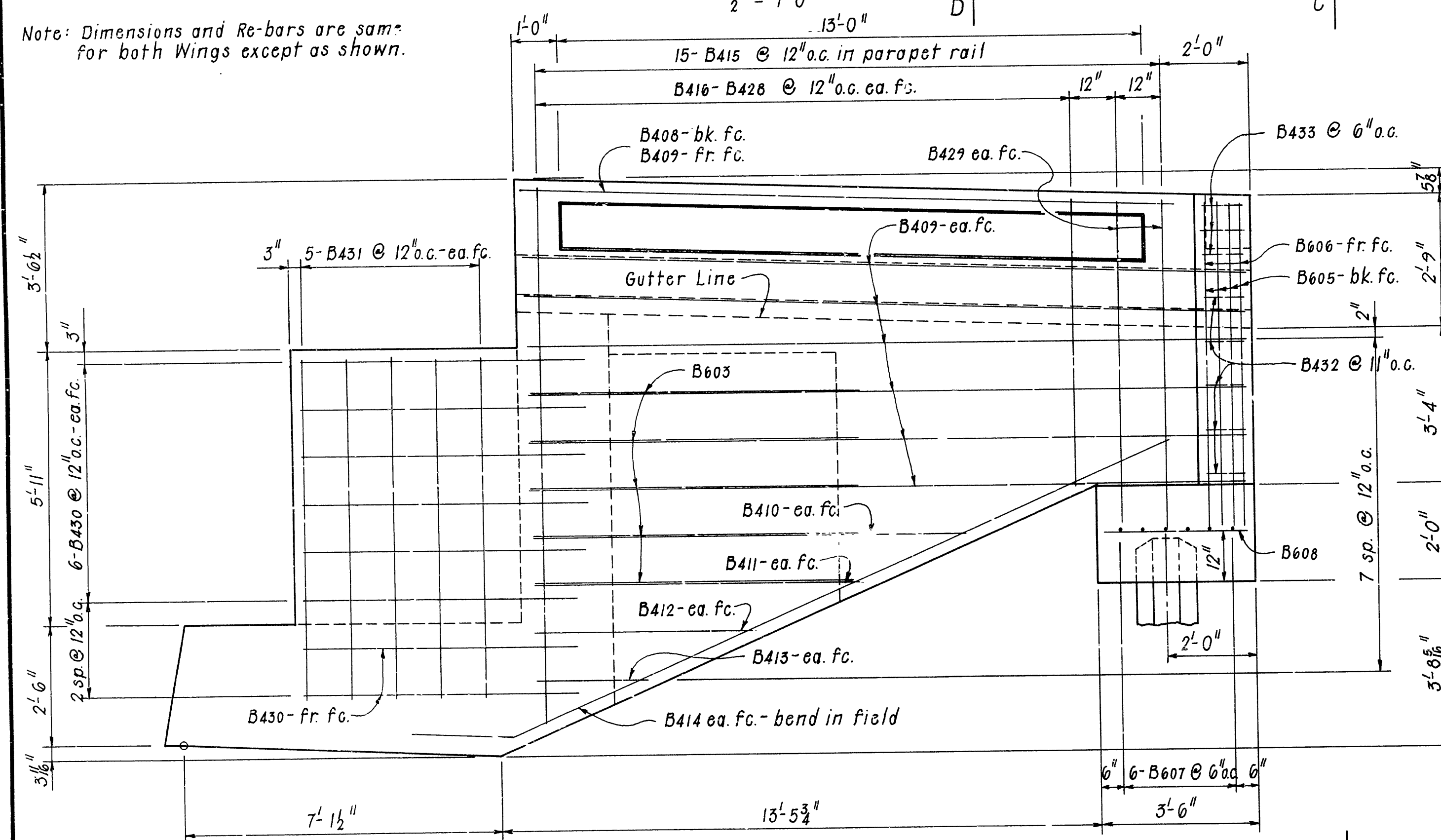
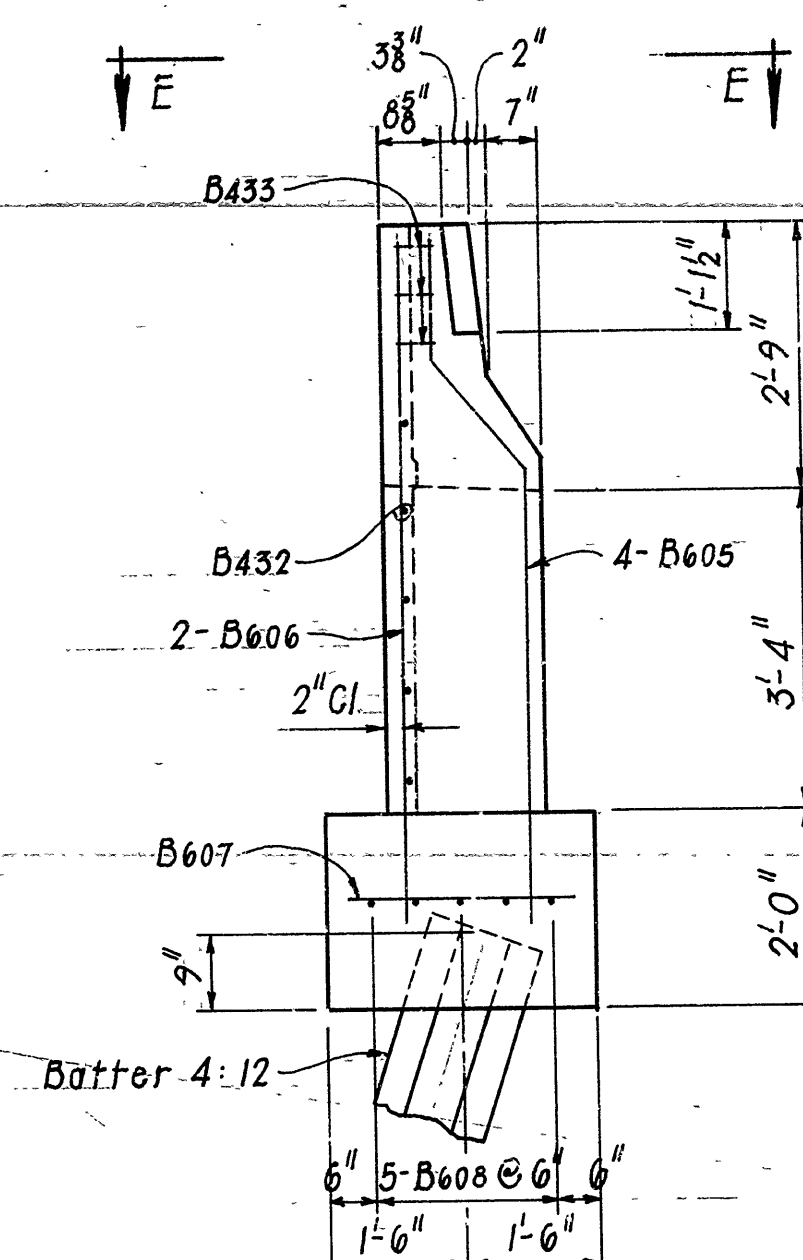
Verla Pinkerton
BRIDGE ENGINEER



V I E W A - A

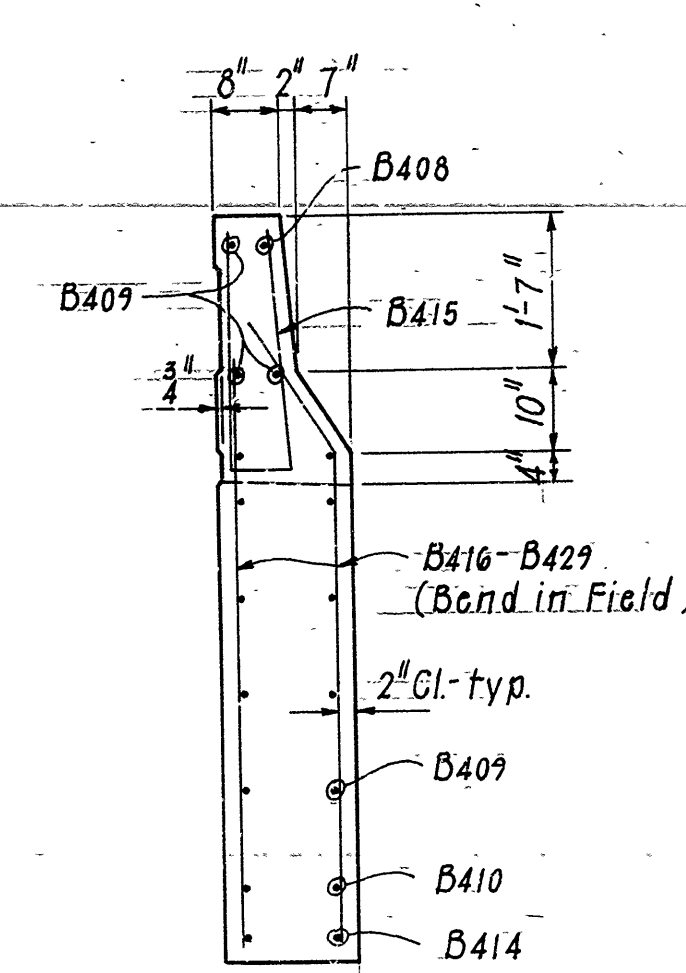
$2'' = 1'-0''$

Note: Dimensions and Re-bars are same for both Wings except as shown.

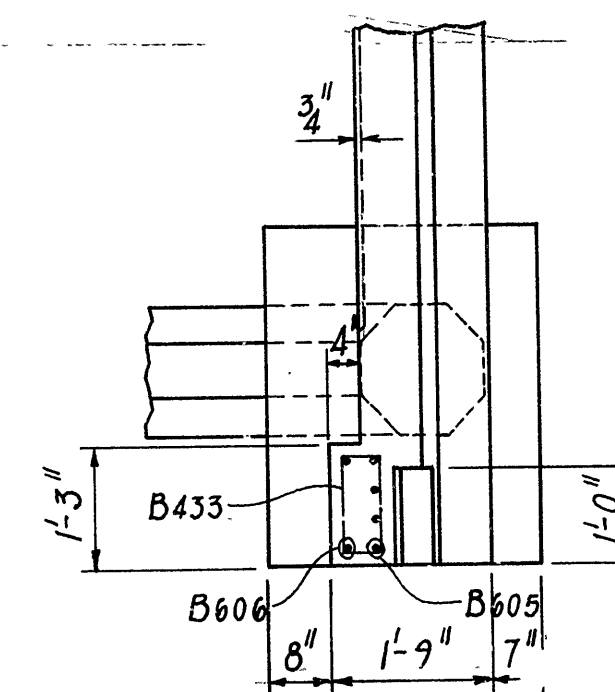

$$\frac{V \quad I \quad E \quad W \quad B-B}{I'' = I' - O''}$$


VIEW C-C
1/2" = 1'-0"
(Similar for both Wings)

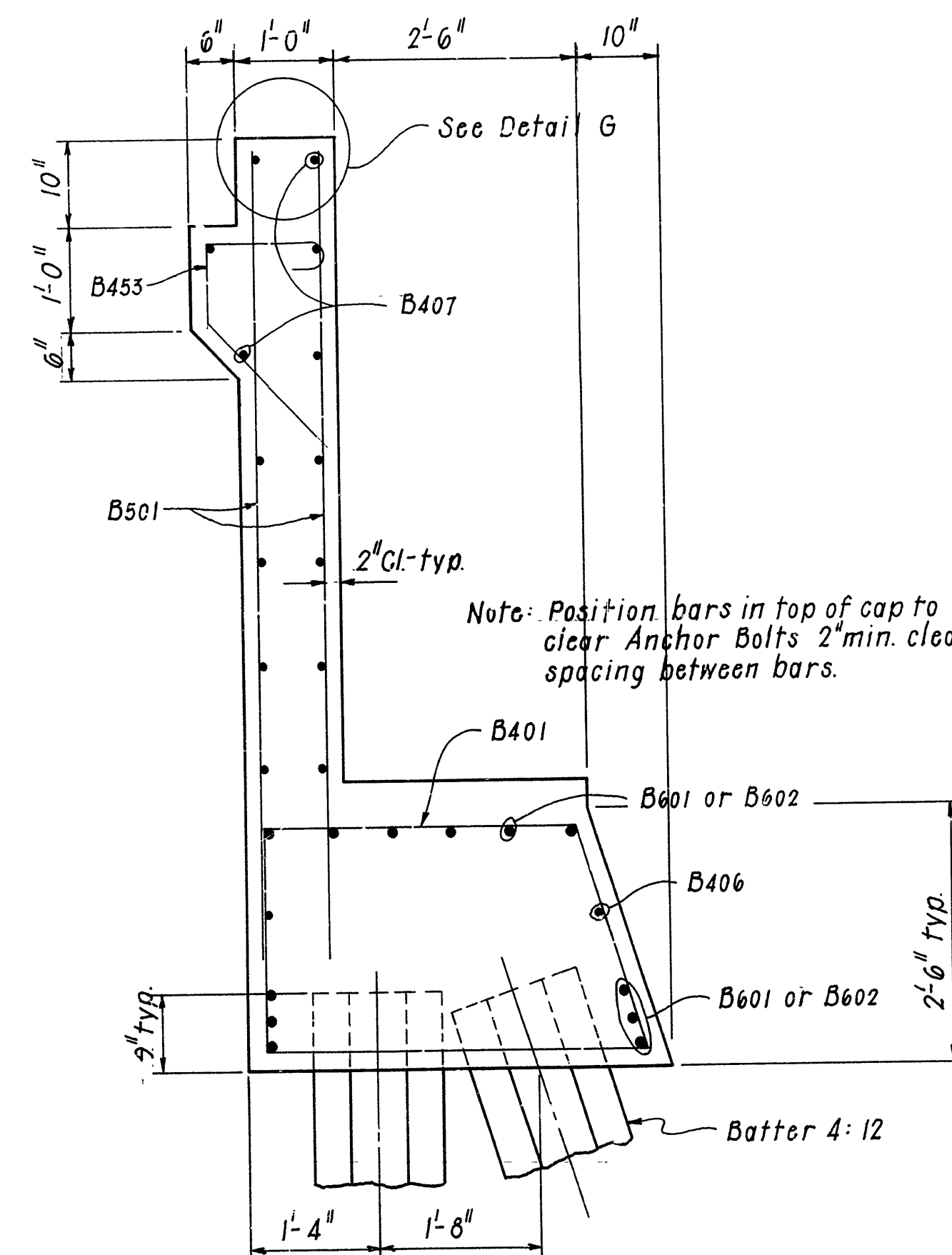
— Drain Attached to Curtain Wall - See
Detail on dwg. no. 27025.



SECTION D-D
 $\frac{1}{2}'' = 1'-0''$
 (Opposite Wing is similar)



V I E W E-E
1/2" = 1'-0"



SECTION F-F
3" = 1'-0"

B A R L I S T

| MK | No. Req'd. | Length | Pin Dia. | Bending Diagrams |
|--------------|------------|-----------------|----------|------------------|
| B601 | 12 | 35'-9" | Str. | |
| B602 | 12 | 57'-3" | Str. | |
| B603 | 5 | 7'-2" | 3 1/2" | |
| B604 | 6 | 15'-6" | Str. | |
| B605 | 8 | 7'-8" | 3 1/2" | |
| B606 | 4 | 7'-7" | Str. | |
| B607 | 12 | 2'-6" | Str. | |
| B608 | 10 | 3'-0" | Str. | |
| B501 | 226 | 8'-3" | Str. | |
| B401 | 92 | 12'-4" | 2" | |
| B402 | 42 | 7'-5" | 2" | |
| B403 | 1 | 18'-0" | 2" | |
| B404 | 1 | 21'-7" | 2" | |
| B405 | 6 | 2'-3" | Str. | |
| B406 | 6 | 31'-1" | Str. | |
| B407 | 42 | 31'-1" | Str. | |
| B408 | 1 | 15'-2" | Str. | |
| B409 | 13 | 16'-2" | Str. | |
| B410 | 2 | 10'-2" | Str. | |
| B411 | 2 | 7'-9" | Str. | |
| B412 | 2 | 5'-5" | Str. | |
| B413 | 2 | 3'-0" | Str. | |
| B414 | 2 | 18'-0" | Str. | |
| B415 | 32 | 5'-5" | 2" | |
| B416 to B428 | 2 ea. | 10'-8" to 5'-3" | Str. | |
| B429 | 8 | 6'-6" | Str. | |
| B430 | 14 | 6'-7" | Str. | |
| B431 | 10 | 7'-5" | Str. | |
| B432 | 10 | 1'-0" | Str. | |
| B433 | 6 | 3'-5" | 2" | |
| B434 | 1 | 16'-5" | Str. | |
| B435 | 13 | 17'-5" | Str. | |
| B436 | 2 | 12'-2" | Str. | |
| B437 | 2 | 10'-5" | Str. | |
| B438 | 2 | 8'-9" | Str. | |
| B439 | 2 | 7'-1" | Str. | |
| B440 | 2 | 19'-4" | Str. | |
| B441 to B449 | 2 ea. | 5'-5" to 10'-2" | Str. | |
| B450 | 12 | 10'-8" | Str. | |
| B451 | 6 | 6'-9" | Str. | |
| B452 | 10 | 4'-2" | Str. | |
| B453 | 101 | 3'-11" | 2" | |
| B454 | 3 | 4'-6" | Str. | |
| B455 | 6 | 6'-0" | Str. | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |

Dimens. are out to out of bars.

(SHEET 2 OF 2)

