

