DETAILS OF END BENT 1
M.T. RAMP BRIDGE OVER I-40

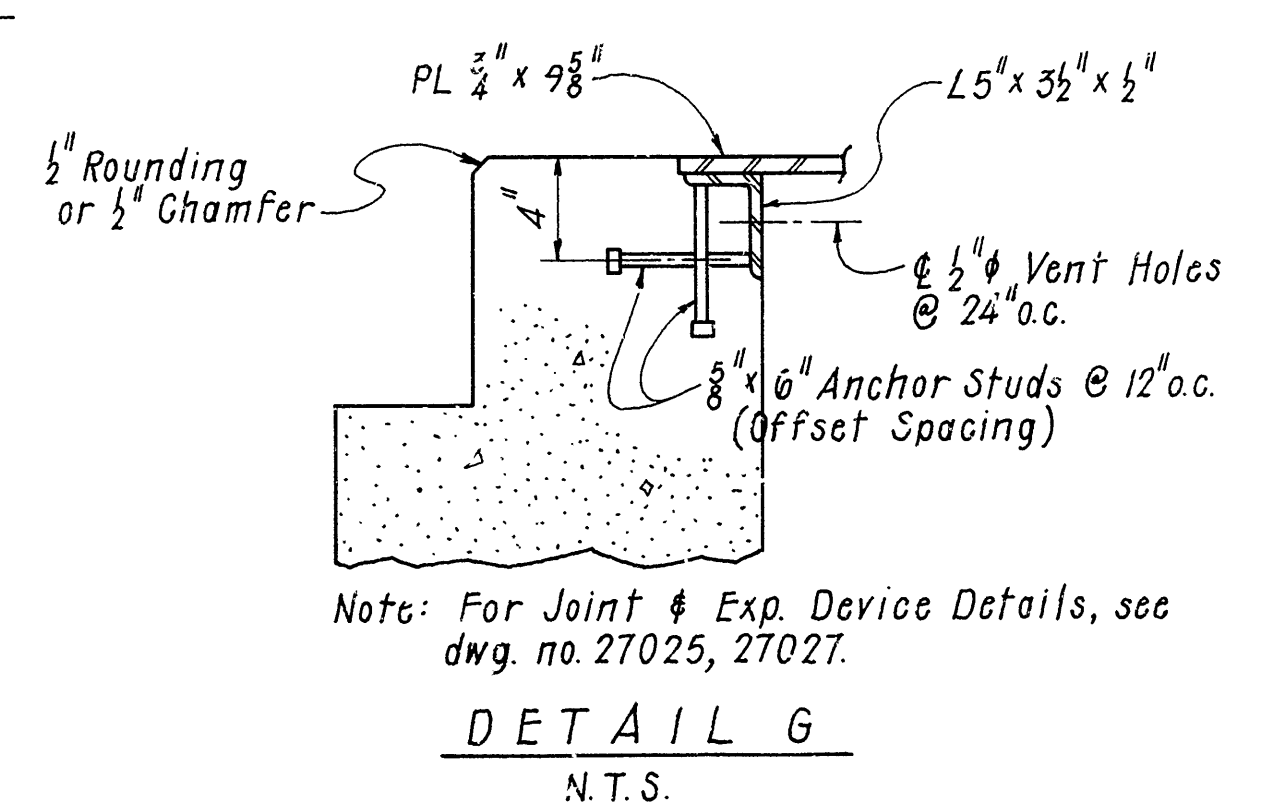
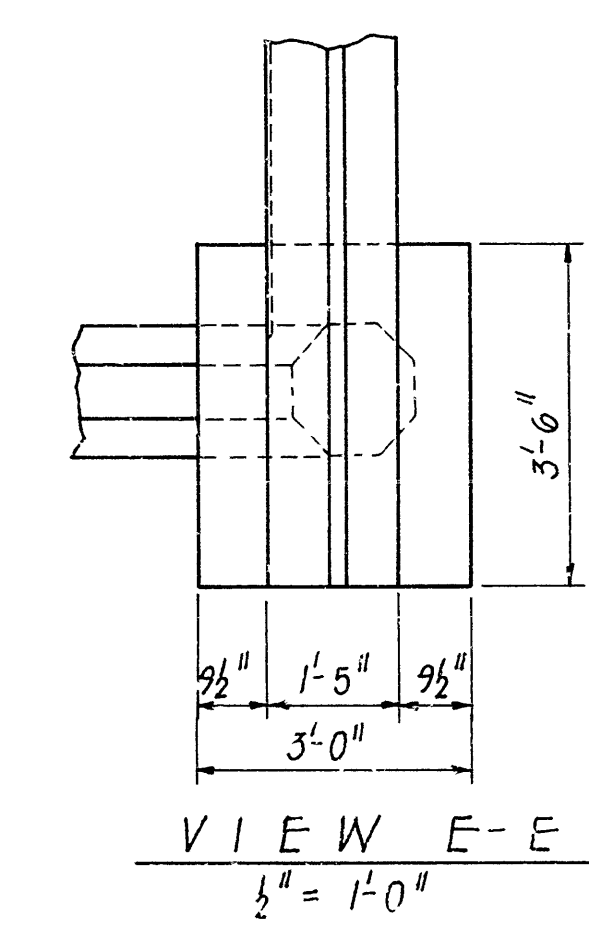
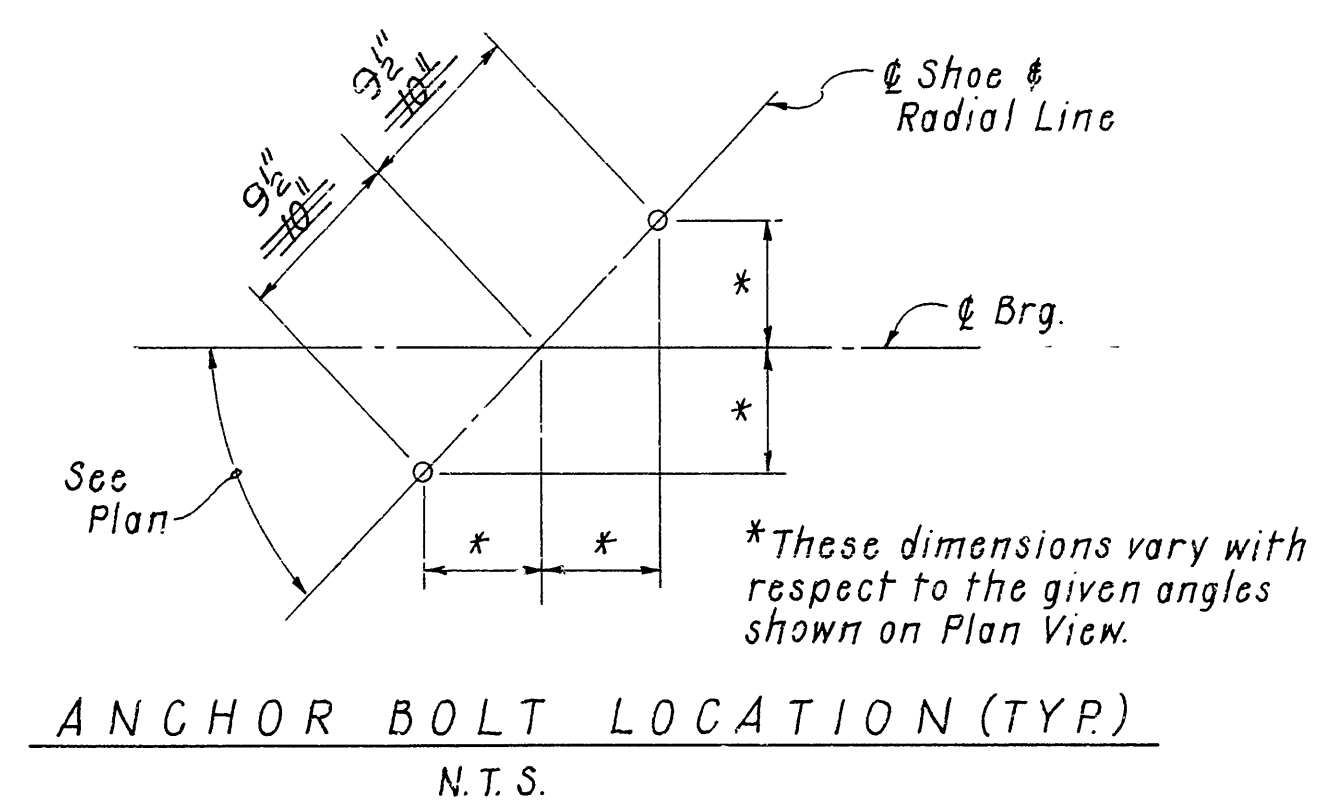
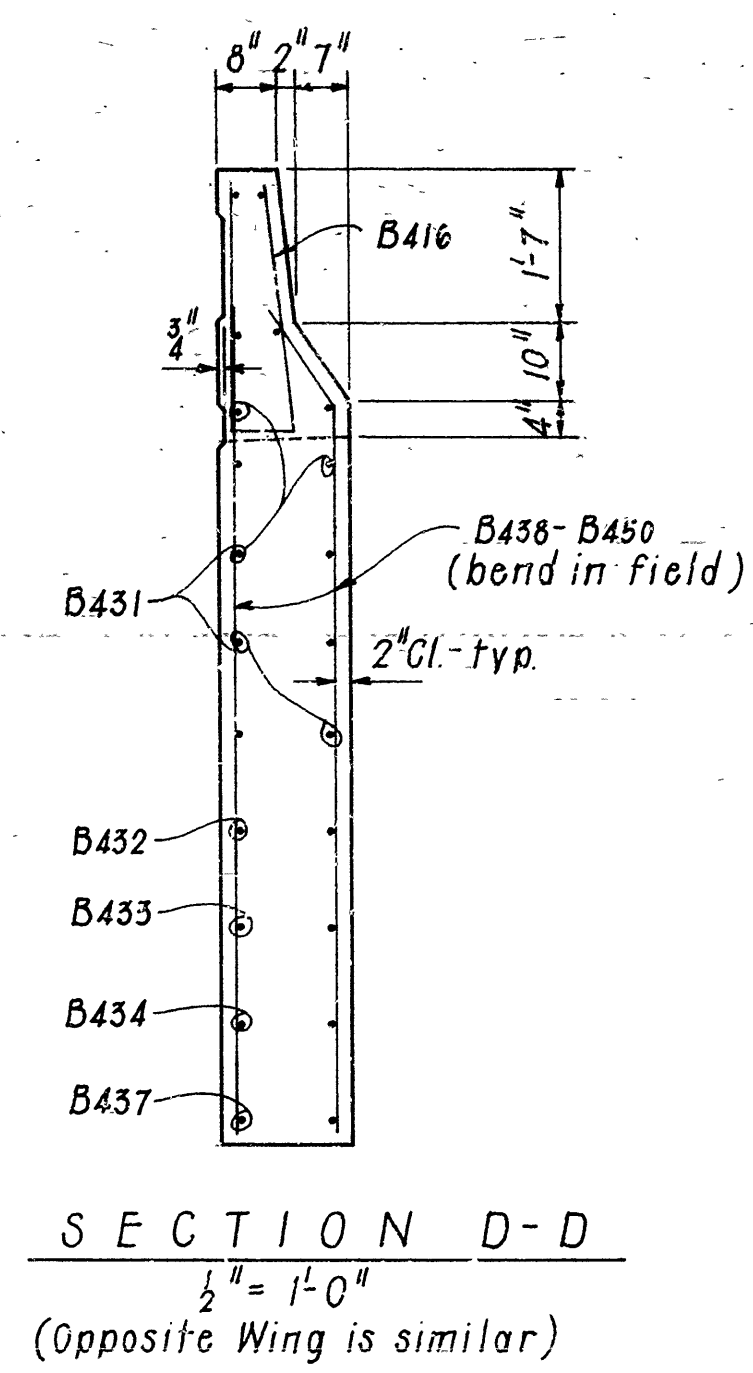
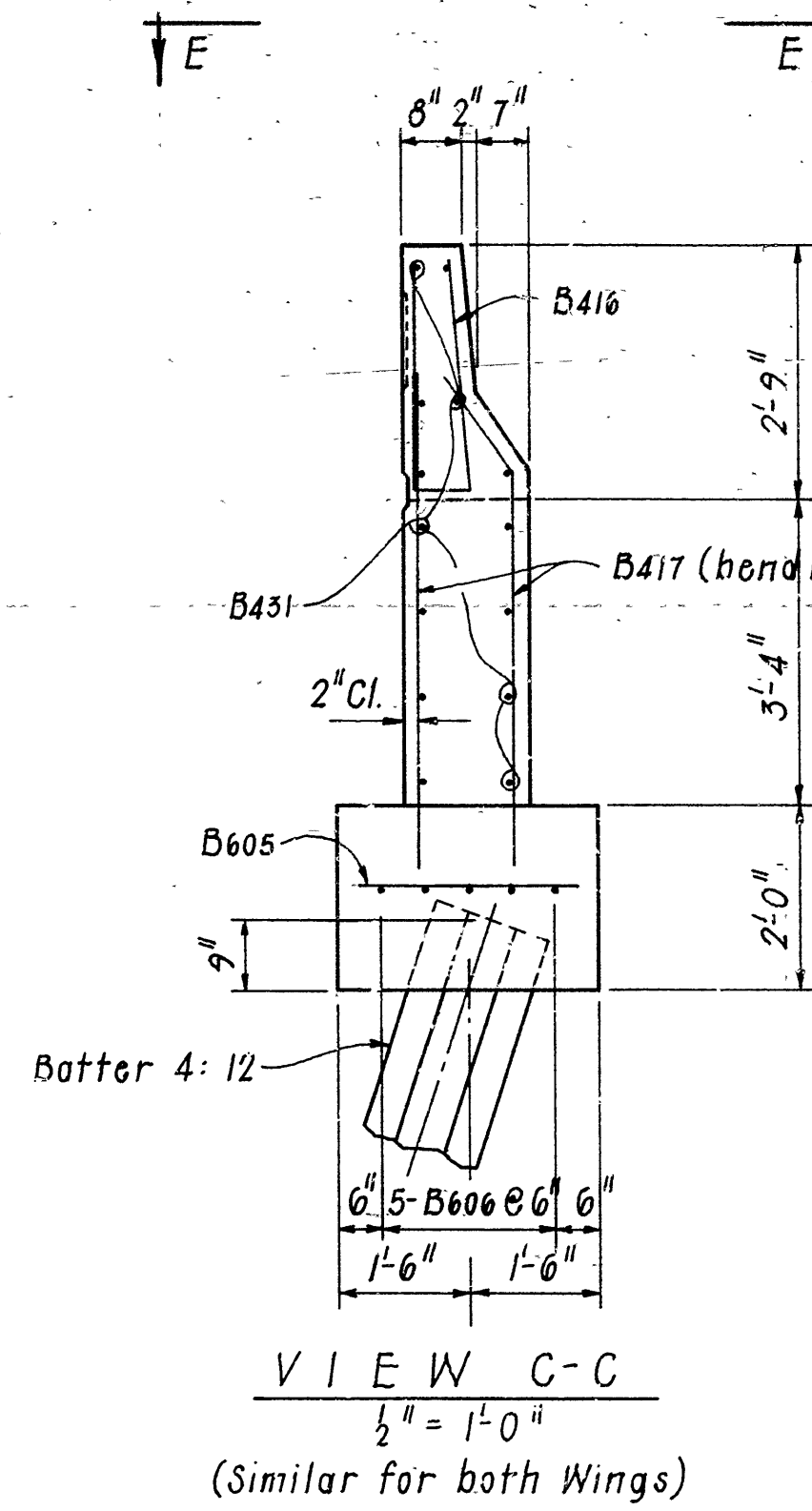
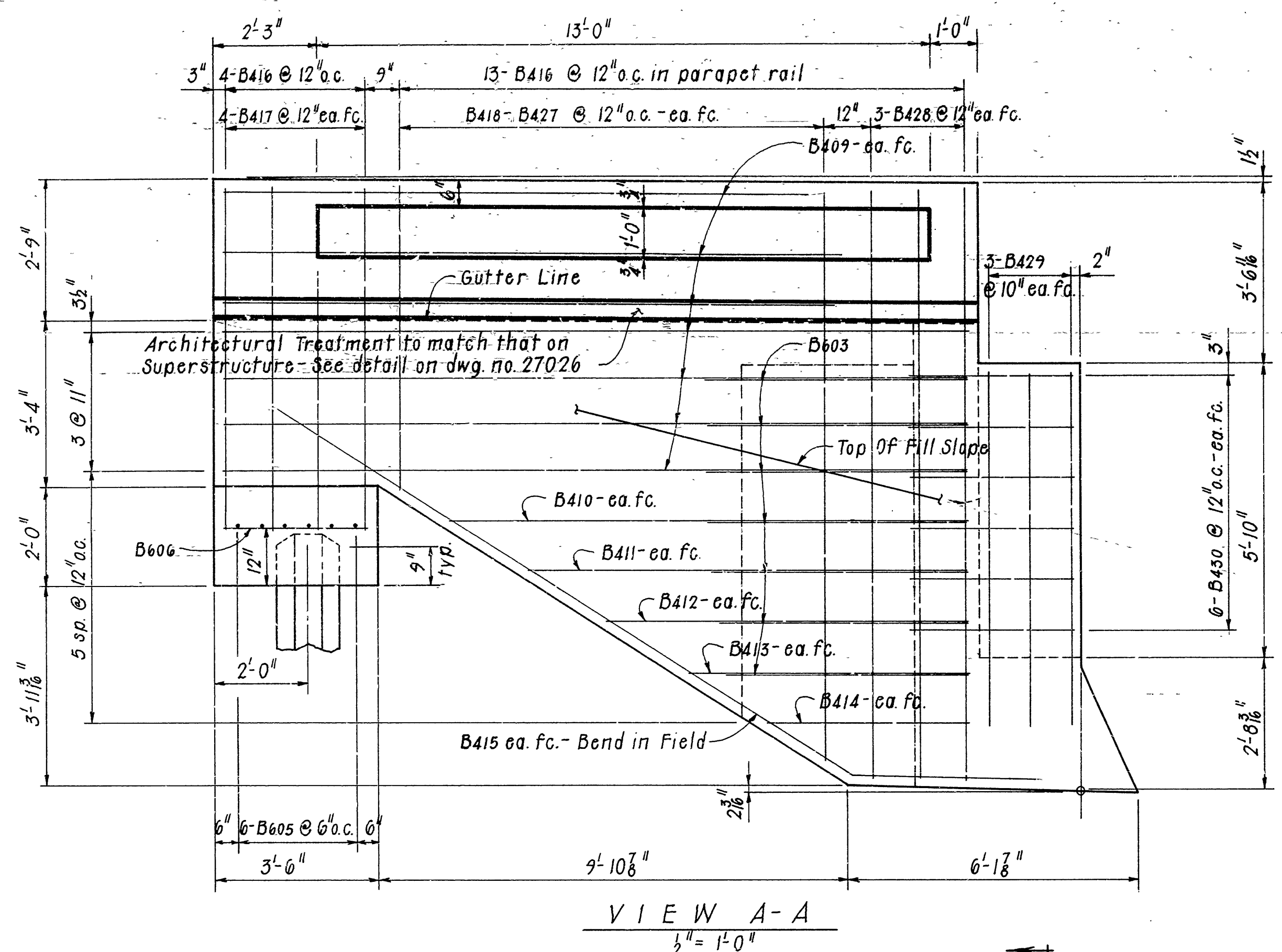
ROUTE SEC.
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

DRAWN BY: W. Maj. DATE: 11-27-84
CHECKED BY: HJD DATE: 1-15-85 SCALE: As Shown
DESIGNED BY: HJD DATE: 1-15-85

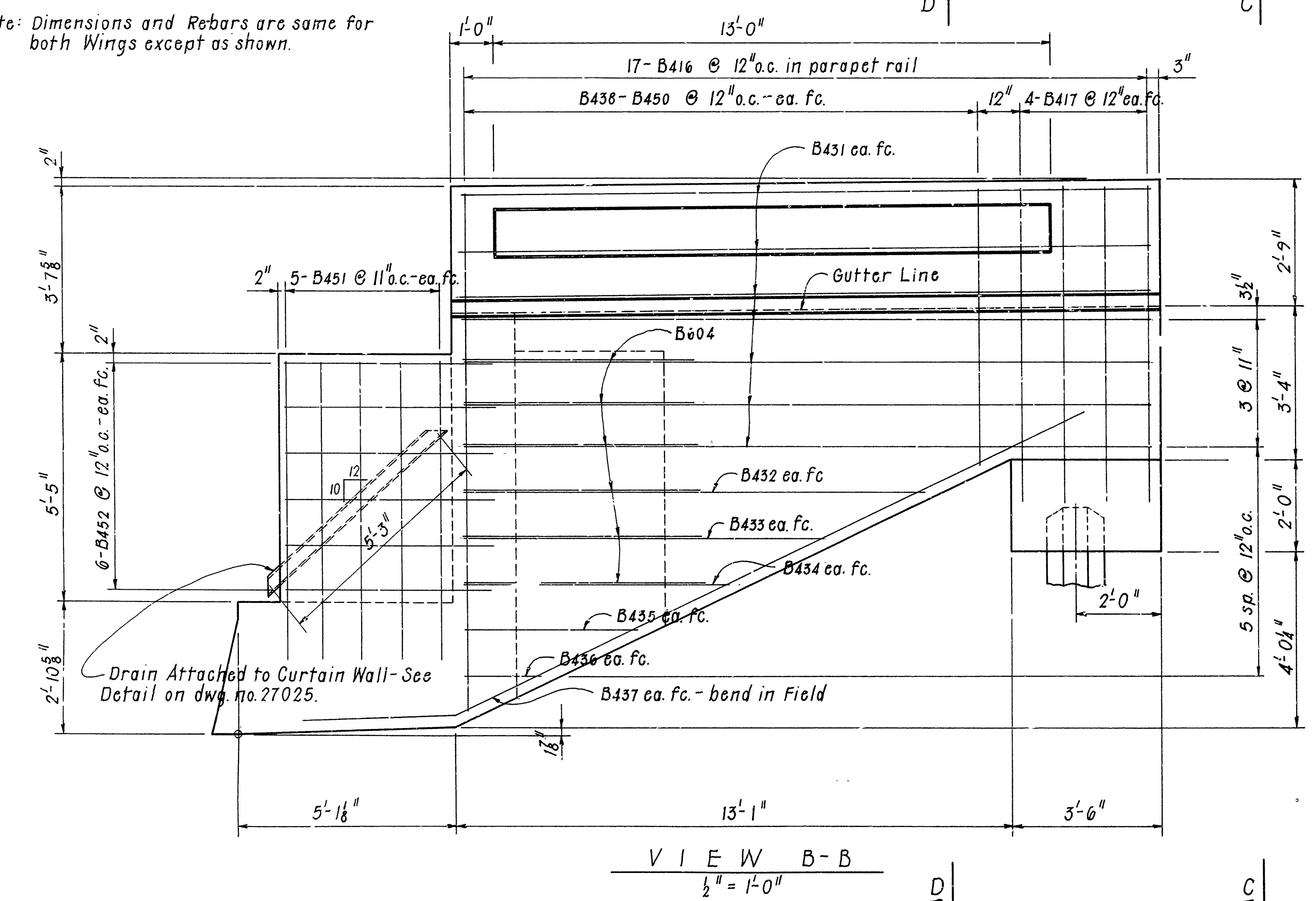
BRIDGE NO. 6102 DRAWING NO. 27014

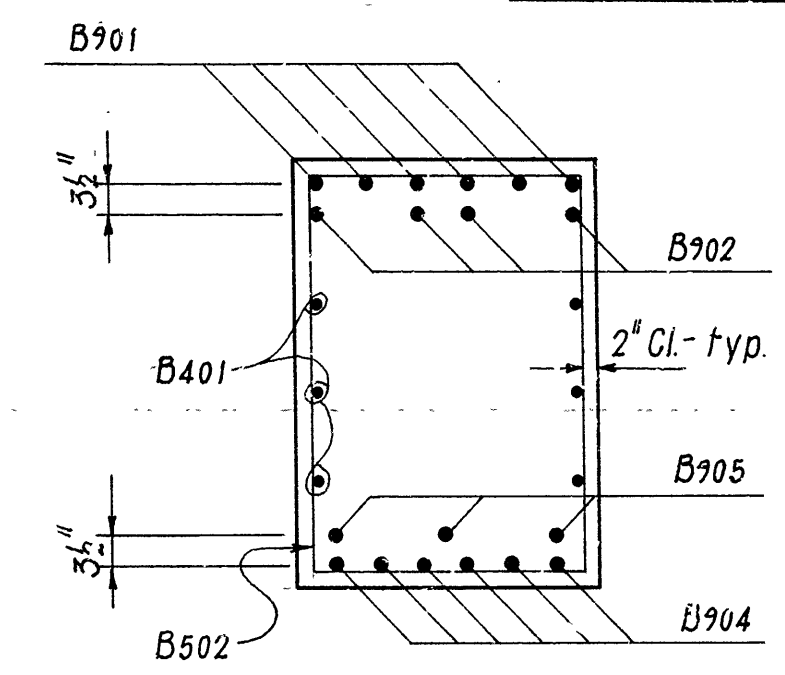
Terrel Pinkerton
BRIDGE ENGINEER

| RAIL | FILE | RAIL | FILE | FED. ROAD NO. | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|--------|-------------|------|------|---------------|--------|--------------------|-----------|--------------|
| 6-7-85 | SC1-6-10-85 | | | 6 | ARK. | | | |
| | | | | JOB NO. | 110065 | 46 | 177 | |
| | | | | | 6102 | BENT | 27016 | |



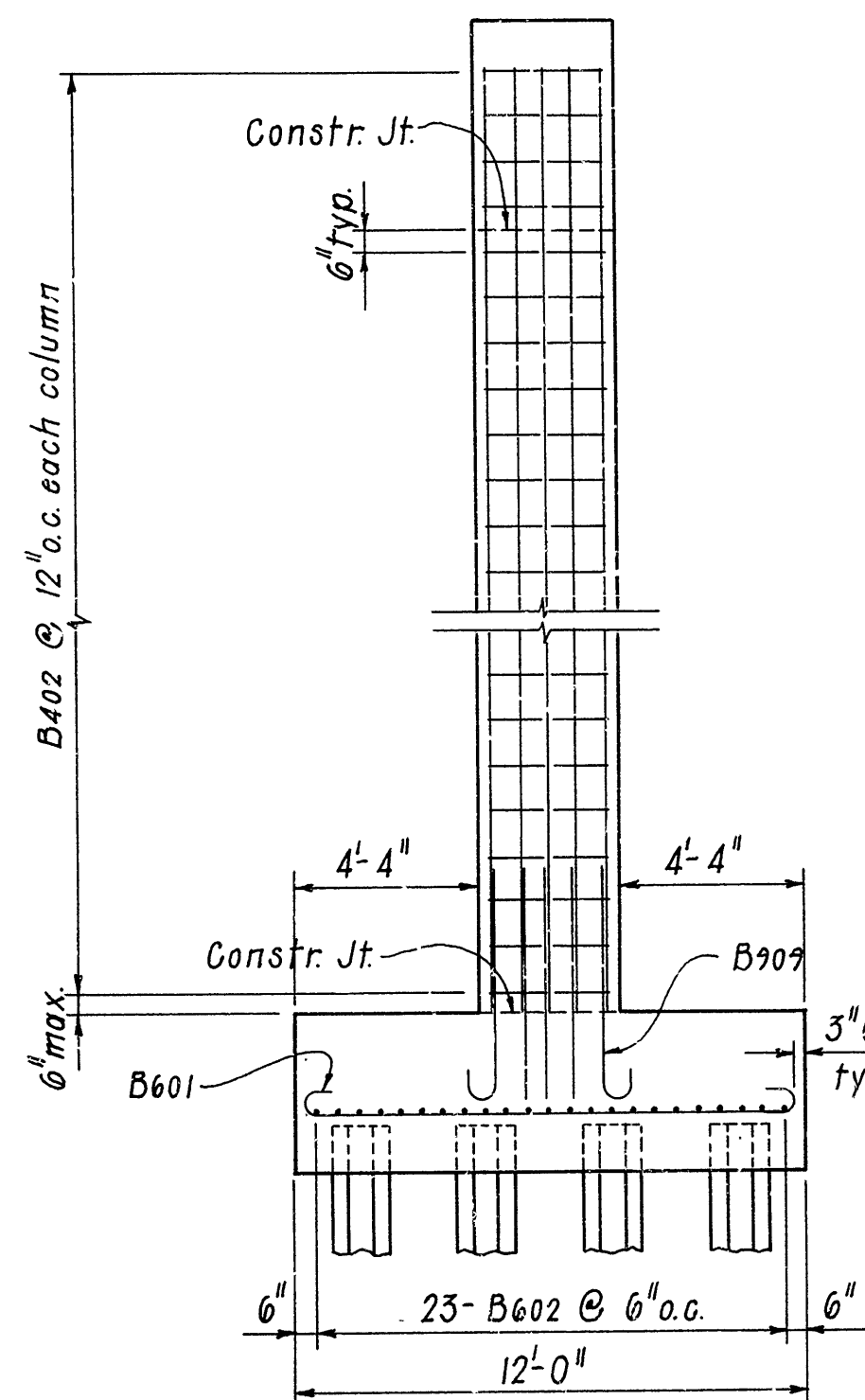
Note: Dimensions and Rebars are same for both Wings except as shown.



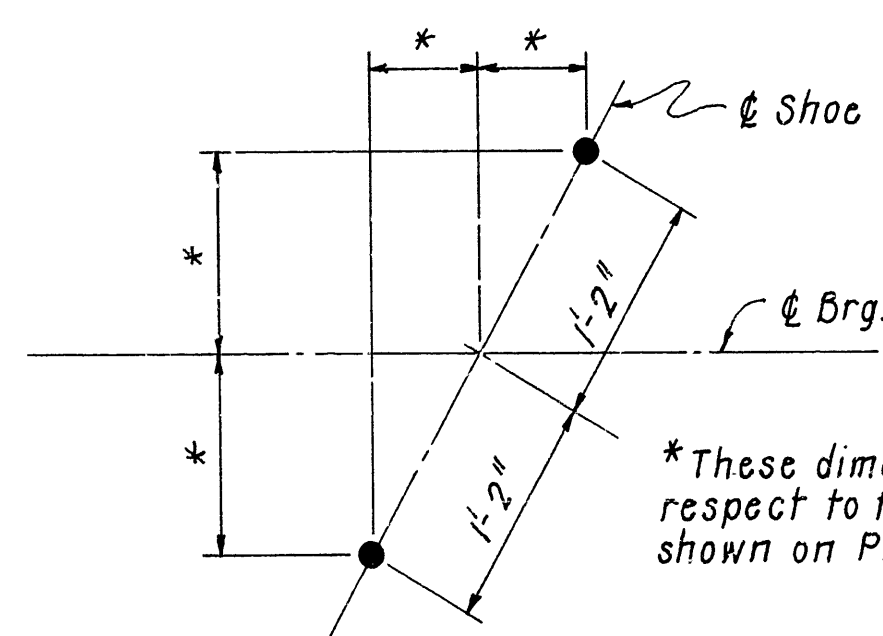


SECTION C-C

BAR LIST

[illegible]

SECTION B-B



ANCHOR BOLT LOCATION (TYP)
N.T.S.

ALL CONCRETE TO BE CLASS "S" AND TO BE POURED IN THE DRY.

ALL CONCRETE SHALL HAVE A MINIMUM 28 DAY COMPRESSIVE STRENGTH
 $f'_c = 3500$ PSI. ALL EXPOSED CORNERS TO BE CHAMFERED $3/4$ INCH.

REINFORCING STEEL TO BE ASTM A615 OR A617, GRADE 60. SEE LAYOUT
FOR PILE DATA AND GENERAL NOTES.

IN GENERAL, ALL CONSTRUCTION JOINTS IN BENTS SHALL BE PROVIDED
WITH KEYS NOT LESS THAN $1-1/2$ INCH HIGH COVERING THE MIDDLE
THIRD OF BOTH DIMENSIONS.

LIVE LOAD: HS 20 ~~CL~~ MILITARY

DESIGN SPECIFICATIONS: AASHTO 1983 WITH CURRENT INTERIM SPECIFICATIONS.

METHOD OF DESIGN: LOAD FACTOR

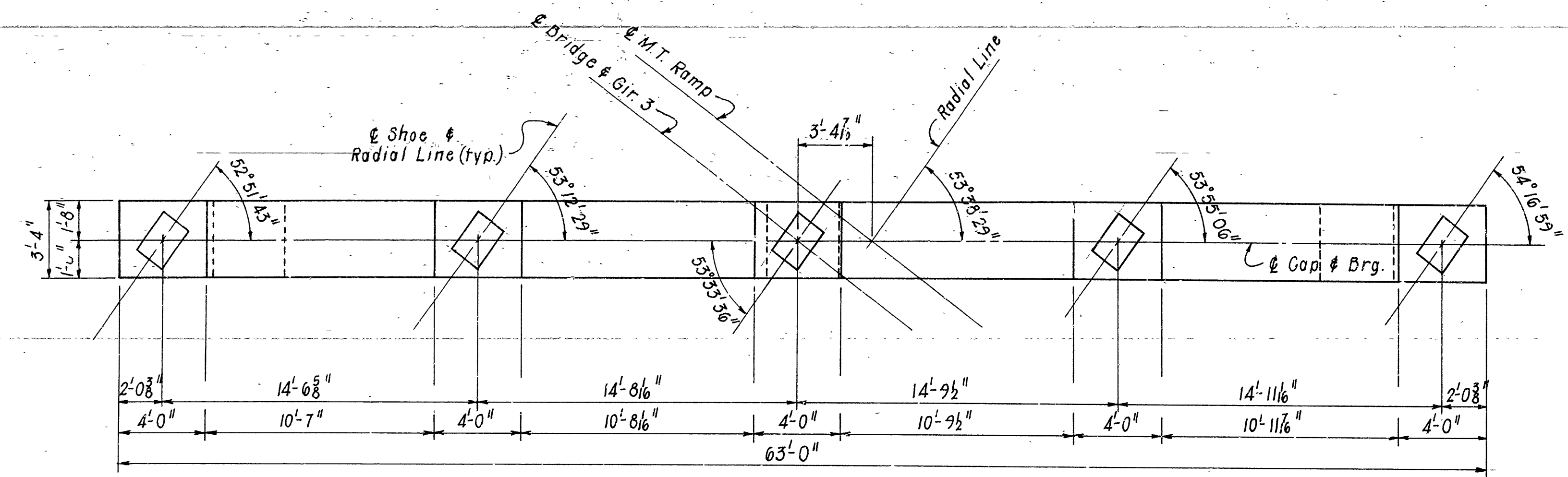
ROUTE SEC.
ARKANSAS STATE HIGHWAY COMMISSION

DRAWN BY: H. Maj. DATE: 11-5-84
CHECKED BY: HJD DATE: 1-15-85
DESIGNED BY: HJD DATE: 1-15-85

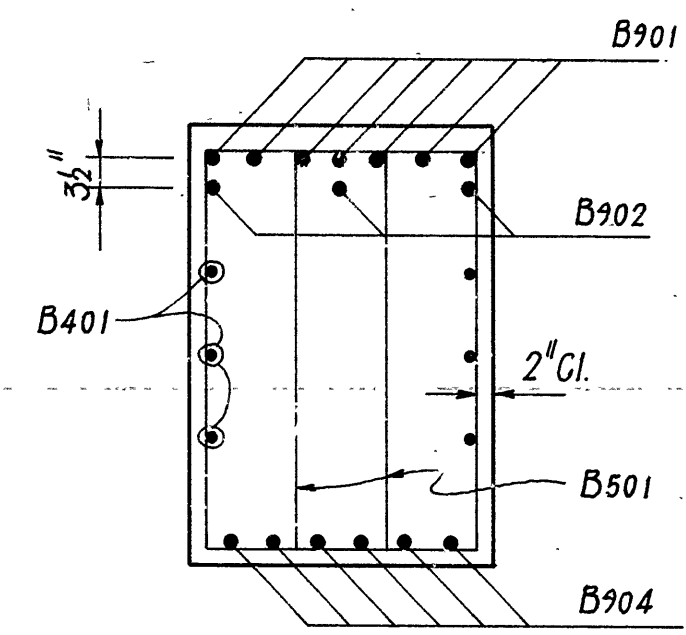
SCALE: $\frac{1}{4}'' = 1'-0''$ or as shown

DRAWING NO. 27017

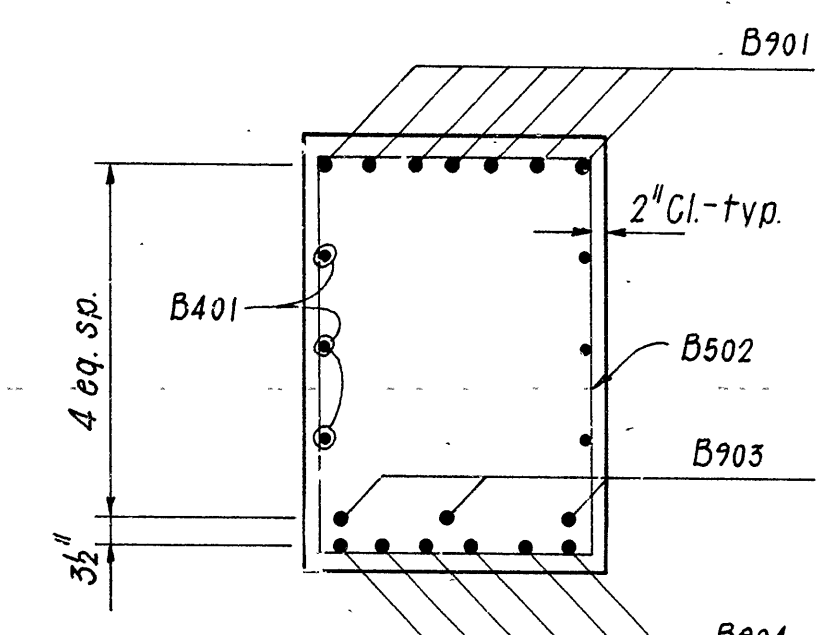
| DATE | REVISION | DATE | REVISION | DATE | REVISION | DATE | REVISION | FED. ROAD NO. | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|------|----------|------|----------|------|----------|------|----------|---------------|--------|--------------------|-----------|--------------|
| | | | | | | | | 6 | ARK. | | | |
| | | | | | | | | JOB NO. | 110065 | 48 | 177 | |
| | | | | | | | | 6102 | BENT | 27018 | | |



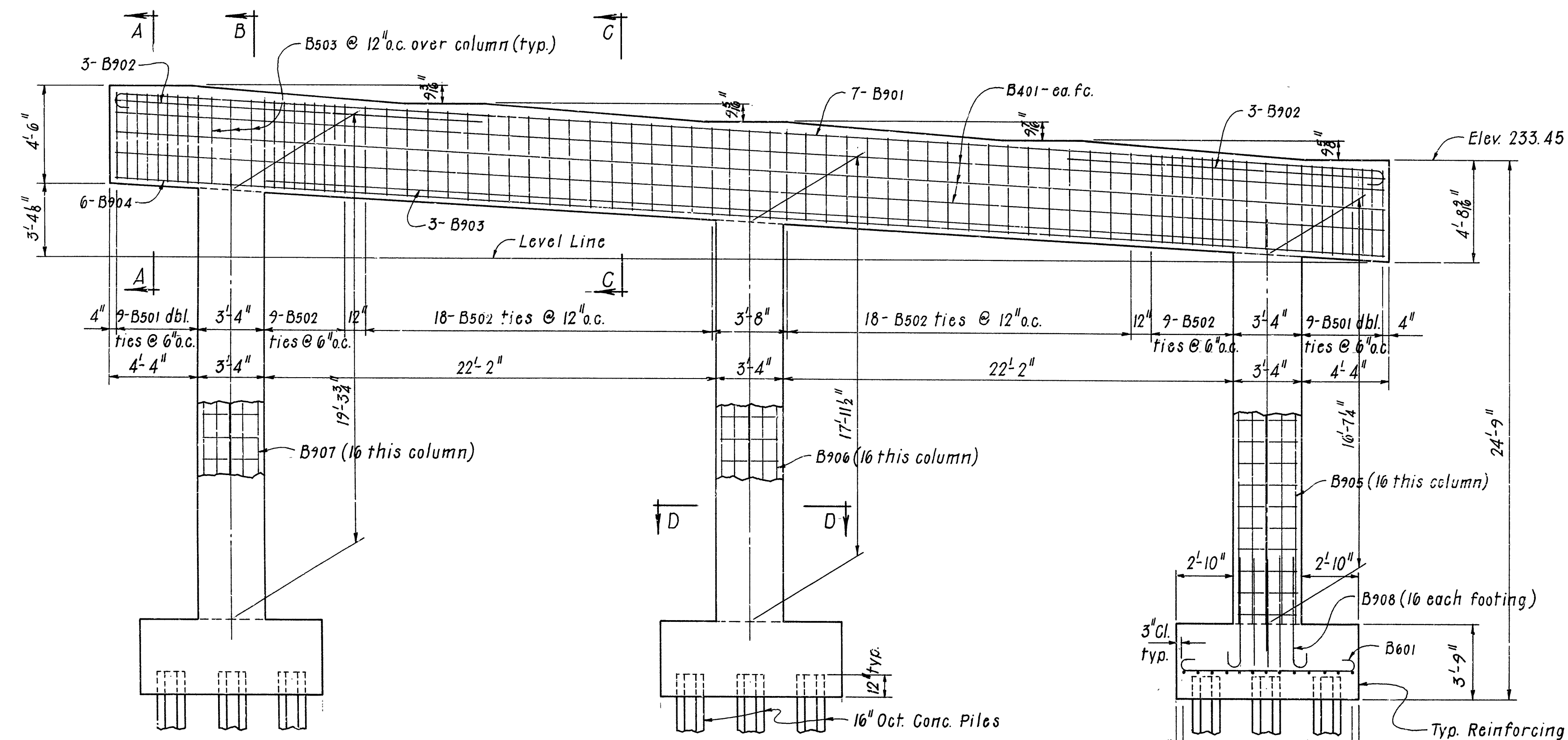
PLAN
(Looking Forward)



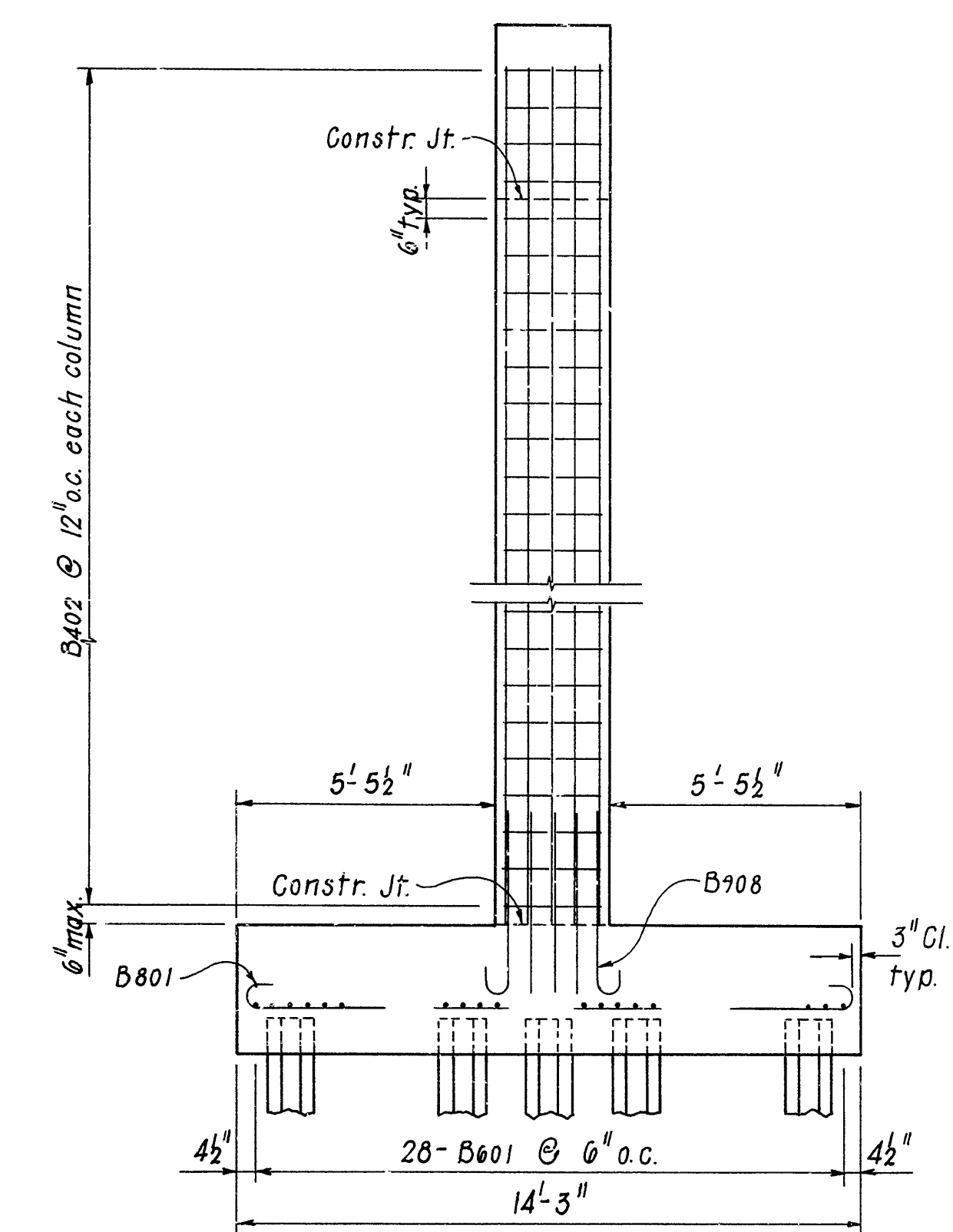
SECTION A-A
1/2" = 1'-0"



SECTION C-C
1/2" = 1'-0"



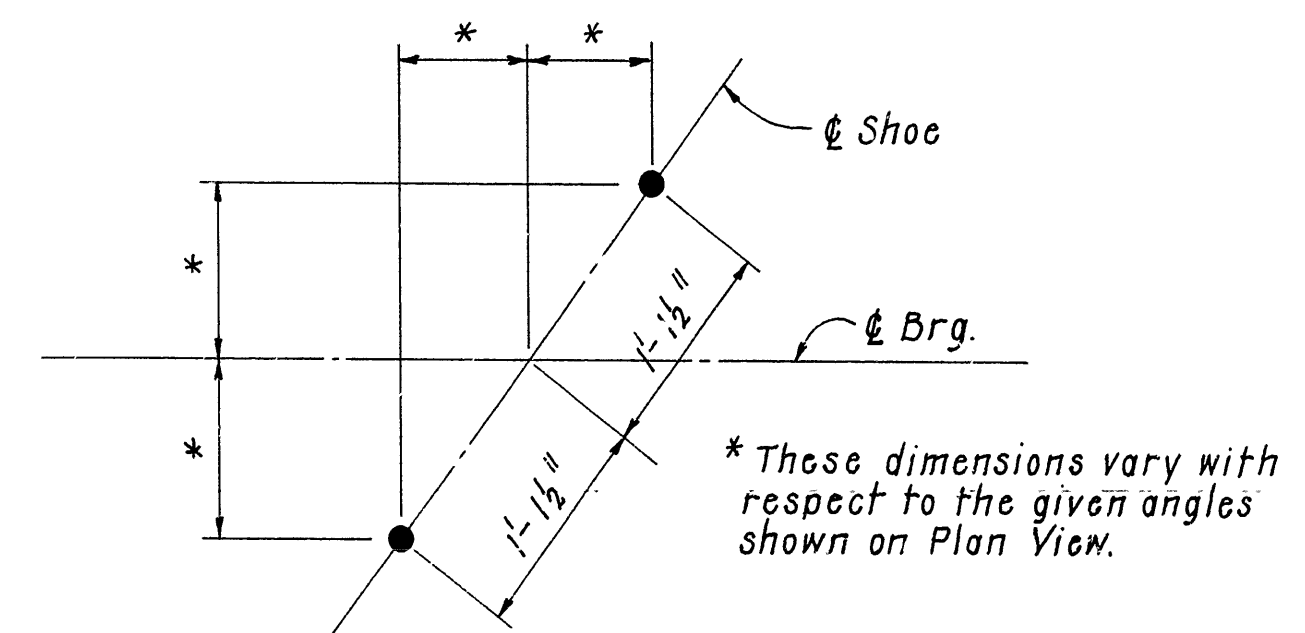
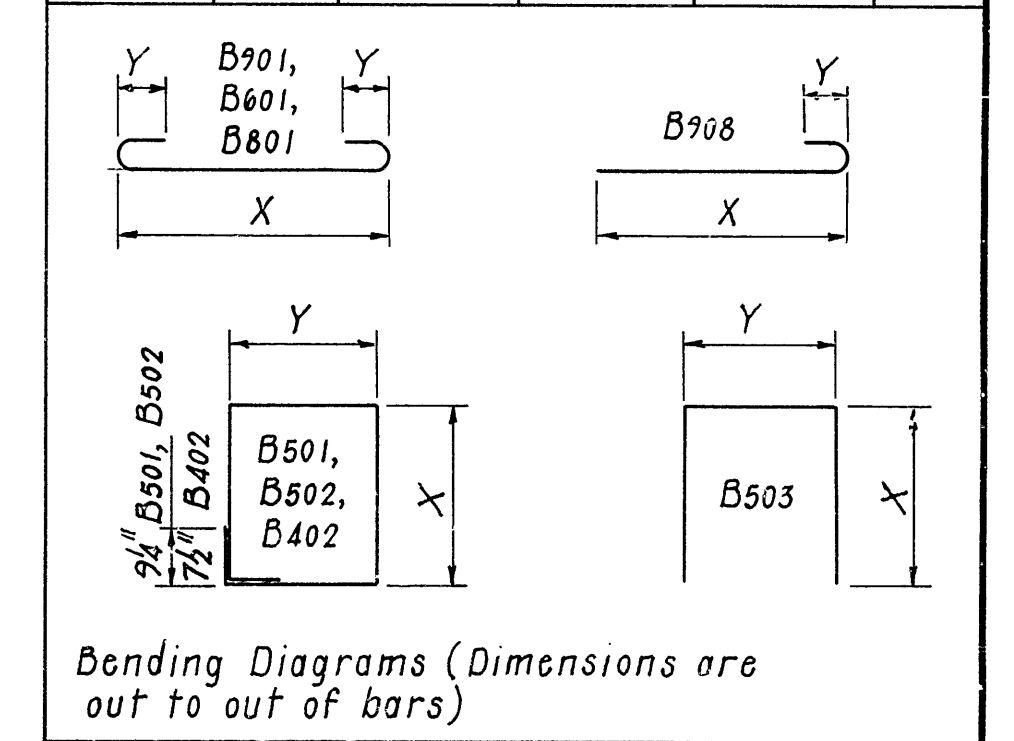
ELEVATION



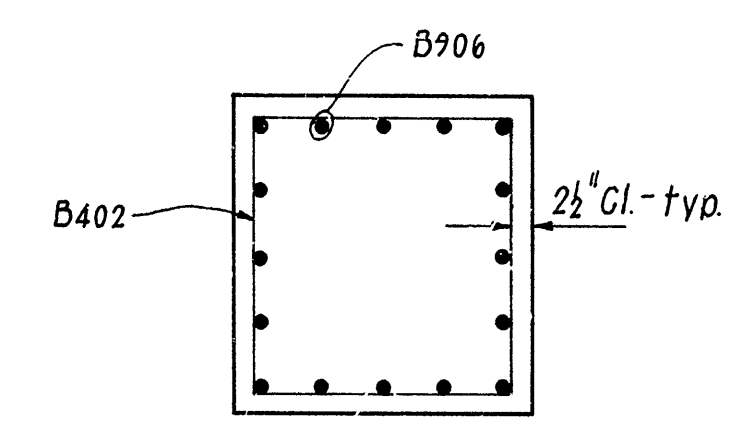
SECTION B-B
N.T.S.

BAR LIST

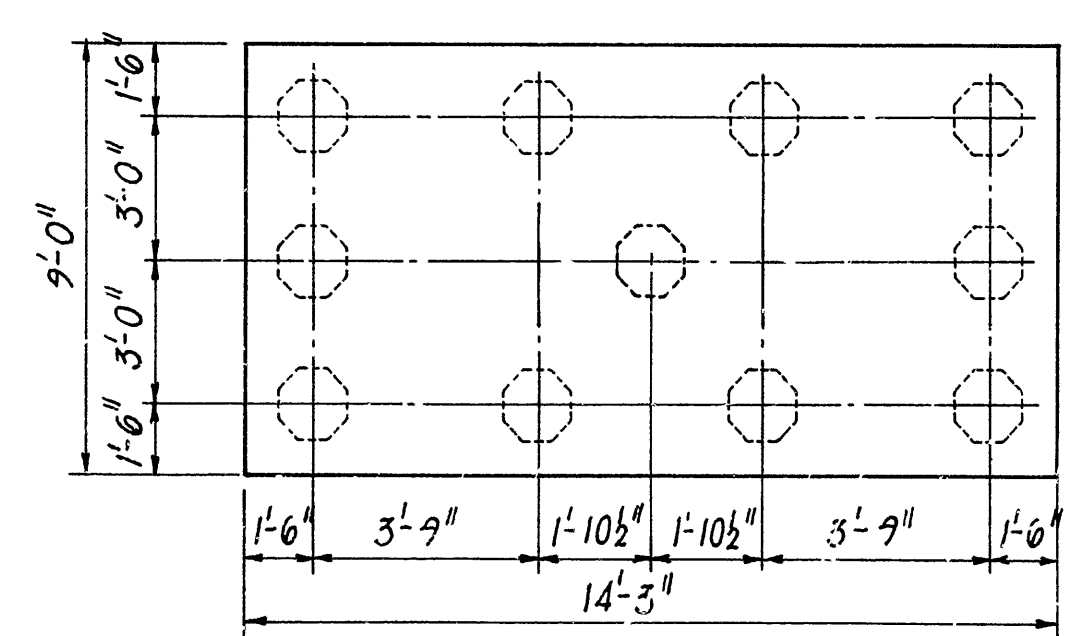
| MK | No. Req'd | Length | X | Y | Pin Dia. |
|------|-----------|---------|--------|--------|----------|
| B801 | 39 | 15'-7" | 13'-9" | 8" | 6" |
| B901 | 7 | 65'-2" | 62'-8" | 10" | 9" |
| B902 | 3 | 19'-10" | | | Str. |
| B903 | 6 | 47'-8" | | | Str. |
| B904 | 6 | 62'-8" | | | Str. |
| B905 | 16 | 20'-1" | | | Str. |
| B906 | 16 | 21'-6" | | | Str. |
| B907 | 16 | 22'-10" | | | Str. |
| B908 | 48 | 9'-3" | 8'-0" | 10" | 9" |
| B601 | 84 | 9'-10" | 8'-6" | 6" | 4 1/2" |
| B501 | 36 | 13'-4" | 4'-2" | 2'-0" | 2 1/2" |
| B502 | 54 | 15'-4" | 4'-2" | 3'-0" | 2 1/2" |
| B503 | 9 | 11'-2" | 4'-2" | 3'-0" | 2 1/2" |
| B401 | 12 | 32'-2" | 2'-11" | 2'-11" | Str. |
| B402 | 67 | 12'-6" | 2'-11" | 2'-11" | 2" |



ANCHOR BOLT LOCATION (TYP.)
N.T.S.



SECTION D-D
1/2" = 1'-0"

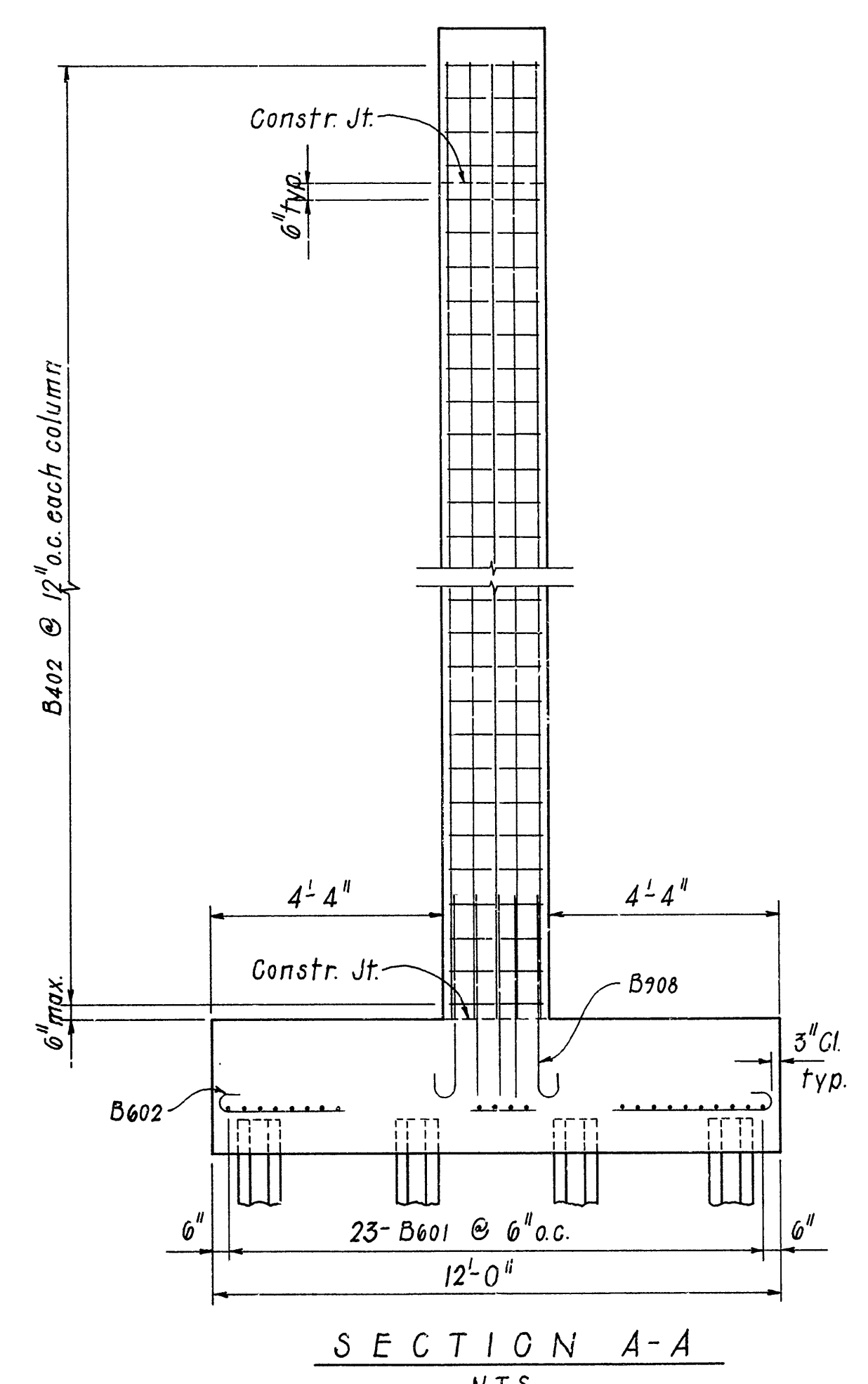
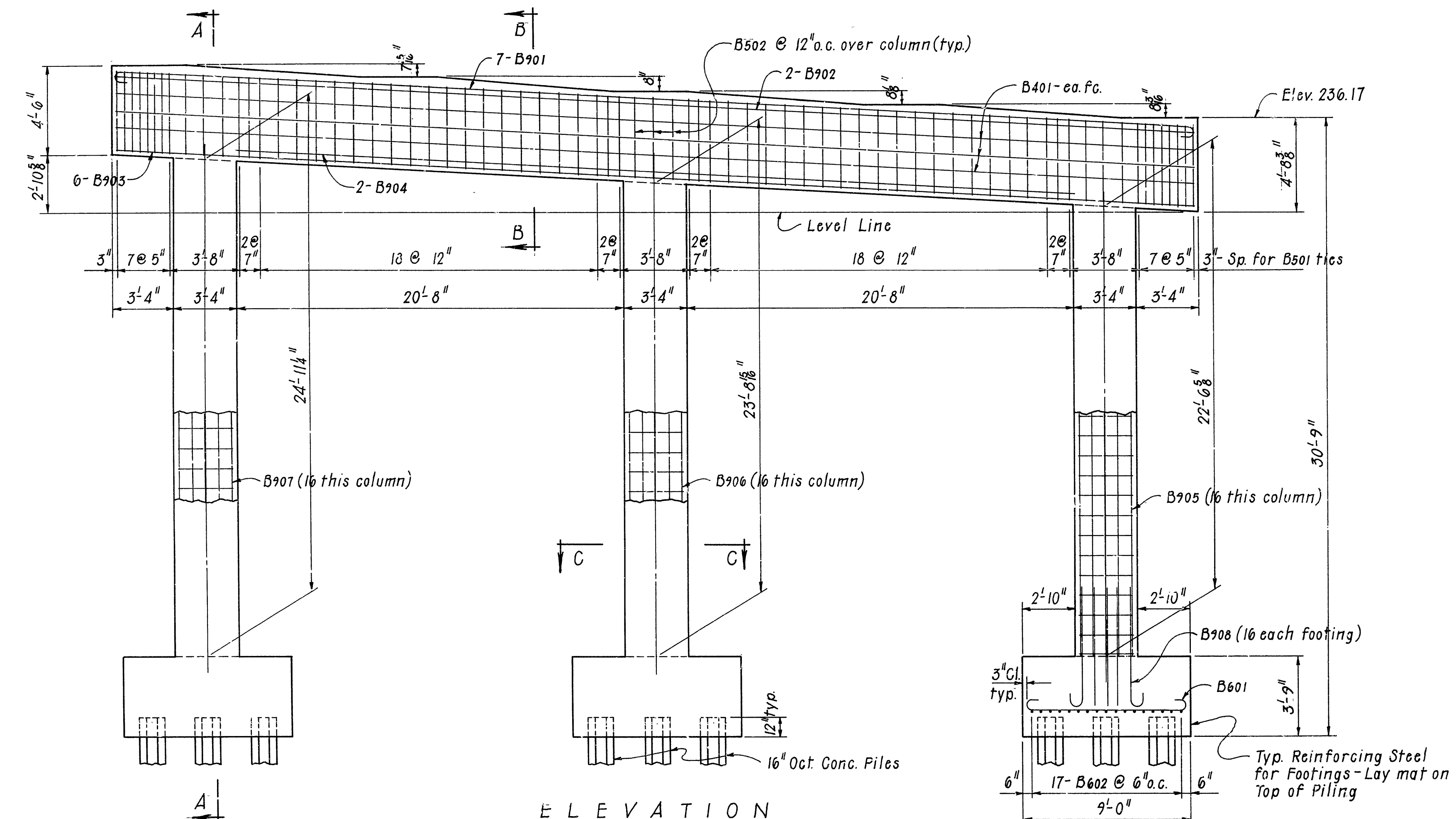
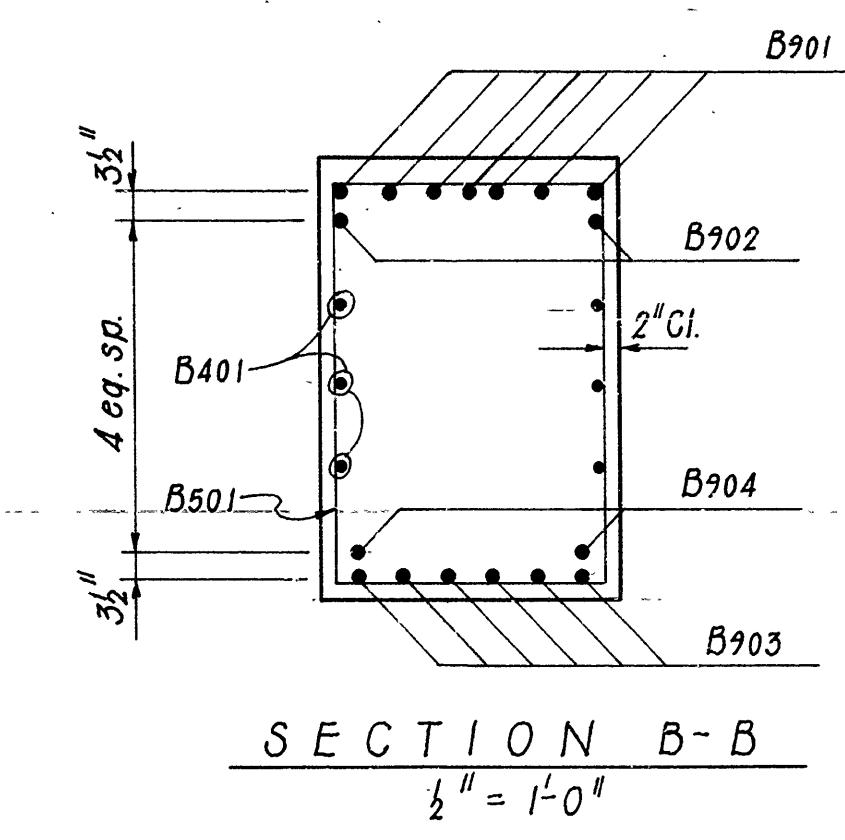
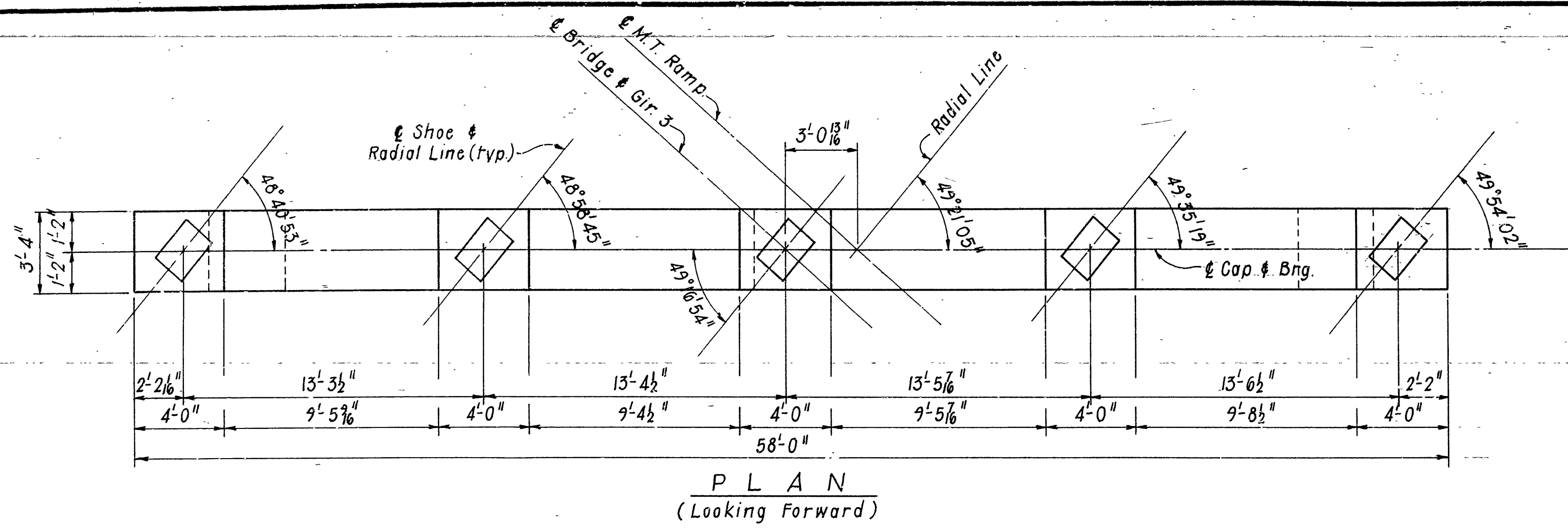


FOOTING PLAN
N.T.S.

NOTE: For General Notes see dwg. no. 27017.

DETAILS OF BENT 3
M.T. RAMP BRIDGE OVER I-40
ROUTE SEC.
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.
DRAWN BY: H.A.J. DATE: 11-7-84
CHECKED BY: H.J.D. DATE: 1-15-85
DESIGNED BY: H.J.D. DATE: 1-15-85
SCALE: 1/4" = 1'-0" or as shown
BRIDGE NO. 6102 DRAWING NO. 27018

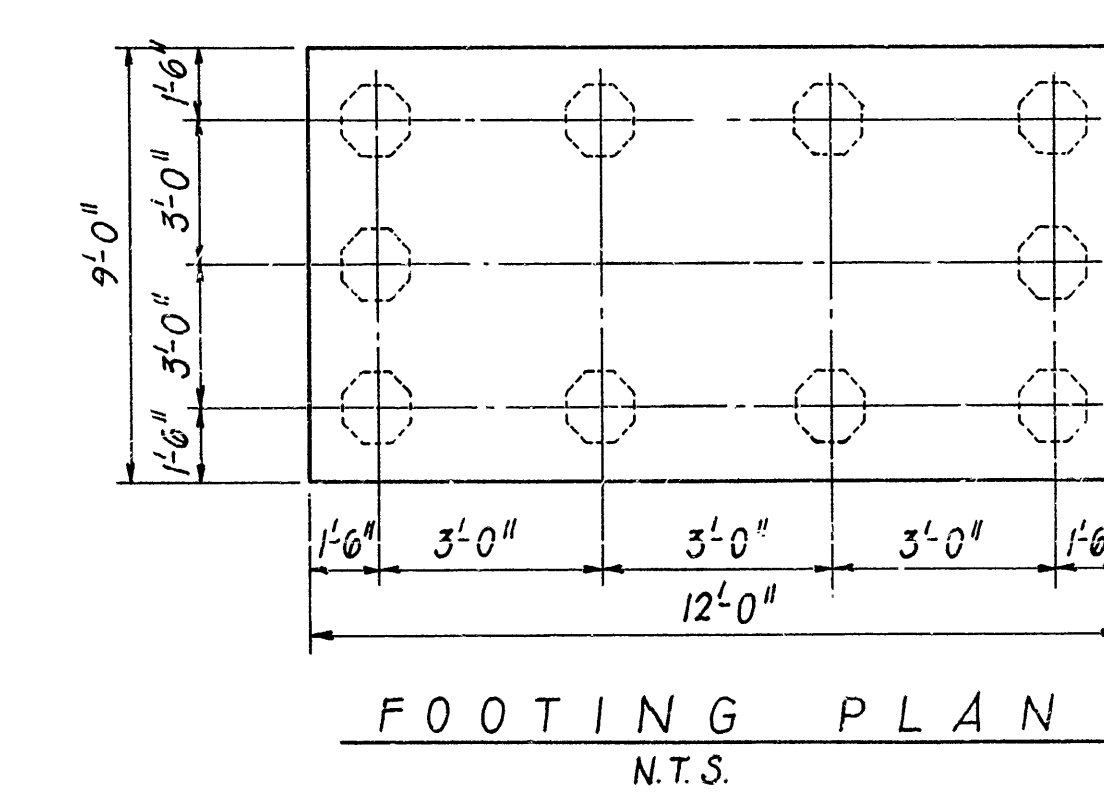
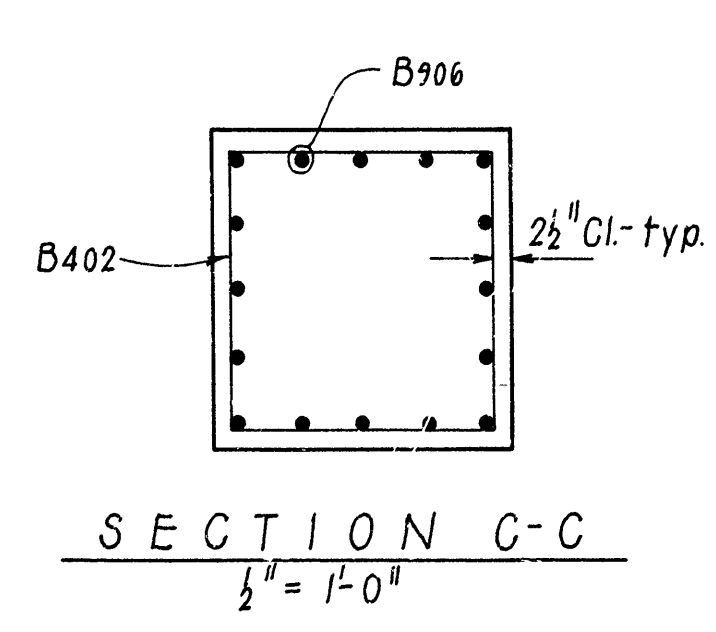
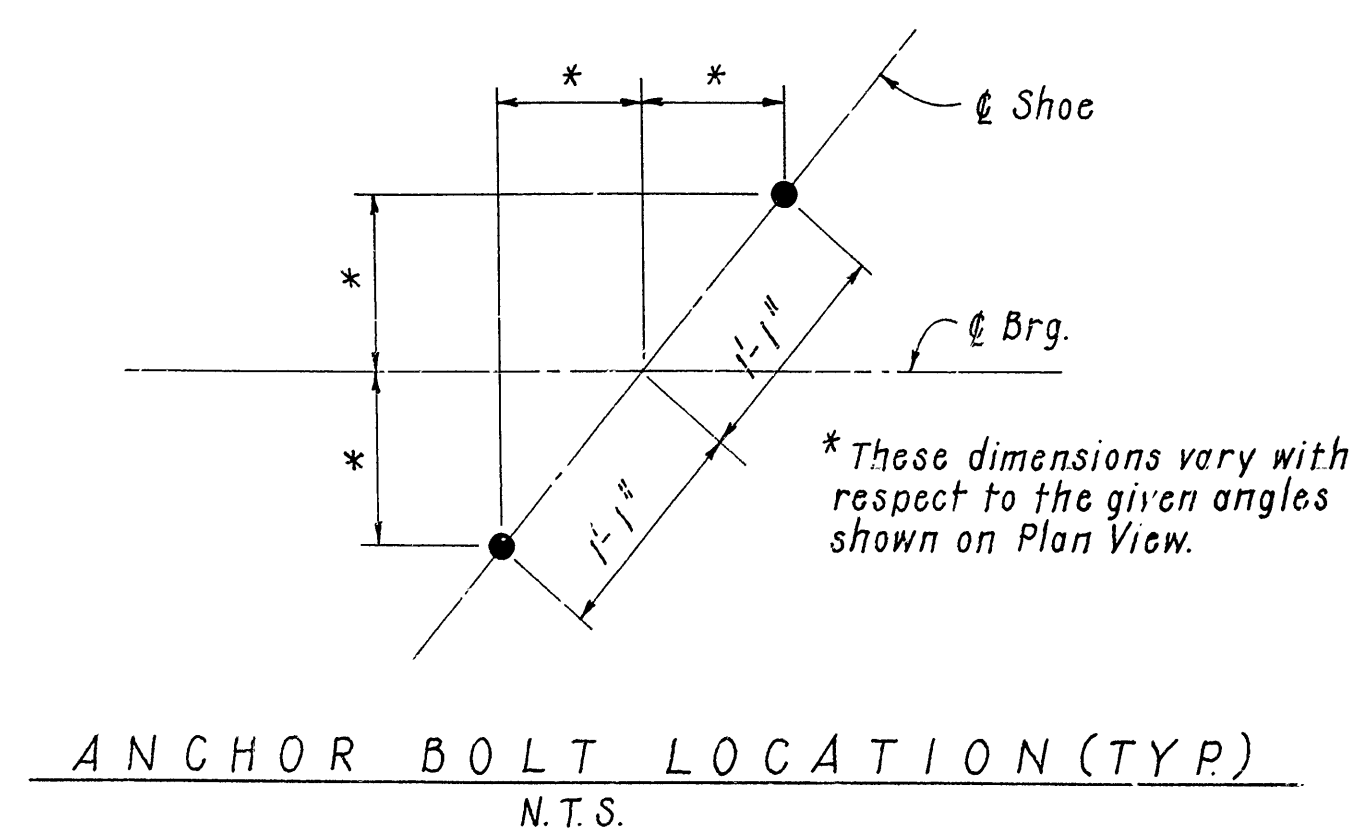
| REVISED | DATE | REVISED | DATE | FED. ROAD NO. | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|---------|------|---------|------|---------------|--------|--------------------|-----------|--------------|
| | | | | 6 | ARK. | | | |
| | | | | JOB NO. | 110065 | 49 | 177 | |
| | | | | 6102 | BENT | 27019 | | |



BAR LIST

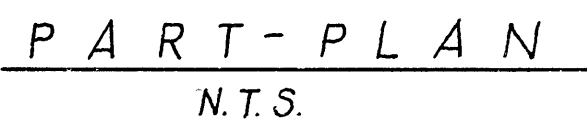
| PK | No. Req'd | Length | X | Y | Pin Dia. |
|------|-----------|---------|--------|--------|----------|
| B901 | 7 | 60'-2" | 57'-8" | 10" | 9" |
| B902 | 2 | 57'-8" | | | Str. |
| B903 | 6 | 57'-8" | | | Str. |
| B904 | 2 | 44'-8" | | | Str. |
| B905 | 16 | 26'-1" | | | Str. |
| B906 | 16 | 27'-3" | | | Str. |
| B907 | 16 | 28'-6" | | | Str. |
| B908 | 48 | 9'-3" | 8'-0" | 10" | 9" |
| B602 | 51 | 12'-10" | 11'-6" | 6" | 4 1/2" |
| B601 | 69 | 9'-10" | 8'-6" | 6" | 4 1/2" |
| B501 | 62 | 15'-4" | 4'-2" | 3'-0" | 2 1/2" |
| B502 | 9 | 11'-2" | 4'-2" | 3'-0" | 2 1/2" |
| B401 | 12 | 29'-8" | | | Str. |
| B402 | 84 | 12'-6" | 2'-11" | 7'-11" | 2" |

Bending Diagrams (Dimensions are out to out of bars)



See dwg. no. 27017 for general notes.

DETAILS OF BENT 4
M.T. RAMP BRIDGE OVER I-40
ROUTE SEC.
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.
DRAWN BY: H.D. DATE: 11-7-84
CHECKED BY: H.D. DATE: 1-15-85
DESIGNED BY: H.D. DATE: 1-15-85
SCALE: 1/4" = 1'-0" or as shown
BRIDGE NO. 6102
DRAWING NO. 27019



At the Contractors option, in lieu of providing Bar S6001, two straight #6 bars may be substituted with the bar in the top mat epoxy coated. Payment for Reinforcing will be based on the weight of Bar S6001.



[illegible]

3'-1 1/4"

PL 1/2" x 12 (A572-Gr.50)

2 1/2"

7"

2 1/2"

1/2" 5 @ 3" 4" 5 @ 3" 1/2"

PL 1/2" x 5 (A572-Gr.50)

2 1/2"

7"

2 1/2"

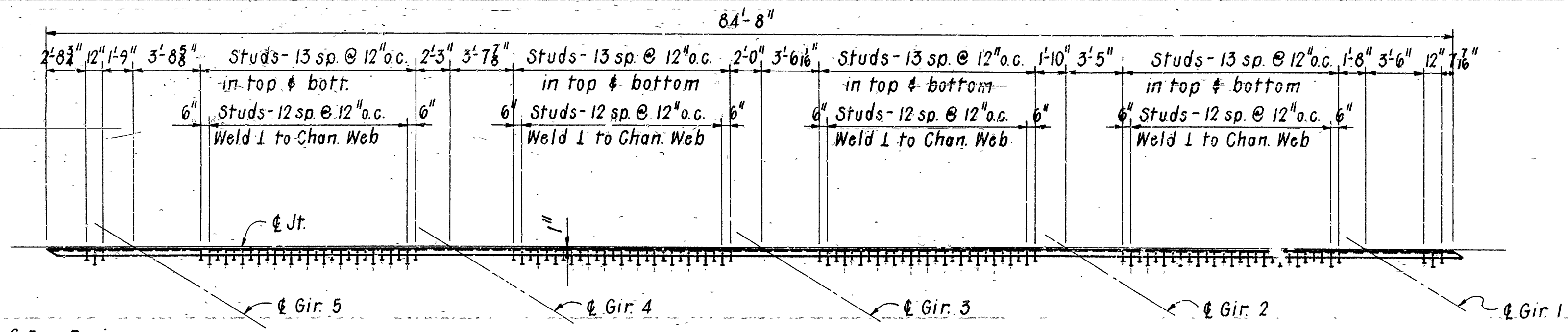
BOTTOM FLANGE SPLICE
PLATES (F.S. #5 & #6)

3/4" = 1'-0"

△ 4-18-85 Rev. Brq. Dimen.

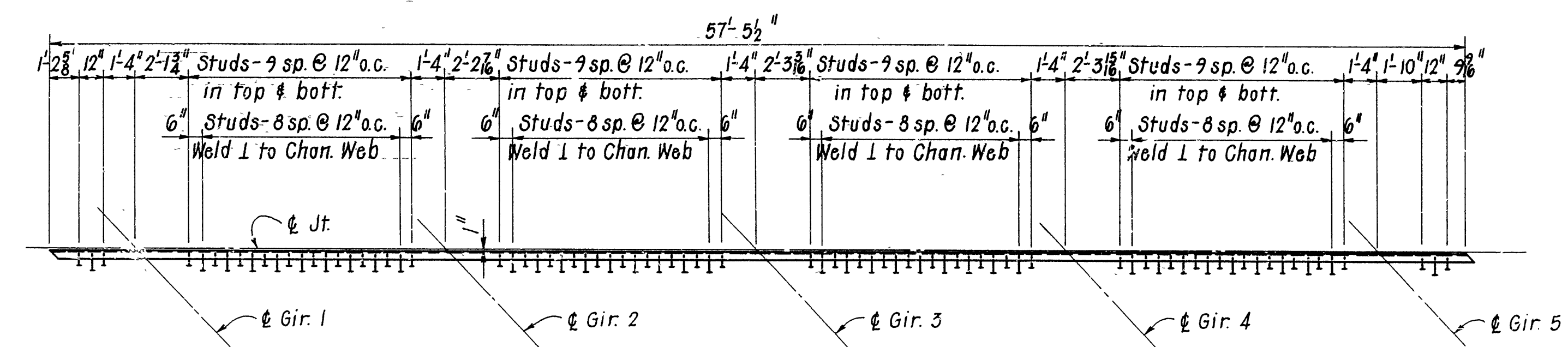
Vera Pinkerton
BRIDGE ENGINEER

| DATE | REV. | DATE | REV. | FED. ROAD NO. | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|---------|------------|------|------|---------------|--------|--------------------|-----------|--------------|
| 4-18-85 | 668-418-85 | | | 6 | ARK. | | | |
| | | | | JOB NO. | 110065 | 54 | 177 | |
| | | | | 6102 | UNIT | 27024 | | |

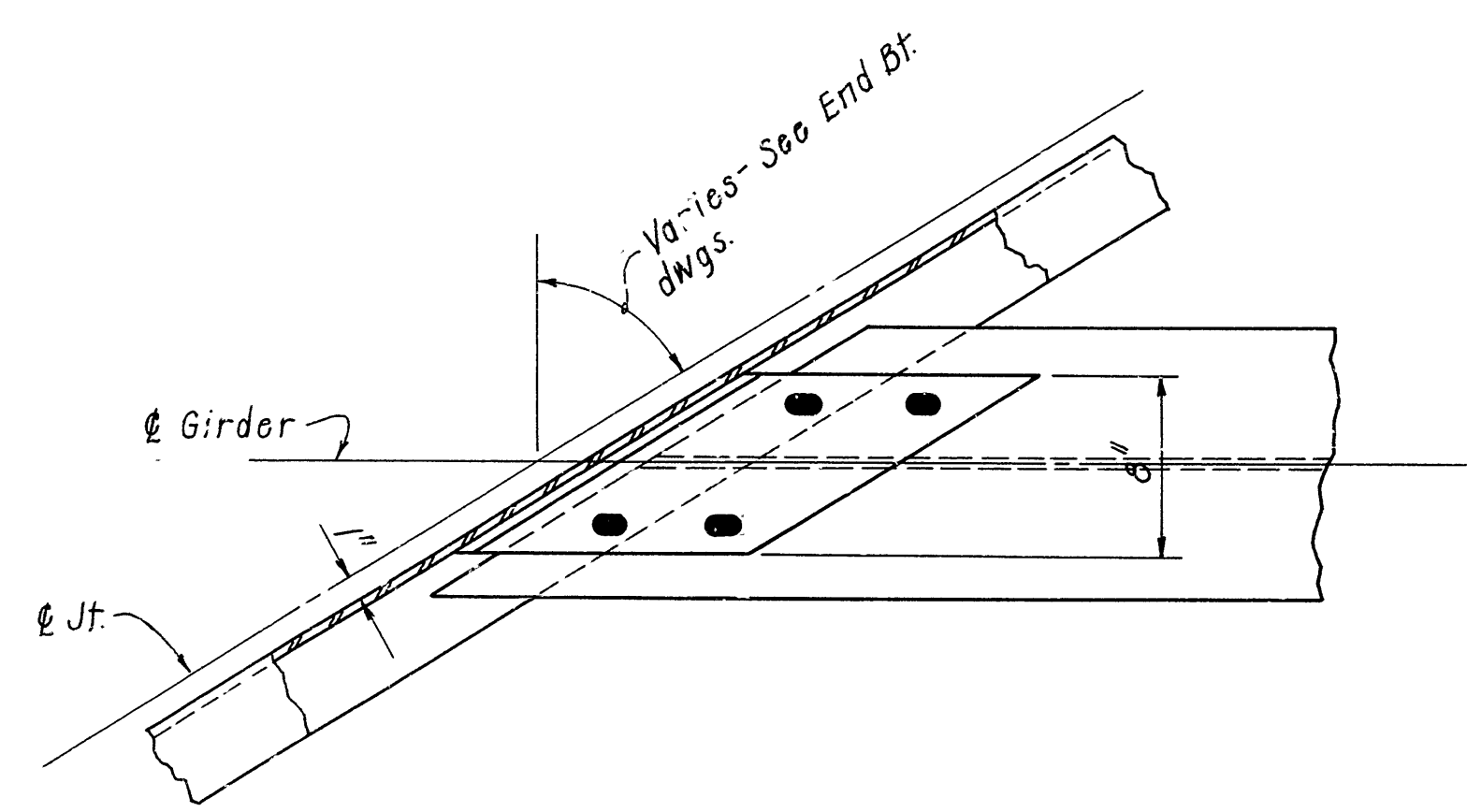


DETAILS OF RDWY CHANNEL - BT. 1
N.T.S.

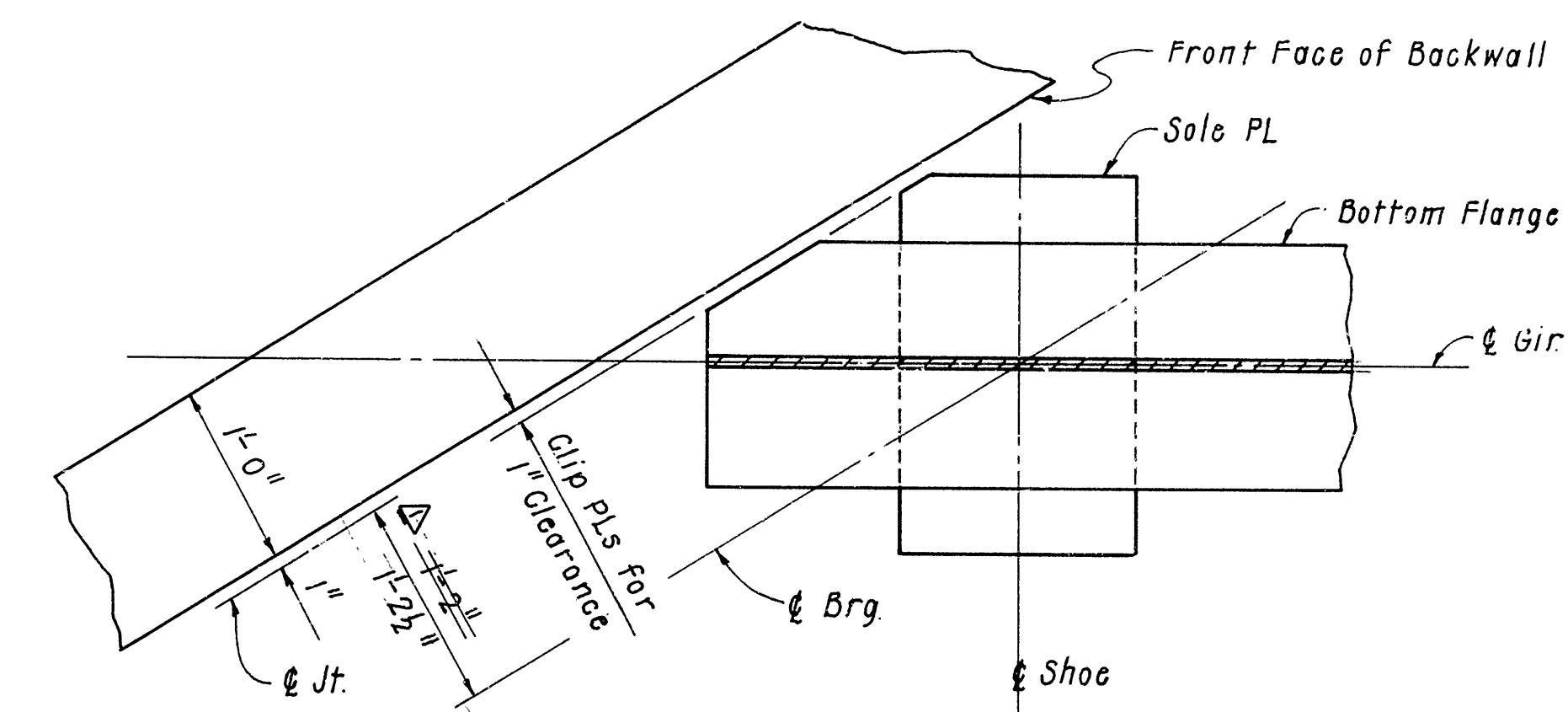
For additional details of Exp. Device
see dwg. no. 27025.



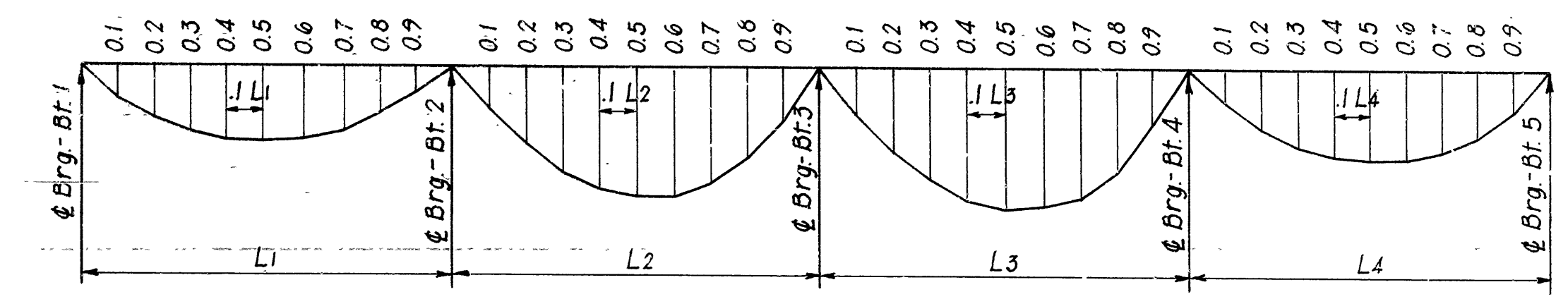
DETAILS OF RDWY CHANNEL - BT. 5
N.T.S.



CHANNEL CONNECTION (TYP)
N.T.S.



BEARING AT BRIDGE ENDS (TYP)
N.T.S.

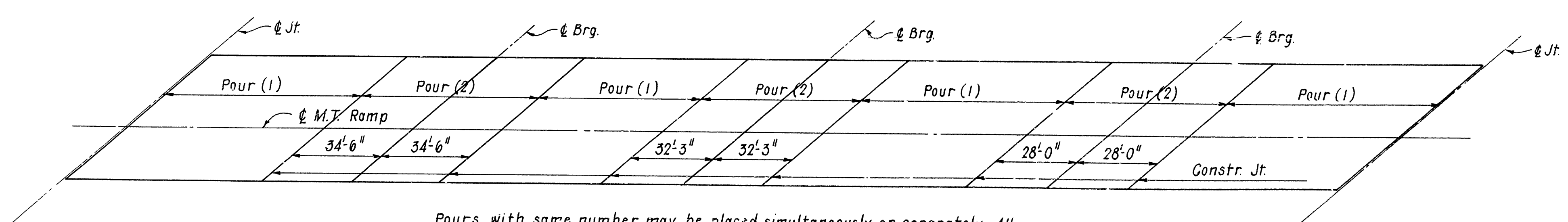


DEAD LOAD DEFLECTIONS (EACH GIRDER)
N.T.S.

Camber for Dead Load Deflection plus Vertical Curve $\pm 1/4$ " tolerance.

TABLE OF DEFLECTIONS (INCHES)

| Point Of Deflection | Weight Of Structural Steel | Wt. Of Structural Steel & Slab | Wt. Of Str. Steel, Slab & Conc. Parapet Rail |
|---------------------|----------------------------|--------------------------------|--|
| | Girder 1, 2, 3, 4 & 5 | Girder 1, 2, 3, 4 & 5 | Girder 1, 2, 3, 4 & 5 |
| 0 | 0 | 0 | 0 |
| 0.1 | 0.172 | 0.963 | 1.032 |
| 0.2 | 0.317 | 1.768 | 1.694 |
| 0.3 | 0.414 | 2.305 | 2.470 |
| 0.4 | 0.453 | 2.516 | 2.698 |
| 0.5 | 0.434 | 2.398 | 2.573 |
| 0.6 | 0.364 | 1.998 | 2.146 |
| 0.7 | 0.261 | 1.419 | 1.525 |
| 0.8 | 0.150 | 0.807 | 0.867 |
| 0.9 | 0.054 | 0.289 | 0.309 |
| 1.0 | 0 | 0 | 0 |
| 0.1 | 0.015 | 0.082 | 0.095 |
| 0.2 | 0.085 | 0.460 | 0.512 |
| 0.3 | 0.172 | 0.952 | 1.048 |
| 0.4 | 0.249 | 1.387 | 1.518 |
| 0.5 | 0.286 | 1.591 | 1.737 |
| 0.6 | 0.271 | 1.492 | 1.627 |
| 0.7 | 0.208 | 1.124 | 1.228 |
| 0.8 | 0.120 | 0.630 | 0.689 |
| 0.9 | 0.039 | 0.193 | 0.212 |
| 1.0 | 0 | 0 | 0 |
| 0.1 | 0.031 | 0.189 | 0.206 |
| 0.2 | 0.103 | 0.613 | 0.669 |
| 0.3 | 0.186 | 1.109 | 1.207 |
| 0.4 | 0.253 | 1.503 | 1.633 |
| 0.5 | 0.278 | 1.649 | 1.792 |
| 0.6 | 0.254 | 1.500 | 1.632 |
| 0.7 | 0.188 | 1.103 | 1.204 |
| 0.8 | 0.104 | 0.602 | 0.660 |
| 0.9 | 0.032 | 0.182 | 0.201 |
| 1.0 | 0 | 0 | 0 |
| 0.1 | 0.021 | 0.125 | 0.134 |
| 0.2 | 0.071 | 0.418 | 0.451 |
| 0.3 | 0.134 | 0.793 | 0.855 |
| 0.4 | 0.197 | 1.169 | 1.259 |
| 0.5 | 0.242 | 1.444 | 1.553 |
| 0.6 | 0.257 | 1.543 | 1.659 |
| 0.7 | 0.238 | 1.431 | 1.537 |
| 0.8 | 0.184 | 1.107 | 1.188 |
| 0.9 | 0.101 | 0.606 | 0.650 |
| 1.0 | 0 | 0 | 0 |



Pours with same number may be placed simultaneously or separately. All pours (1) must be placed before pours (2) can be placed. 48 hours shall elapse between pours except 72 hours shall elapse between adjacent pours.

SLAB POURING SEQUENCE
N.T.S.

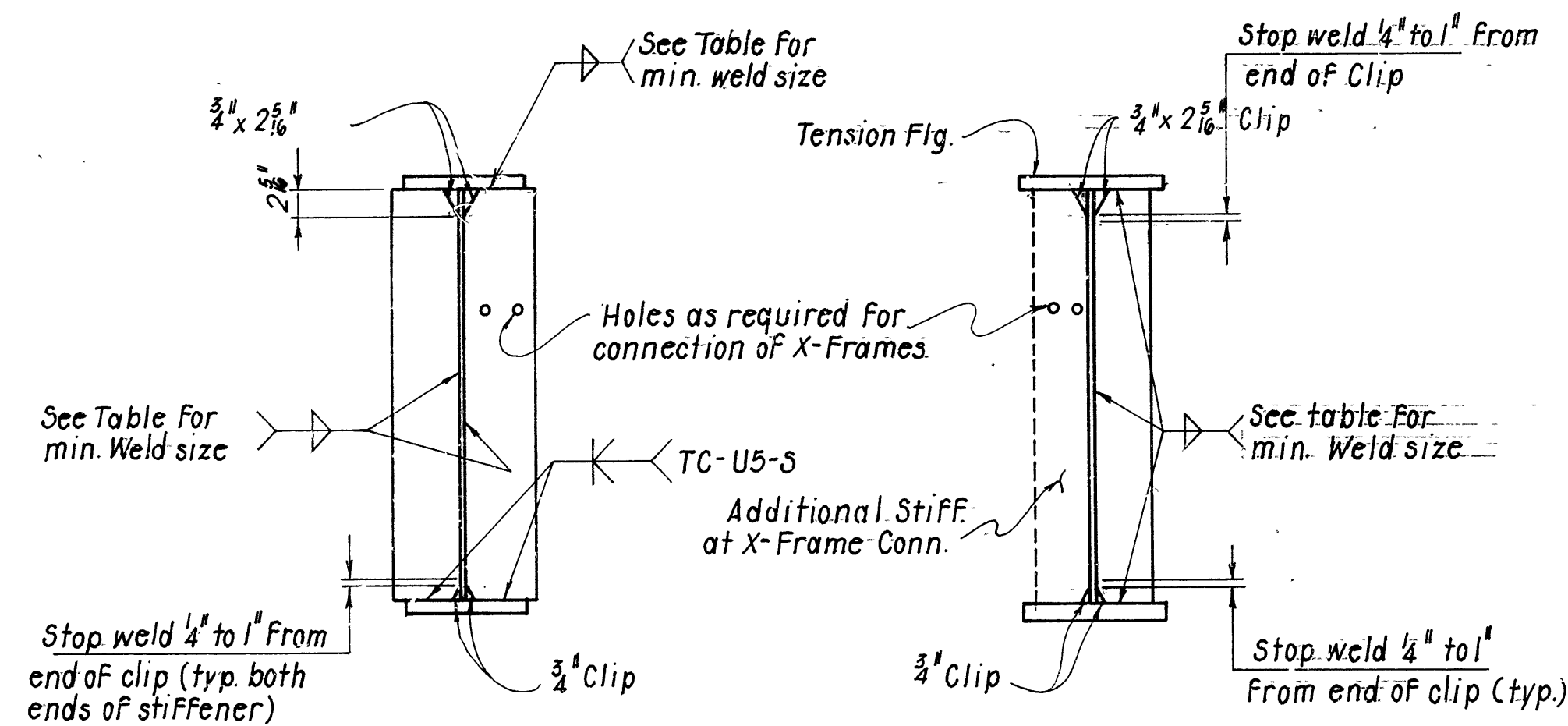
NOTE: Any railing pours made before the entire slab unit has been placed must be approved by the Bridge Engineer.

4-18-85 Rev. Brg. Dimen.

DETAILS OF 532'-0" CONT. PL. GIRDER UNIT
M.T. RAMP BRIDGE OVER I-40
ROUTE SEC.
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.
DRAWN BY: H. J. D. DATE: 10-31-84
CHECKED BY: H. J. D. DATE: 1-15-85
DESIGNED BY: H. J. D. DATE: 1-15-85
BRIDGE NO. 6102 DRAWING NO. 27024

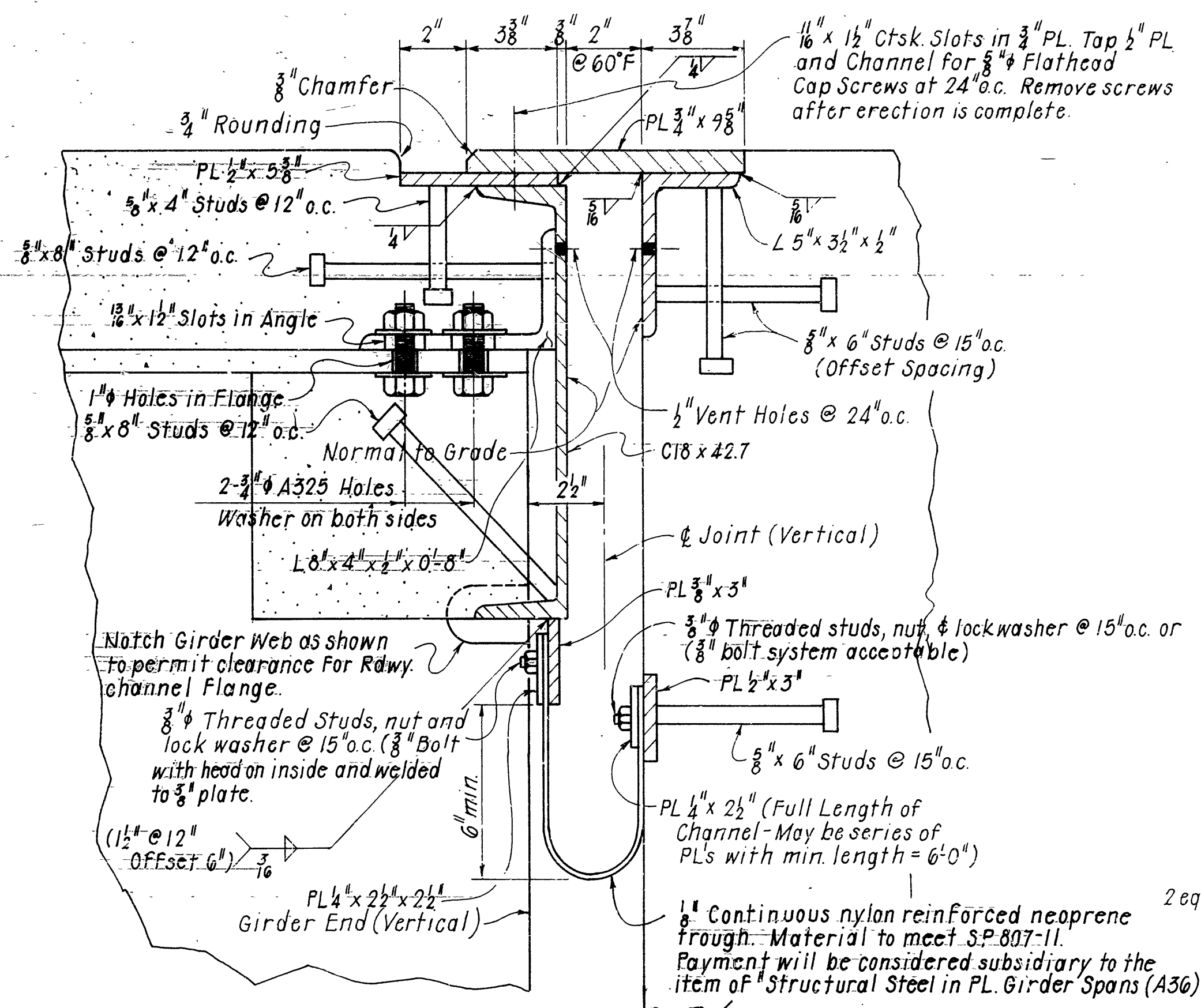
| DATE REVISED | DATE FILMED | DATE REVISED | DATE FILMED | FED. ROAD NO. | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-----------------|----------------|-----------------|----------------|------------------|--------|--------------------|--------------|-----------------|
| | | | | 6 | ARK. | | | |
| | | | | JOB NO. | 110065 | 55 | 177 | |

① 6102 SPANS. 27025

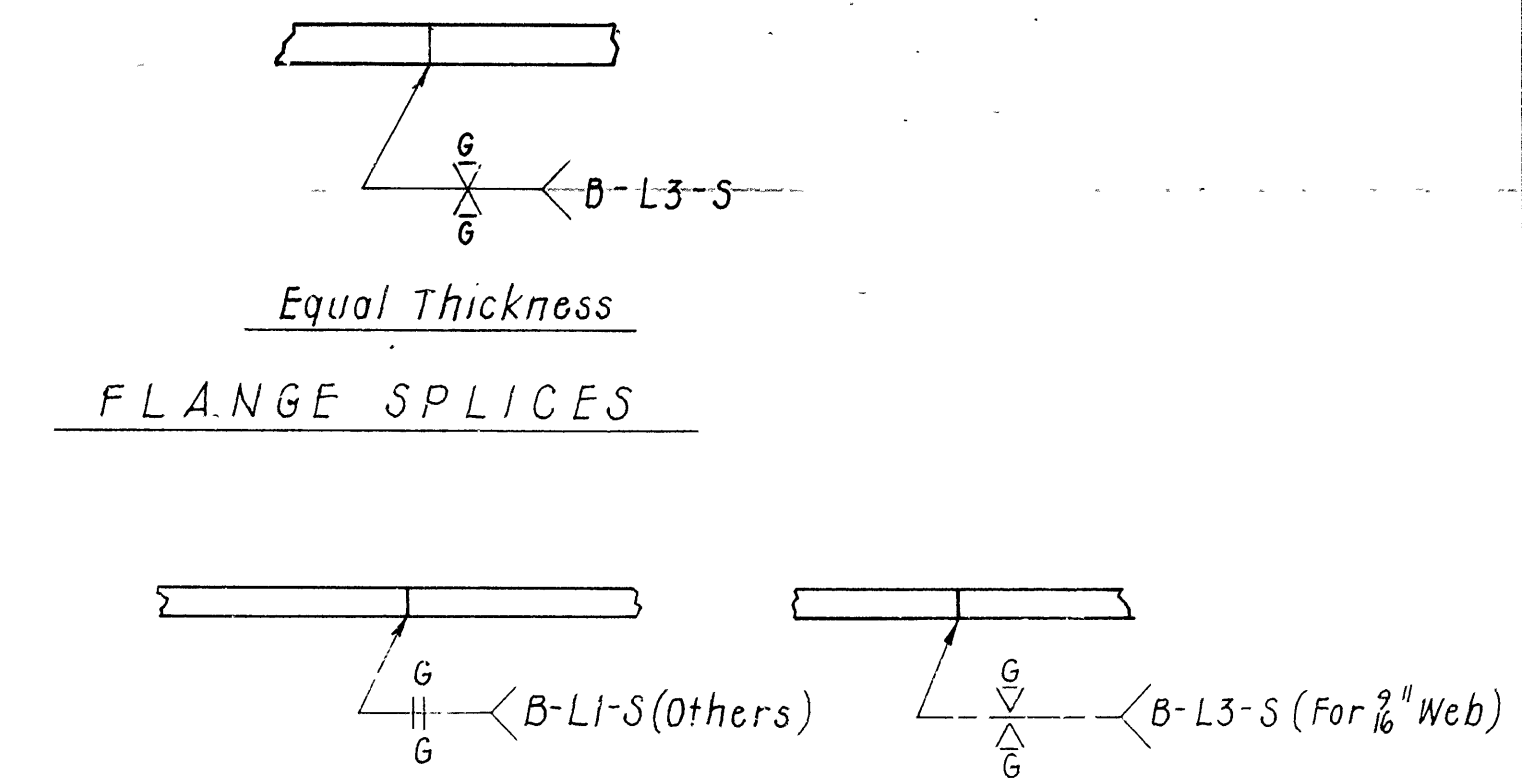


BEARING STIFFENER

INTERMEDIATE
STIFFENER



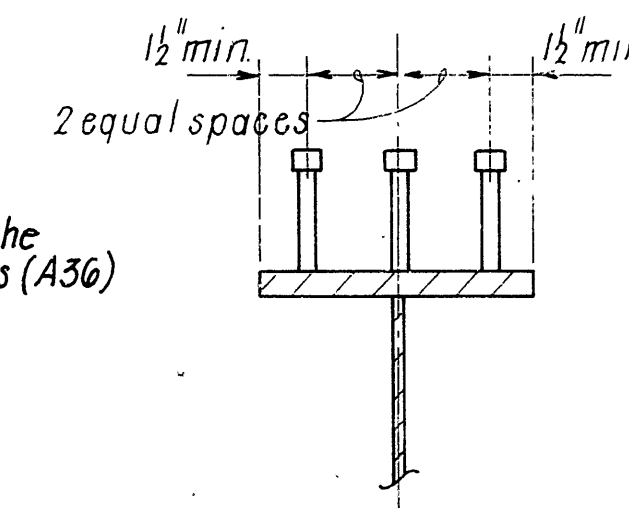
EXPANSION DEVICE DETAILS AT END BENTS
N.T.S.



WEB SPLICE

DETAILS OF WELDED SPLICES

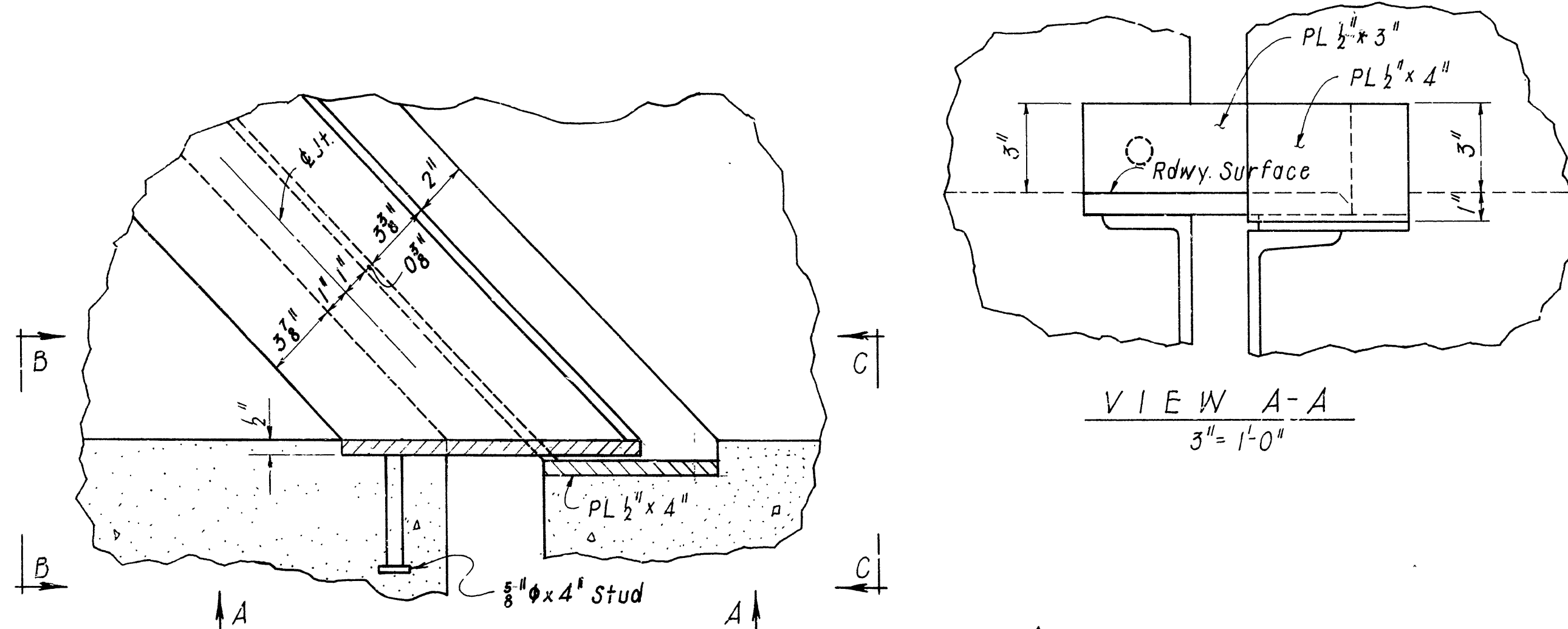
N. T. S



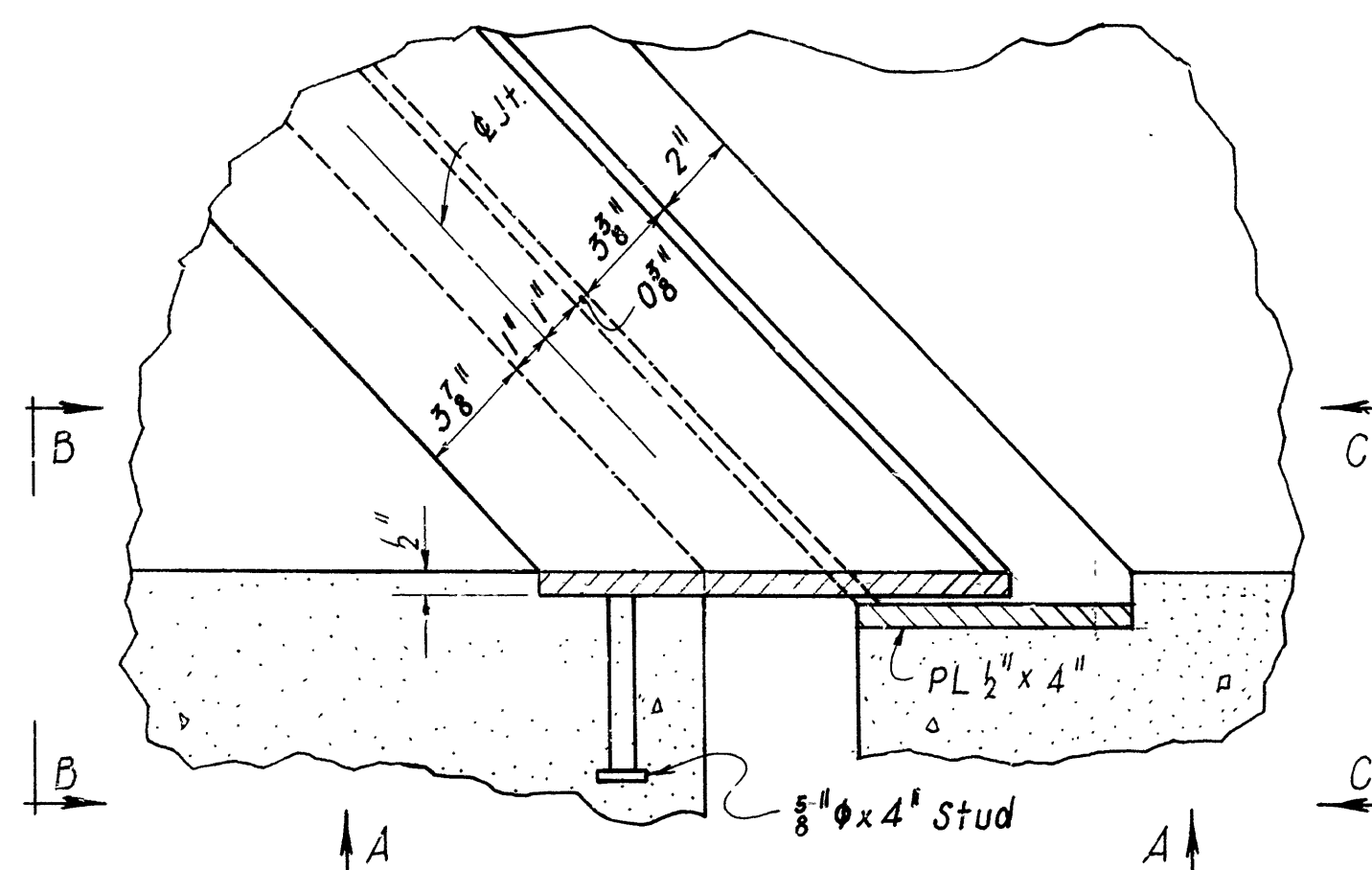
Stud Shear Connectors shown shall be $3\frac{1}{2}" \times 5"$ long granular flux filled, solid fluxed or equal and automatically welded to girder flange in accordance with recommendations of the Manufacturer. The $2\frac{1}{2}"$ studs are estimated at 740 lb. per 100 as a basis of payment. The $8\frac{1}{2}"$ studs may be substituted for the $2\frac{1}{2}"$ studs shown at the ratio of 0.73- $2\frac{1}{2}"$ studs in place of 1- $8\frac{1}{2}"$ stud.

SHEAR CONNECTOR DETAIL

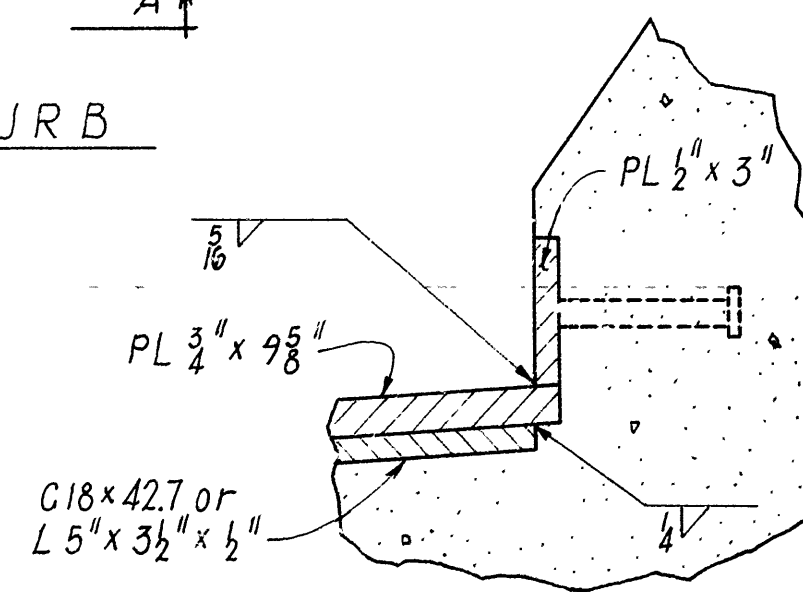
N.T.S.



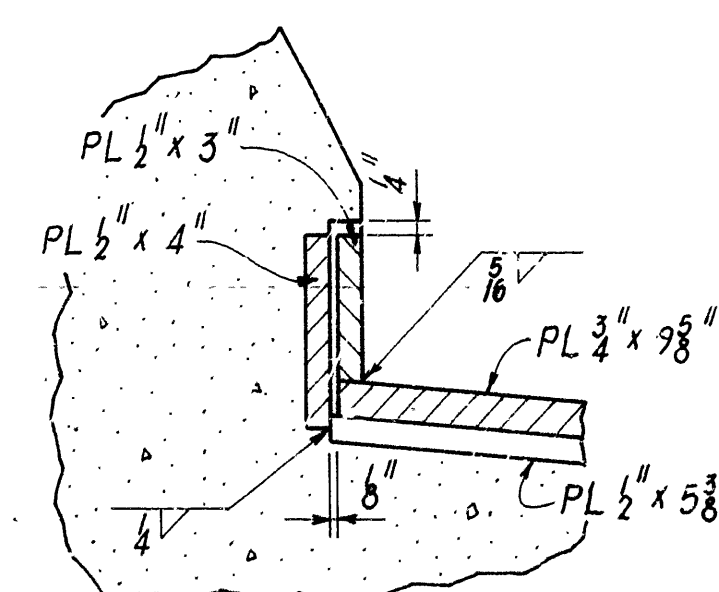
V I E W A-A
3" = 1'-0"



PLAN OF JOINT AT CURB

$$3'' = 1' - 0''$$


V I E W B-B

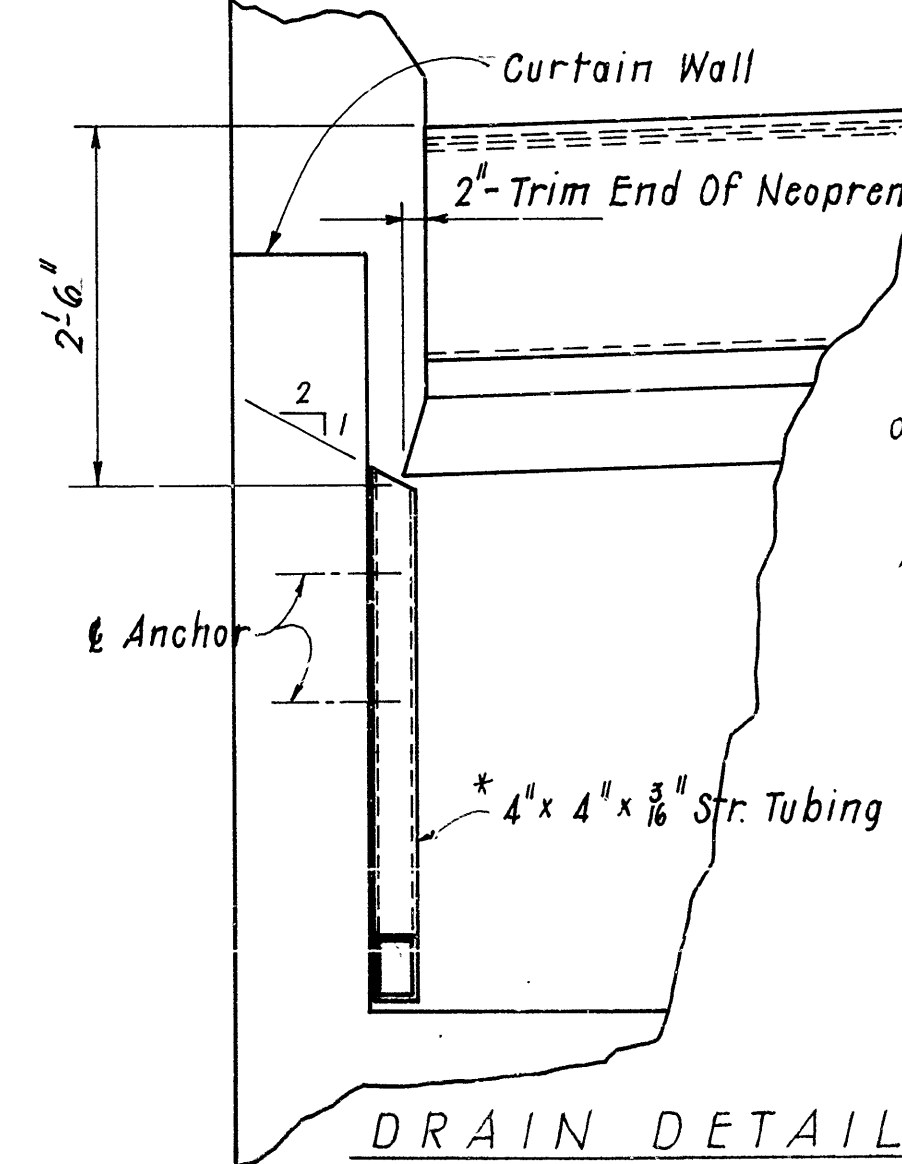
$$3'' = 1' - 0''$$


V I E W C - C

$$3'' = 1'-0''$$
TABLE FOR WELD

| Material Thickness Of Thicker Part Joined (Inches) | Minimum Size Of Fillet Weld (Inches) | Single Pass Weld Must Be Used |
|--|--|---|
| To $\frac{1}{2}$ " Inclusive | $\frac{3}{16}$ " | |
| Over $\frac{1}{2}$ " to $\frac{3}{4}$ " | $\frac{1}{4}$ " | |
| Over $\frac{3}{4}$ " | $\frac{5}{16}$ " | |

Note: When a fillet weld size, as shown on the Plans, is larger than the minimum, the First Pass shall be that specified for minimum size of fillet weld.

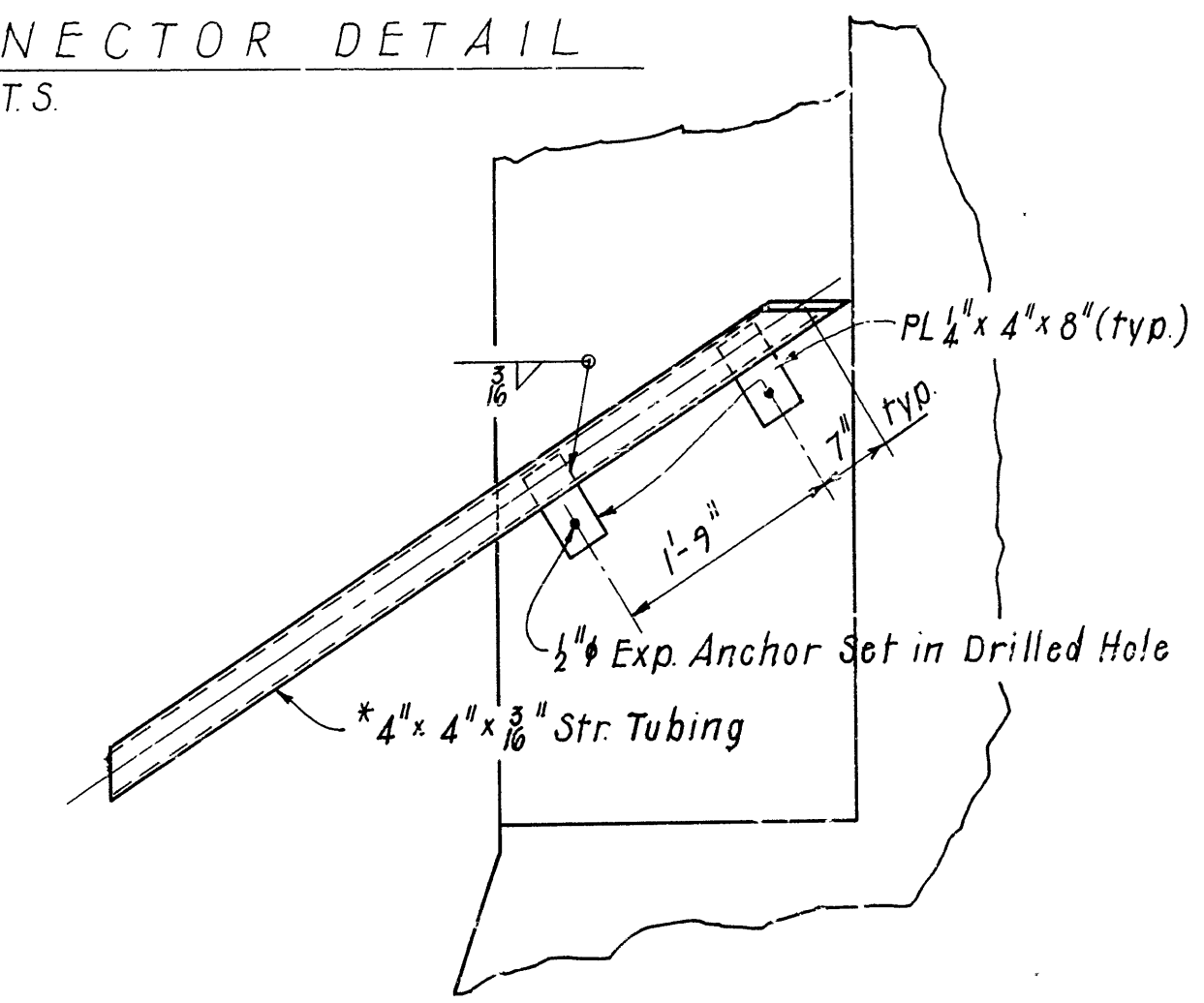


DRAIN DETAIL

$$\frac{3}{4}'' = 1\frac{1}{4}''$$

Drains are to be installed on low side of Bridge at End Bents.
Drains are to be hot-dip galvanized after fabrication in accordance with ASTM A-153.
Drains will not be paid for directly but will be considered subsidiary to the item of "Structural Steel in Plate Girder Spans (A36)".

* See End Bent Details for Lengths of
rain Tubing. Drains to be attached as
shown to Curtain Wall.



V I E W J - J
N.T.S.

SHEET 1 OF 3

SUPPLEMENTARY DETAILS FOR
CONT. PL. GIRDER UNITS
M.T. RAMP BRIDGES

| ROUTE | SEC. |
|-------|------|
|-------|------|

ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

DRAWN BY: *H. Maj.* DATE: *12-20-82*

DRAWN BY: DATE:
CHECKED BY: HJD DATE: 1-15-85 SCALE: As Shown

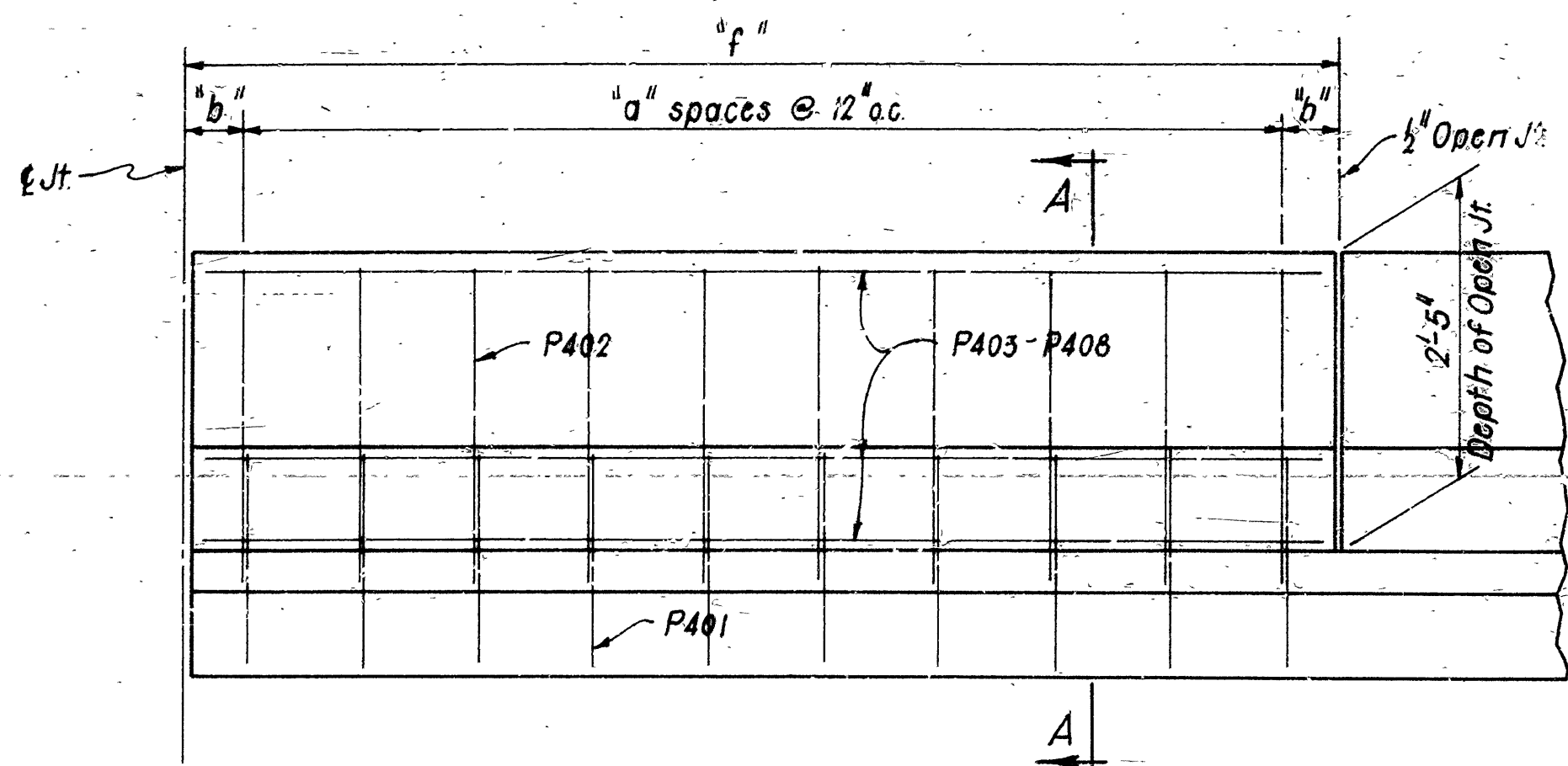
CHECKED BY: DATE:
DESIGNED BY: GVA DATE: 12-20-82

PROJECT NO. 6102 DRAWING NO. 3

DRAWING NO.27025

Verel Pinkerton
BRIDGE ENGINEER

| DATE | REV. | DATE | REV. | FED. ROAD NO. | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|------|------|------|------|---------------|--------|--------------------|-----------|--------------|
| | | | | 6 | ARK. | | | |
| | | | | JOB NO. | 110065 | | 56 | 177 |
| | | | | 6102 | SPANS | 27026 | | |



TYP SECTION FOR CLOSED PARAPET RAIL
N.T.S.

VARIABLES

| | "f" | "a" | "b" |
|-------------|-------------|-----|--------|
| DR NO. 6102 | 15'-5 1/2" | 15 | 28" |
| | 15'-11 1/2" | 15 | 58" |
| | 15'-7 1/2" | 15 | 38" |
| | 16'-0 1/2" | 15 | 68" |
| | 16'-5 1/2" | 16 | 28" |
| | 16'-8 1/2" | 16 | 48" |
| | 16'-1 1/2" | 15 | 68" |
| | 16'-5 1/2" | 16 | 28" |
| DR NO. 6105 | 15'-3" | 14 | 7 1/2" |
| | 15'-2 1/2" | 14 | 7 1/2" |
| | 15'-3 1/2" | 14 | 7 1/2" |
| | 15'-3 1/2" | 14 | 7 1/2" |
| | 16'-4 1/2" | 16 | 28" |
| | 16'-4 1/2" | 16 | 28" |
| | 15'-7" | 15 | 38" |
| | 15'-7 1/2" | 15 | 38" |
| | 15'-2 1/2" | 15 | 48" |
| | 15'-2 1/2" | 15 | 48" |
| | 17'-0 1/2" | 16 | 68" |
| | 17'-0 1/2" | 16 | 68" |

STRUCTURAL STEEL:

ALL X-FRAMES SHALL BE INSTALLED AND COMPLETELY BOLTED PRIOR TO POURING OF FLOOR SLABS. X-FRAMES AT BEARING STIFFENERS SHALL BE INSTALLED AS GIRDERS ARE ERECTED.

FLANGE PLATES NOTED ON DWG. 27023 AS HIGH STRENGTH LOW ALLOY COLUMBIUM VANADIUM STEEL, ASTM DESIGNATION A572, GRADE 50 SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER POUND BID FOR "STRUCTURAL STEEL IN PLATE GIRDER SPANS A572, GRADE 50." ALL OTHER STRUCTURAL STEEL SHALL BE ASTM DESIGNATION A36 AND SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER POUND BID FOR "STRUCTURAL STEEL IN PLATE GIRDER SPANS A36."

STRUCTURAL SHAPES OF EQUAL OR GREATER STRENGTH MAY BE SUBSTITUTED FOR SHAPES SHOWN IF APPROVAL IS OBTAINED FROM THE BRIDGE ENGINEER. PAYMENT WILL BE MADE ON THE BASIS OF SHAPES SHOWN.

GIRDER WEBS MAY BE MADE BY SHOP SPlicing WITH MIN. 10" LENGTH OF 25'-0" FOR SECTIONS. FLANGE PLATES LONGER THAN 50' MAY BE MADE BY SHOP SPlicing WITH MINIMUM LENGTH OF 25'-0" FOR SECTIONS. NO ADDITIONAL PAYMENT FOR WELDS FOR THESE SPLICES WILL BE MADE.

STEEL PLATES FOR MAIN MEMBERS, AND MAIN TENSION MEMBERS, NOT SECONDARY MEMBERS, SHALL BE CUT AND FACED TO THAT THE PRIMARY DIRECTION OF ROLLING IS PARALLEL TO THE DIRECTION OF THE MAIN TENSILE AND/OR COMPRESSIVE STRESSES.

DRAWINGS SHOW GENERAL FEATURES OF DESIGN ONLY. SHOP DRAWINGS SHALL BE MADE IN ACCORDANCE WITH THE SPECIFICATIONS, SUBMITTED AND APPROVAL SECURED BEFORE FABRICATION IS BEGUN.

ALL PLATE GIRDER WEBS AND FLANGES ARE TO BE CONSIDERED MAIN LOAD CARRYING MEMBERS, AND SHALL MEET THE LONGITUDINAL CHARTY V-NOTCH TEST SPECIFIED IN SECTION 807.05 OF THE STANDARD SPECIFICATIONS. THIS WORK AND MATERIAL IS TO BE CONSIDERED AS SUBSIDIARY TO THE ITEM, "STRUCTURAL STEEL IN PLATE GIRDER SPANS (A36)" AND WILL NOT BE PAID FOR DIRECTLY.

ALL GIRDERS SHALL BE BLOCKED IN THEIR TRUE POSITION, WITH WEBS, VERTICAL IN THE SHOP IN GROUPS OF A MINIMUM OF TWO SECTIONS OR ENOUGH SECTIONS TO INCLUDE TWO BEARINGS. THE CAMBER, LENGTH OF SECTIONS, DISTANCE BETWEEN BEARINGS AND OPENINGS OF THE JOINTS SHALL BE MEASURED WITH THE GIRDERS IN THIS POSITION AND THIS INFORMATION SHALL BECOME A PART OF THE PERMANENT RECORD OF THIS JOB. THE COMPONENT PARTS SHALL BE MATCH MARKED IN THIS ASSEMBLY AND THESE MARKS SHALL BE SHOWN ON THE ERECTION DIAGRAM. ALL GIRDER DIMENSIONS ARE BASED ON THE TEMPERATURE OF 60°F. A TOLERANCE OF + 1/4" IS ALLOWED FOR CAMBER. SEE SECTION 807.16(b) OF THE STANDARD SPECIFICATIONS.

ALL WELDING THAT IS TO BE DONE DURING FABRICATION OF STRUCTURAL STEEL, INCLUDING TEMPORARY WELDS SHALL BE DETAILED ON THE SHOP DRAWINGS AND SUBMITTED FOR APPROVAL. IF THE CONTRACTOR OR ERECTOR SHOULD WANT TO MAKE ADDITIONAL WELDS, WHETHER TEMPORARY OR PERMANENT, HE SHALL SUBMIT DETAILED DRAWINGS WITH A FORMAL REQUEST TO THE BRIDGE DESIGN DIVISION OF THE ARKANSAS STATE HIGHWAY AND TRANSPORTATION DEPARTMENT FOR APPROVAL. ALL WELDING SHALL CONFORM TO SP 807-5.

FIELD CONNECTIONS TO BE BOLTED WITH HIGH STRENGTH BOLTS: BOLTS SHALL BE 3/4" Ø AND OPEN HOLES 13/16" IN DIAMETER EXCEPT WHERE NOTED OTHERWISE. BOLT SPACING SHALL BE 2-1/2" FOR 3/4" Ø BOLTS. BOLTS SHALL BE PLACED WITH HEADS ON THE OUTSIDE FACE OF THE EXTERIOR GIRDER AND ON THE BOTTOM OF THE GIRDER FLANGES.

HOLES FOR 3/4" Ø HIGH-STRENGTH BOLTS IN X-FRAMES MAY BE 15/16" Ø, IF A WASHER IS SUPPLIED FOR USE UNDER BOTH THE NUT AND HEAD OF THE BOLT.

SHOP PAINT: ALL STRUCTURAL STEEL EXCEPT GALVANIZED MEMBERS, CONTACT SURFACES OF BOLTED CONNECTIONS, AND SURFACES WITHIN 3" OF HOLES AND FIELD WELDS, AND SURFACES IN CONTACT WITH CONCRETE SHALL BE GIVEN ONE PRIME COAT AS SPECIFIED IN SECTION 807.59 OF THE STANDARD SPECIFICATIONS.

FIELD PAINT: IN ADDITION TO THE PRIME COAT ALL STRUCTURAL STEEL EXCEPT GALVANIZED MEMBERS AND SURFACES IN CONTACT WITH CONCRETE SHALL RECEIVE TWO COATS OF FIELD PAINT.

FIRST COAT - SEE SPECIAL PROVISION 807-10, "PAINTING OF STEEL STRUCTURES." SECOND COAT - BLUE. SEE SECTION 807.59 OF THE STANDARD SPECIFICATIONS AND SPECIAL PROVISION J03 110065 "PAINTING OF STEEL STRUCTURES."

GENERAL NOTES

REINFORCING STEEL:

REINFORCING STEEL TO BE ASTM A615 OR A617, GRADE 60. THE REINFORCING STEEL IS TO BE ACCURATELY LOCATED IN THE FORMS AND FIRMLY HELD IN PLACE BY STEEL WIRE SUPPORTS, SUFFICIENT IN NUMBER AND SIZE TO PREVENT DISPLACEMENT DURING THE COURSE OF THE CONSTRUCTION. THE WIRE SUPPORTS WILL NOT BE PAID FOR DIRECTLY BUT WILL BE CONSIDERED SUBSIDIARY TO THE ITEM OF "REINFORCING STEEL."

CLASS (S(AE)) CONCRETE:

ALL CONCRETE TO BE CLASS (S(AE)) CONCRETE. ALL EXPOSED CORNERS TO BE CHAMFERED 3/4" UNLESS OTHERWISE NOTED.

THE CONCRETE BRIDGE DECK SHALL BE GIVEN A FINE FINISH AS SPECIFIED FOR FINAL FINISHING IN SUBSECTION 802.23 FOR CLASS 6, ROADWAY SURFACE FINISH.

THE CONTRACTOR, AT HIS OPTION, MAY POUR BRIDGE SLAB CONTINUOUS OVER AN ENTIRE CONTINUOUS UNIT USING A RETARDING AGENT TO RETARD SET. A MINIMUM OF 72 HOURS SHALL ELAPSE BETWEEN POURING OF SLAB AND PARAPET SECTIONS.

ALL CONCRETE SHALL BE POURED AND SCREEDED OFF PRIOR TO INITIAL SET. THE CONCRETE DECK SHALL BE FINISHED IN ACCORDANCE WITH SECTION 802.23 OF THE STANDARD SPECIFICATIONS. MOVEMENT OF THE FINISHING MACHINE ACROSS NEW CONCRETE SHALL BE ON PLANKS PLACED ON THE SURFACE AND SHALL BE PROHIBITED FOR 72 HOURS AFTER FINISHING THE POUR. SUFFICIENT CONCRETE MUST BE PLACED AHEAD OF THE STRIKE-OFF TO FULLY LOAD THE GIRDER. IF A LONGITUDINAL STRIKE-OFF IS USED, A VERTICAL CAMBER ADJUSTMENT MUST BE MADE IN THE STRIKE-OFF TO ACCOUNT FOR THE FUTURE DEAD LOAD DEFLECTION OF THE RAILING.

SPECIFICATIONS: ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 1978 AND APPLICABLE SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS: AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 1977 WITH INTERIM SPECIFICATIONS.

METHOD OF DESIGN: LOAD FACTOR

DESIGN LIVE LOADING: HS20

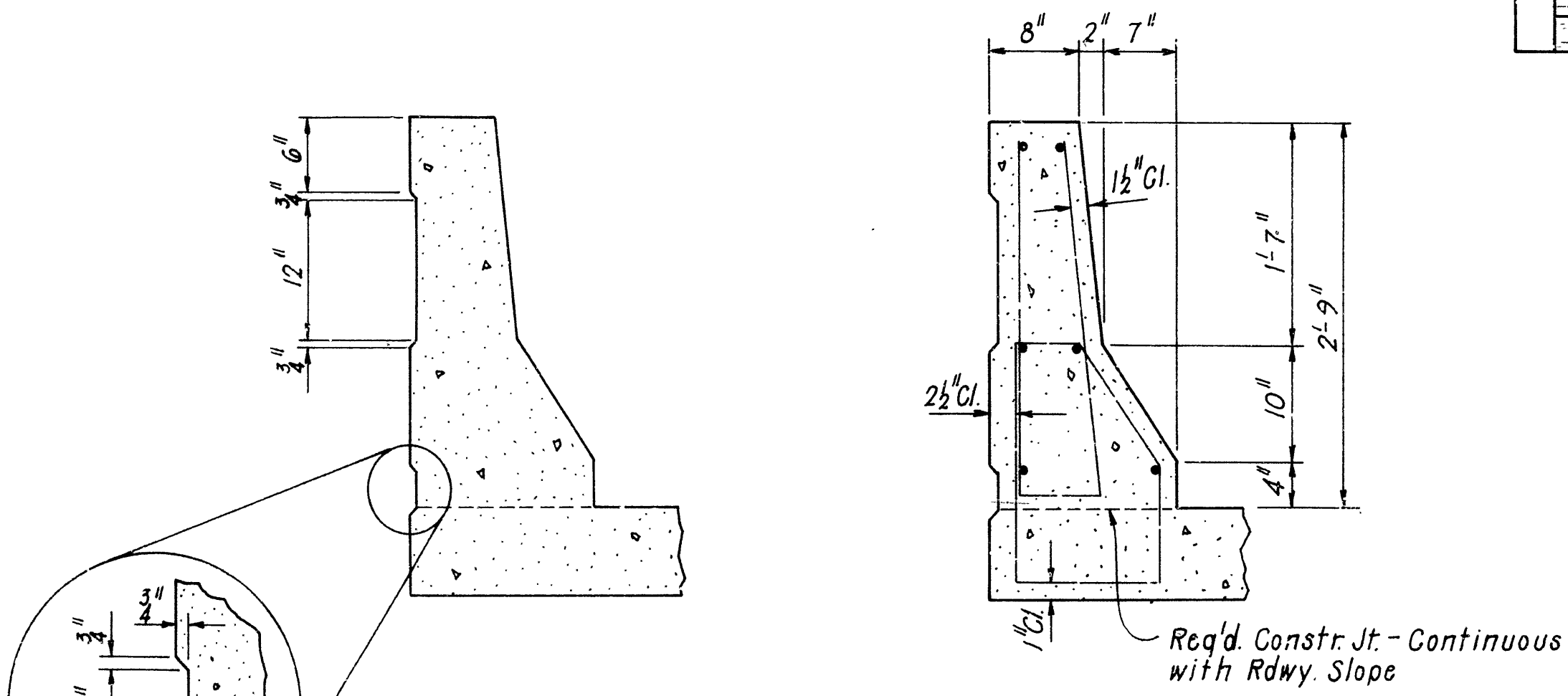
UNIT STRESSES:

CLASS (S(AE)) CONCRETE (f_c = 3500 PSI)
REINFORCING STEEL (GRADE 60) f_y = 60,000 PSI
STRUCTURAL STEEL (A572, GR. 50) f_y = 50,000 PSI
STRUCTURAL STEEL (A36) f_y = 36,000 PSI

LOAD DISTRIBUTION: TO INTERIOR GIRDER TO EXTERIOR GIRDER
DEAD LOAD NON-COMPOSITE 937 ±/FT. ±WT. OF GIRDER 868 ±/FT. ±WT. OF GIRDER

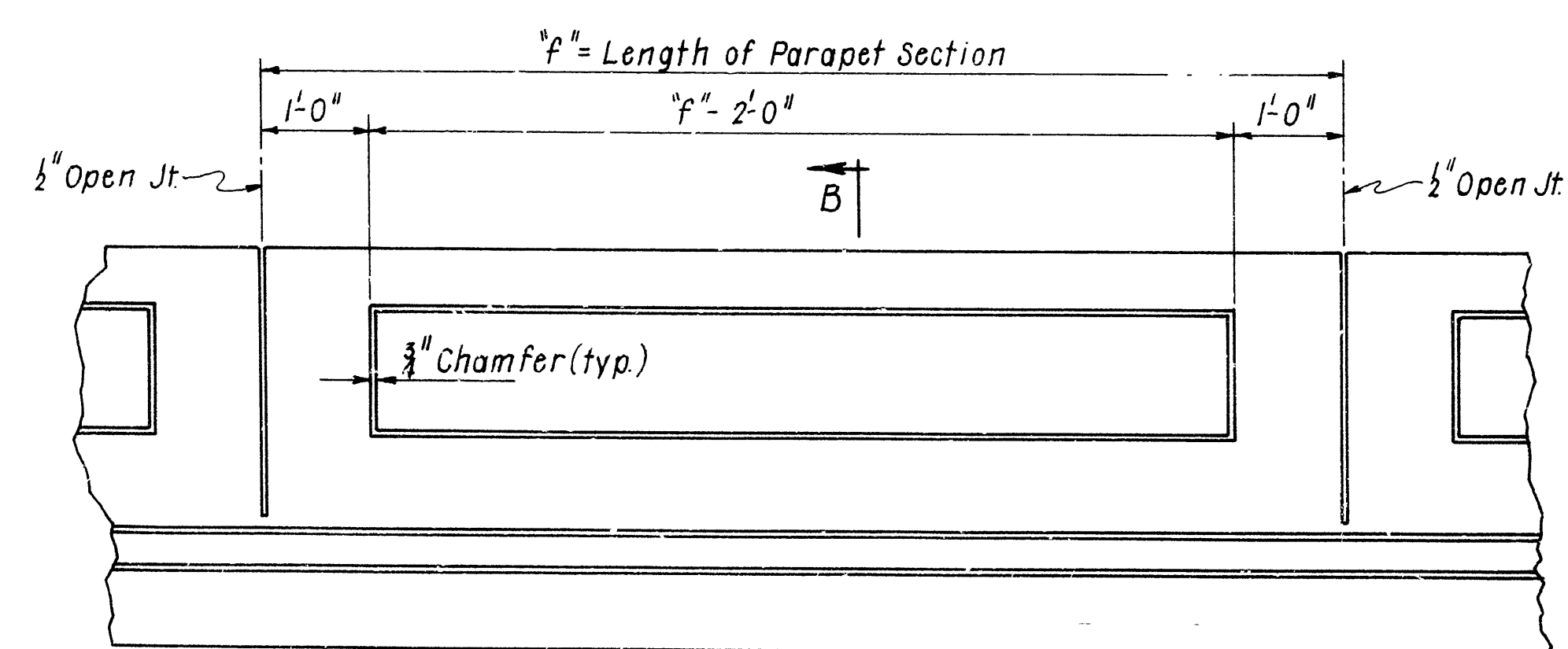
DEAD LOAD - COMPOSITE (INCLUDES 20 ±/FT. FOR FUTURE WEARING SURFACE) 287 ±/FT. 350 ±/FT.

LIVE LOAD TO COMPOSITE BEAM 1.59 WHEELS - 11' PACT 1.43 WHEELS - 11' PACT

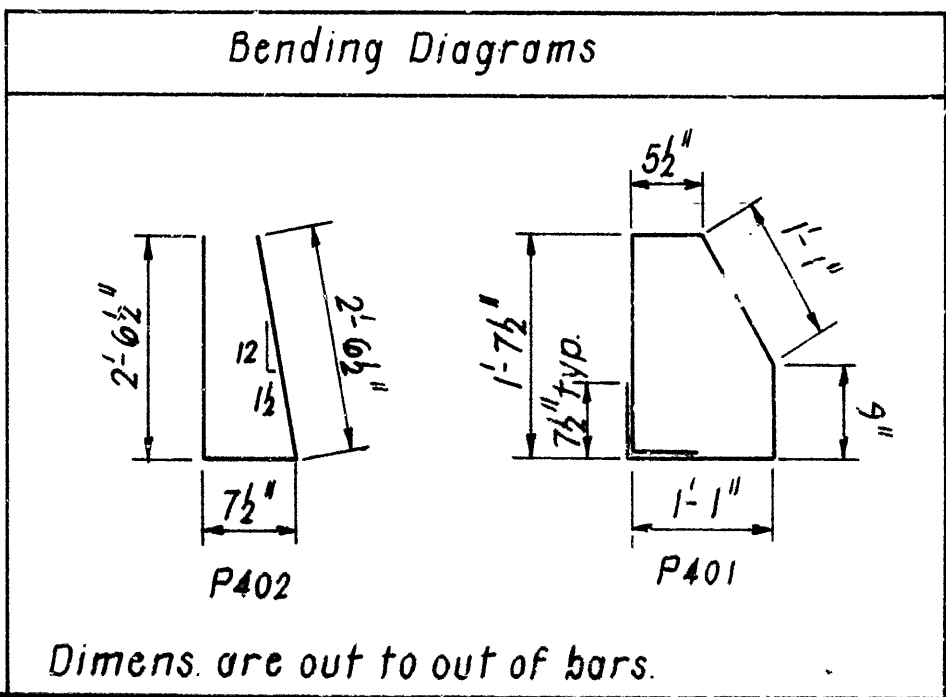


SECTION B-B
N.T.S.

SECTION A-A
1" = 1'-0"



ELEVATION SHOWING TREATMENT
FOR PARAPET RAIL
N.T.S.



Dimens. are out to out of bars.

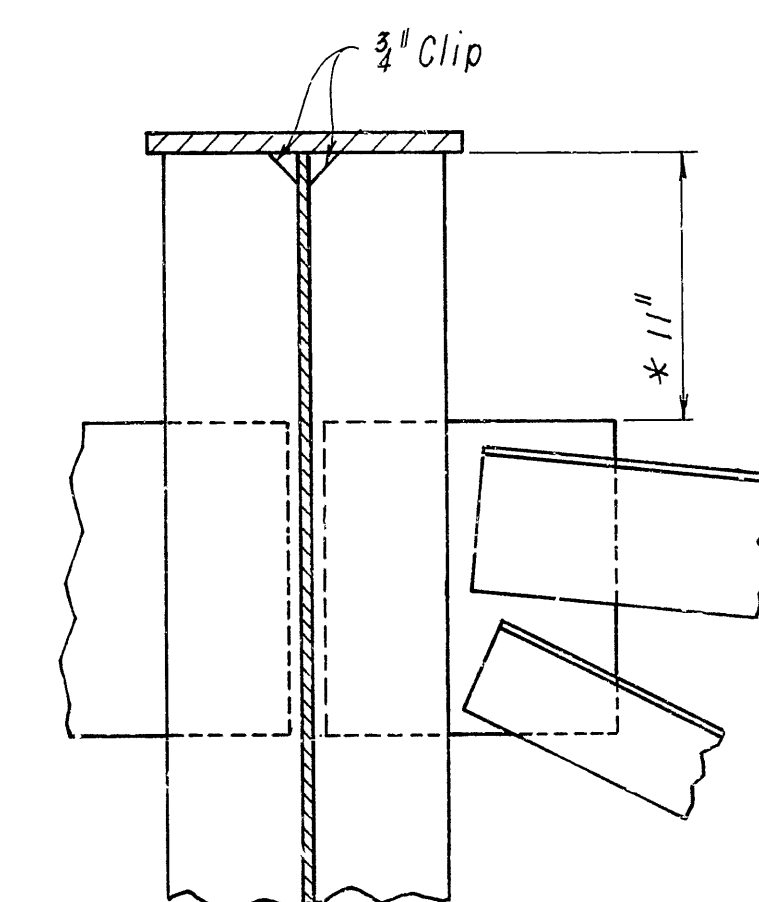
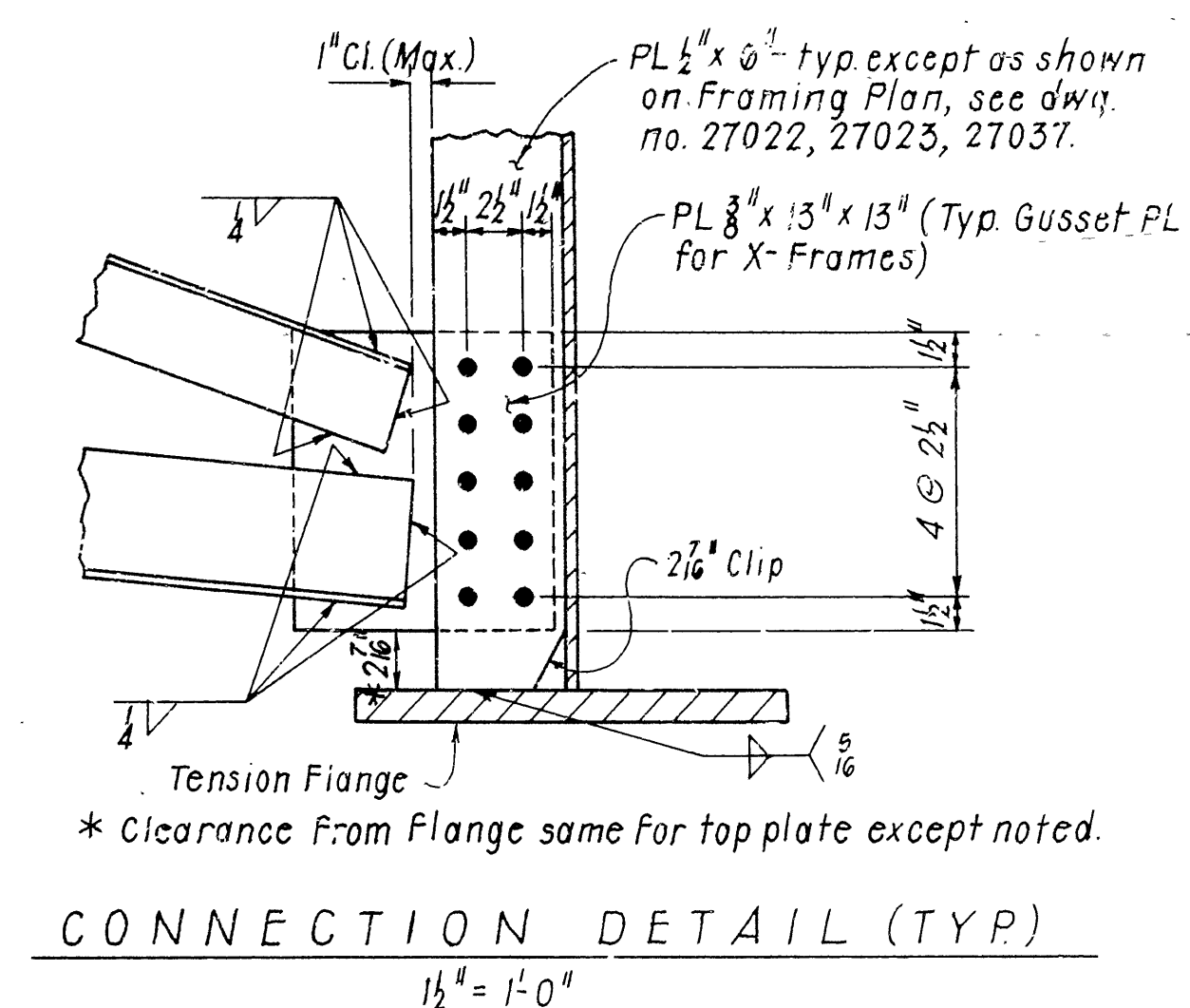
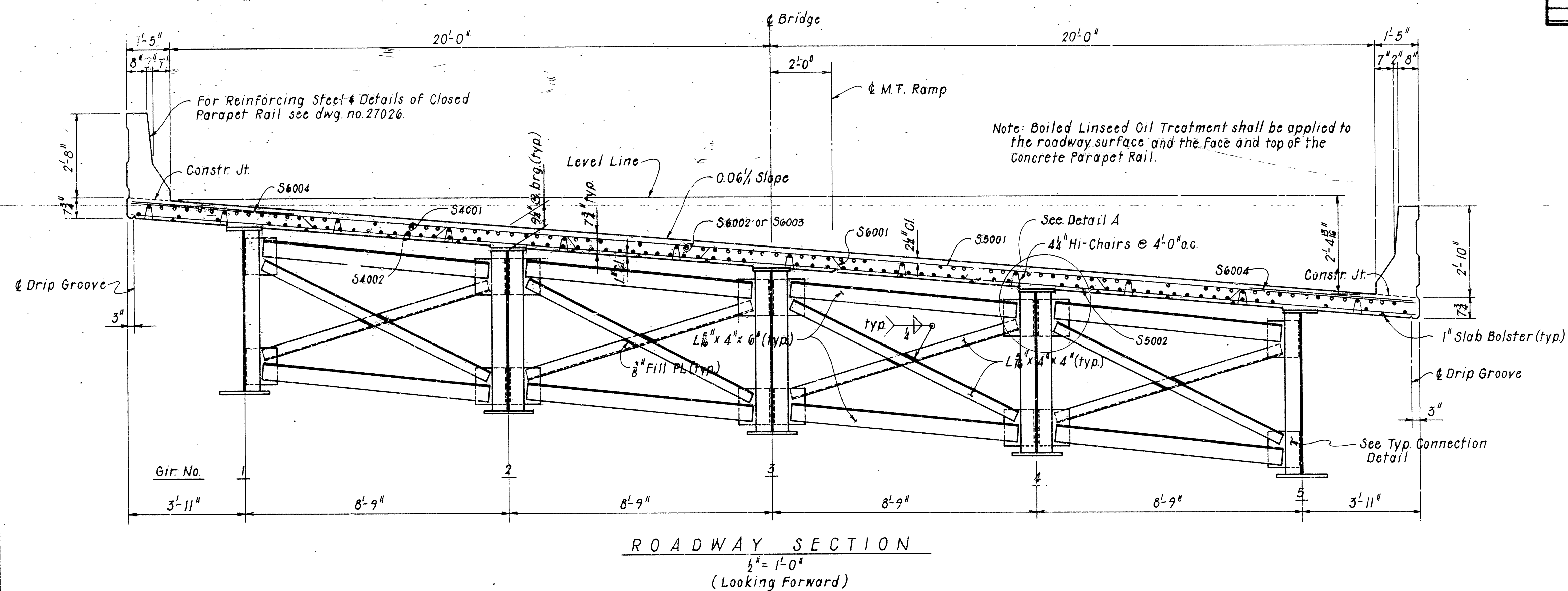
SHEET 2 OF 3
SUPPLEMENTARY DETAILS FOR
CONT. PL. GIRDER UNITS
M.T. RAMP BRIDGES
ROUTE SEC.

ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

DRAWN BY: H. May DATE: 10-31-84
CHECKED BY: H. D. DATE: 1-15-85
DESIGNED BY: DATE: SCALE: As Shown
BRIDGE NO. 6102 DRAWING NO. 27026

BRIDGE ENGINEER

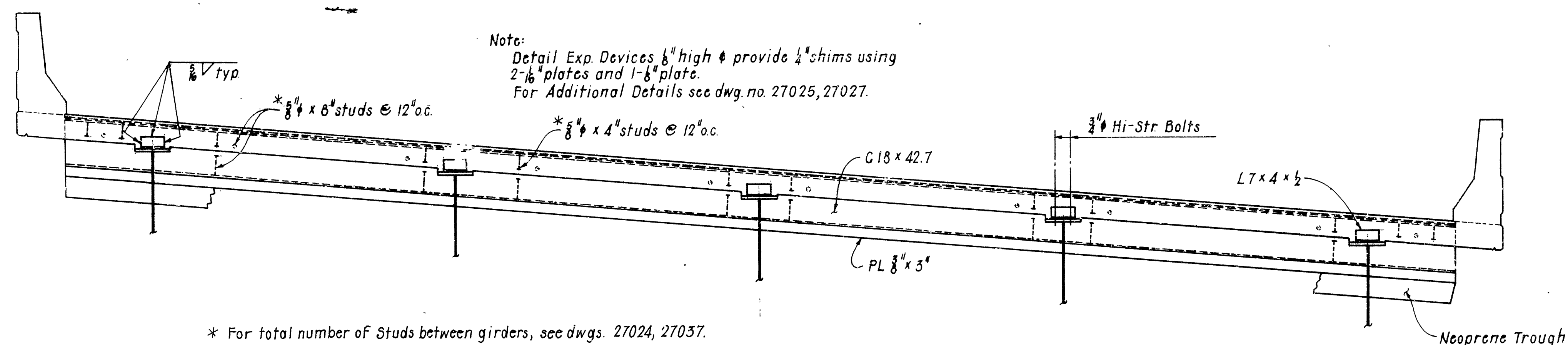
| DATE FILMED | DATE FILMED | DATE FILMED | DATE FILMED | FED. ROAD NO. | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|----------------|----------------|----------------|----------------|------------------|--------|--------------------|--------------|-----------------|
| | | | | 6 | ARK. | | | |
| | | | | JOB NO. | 110065 | 57 | 177 | |
| | | | | ① 6102 | SPAN 5 | 27027 | | |



* Clearance at top plate for connections at end brgs. only.

DETAIL A

$1_2'' = 1-0''$



* For total number of Studs between girders, see dwgs. 27024, 27037.

RDWY. SECTION AT EXP. DEVICE (TYP)
 $h'' = 1'-0''$

SHEET 3 OF 3
SUPPLEMENTARY DETAILS FOR
CONT. PL. GIRDER UNITS
M.T. RAMP BRIDGES
ROUTE SEC.
KANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

DRAWN BY: *H. Hag.* DATE: *1-10-83*
 CHECKED BY: *HJD* DATE: *1-15-85*
 DESIGNED BY: *GVA* DATE: *1-10-83*
 SCALE: *As Shown*
 BRIDGE NO. 6102
 6103
 DRAWING NO. 27027

Verbal Pinkerton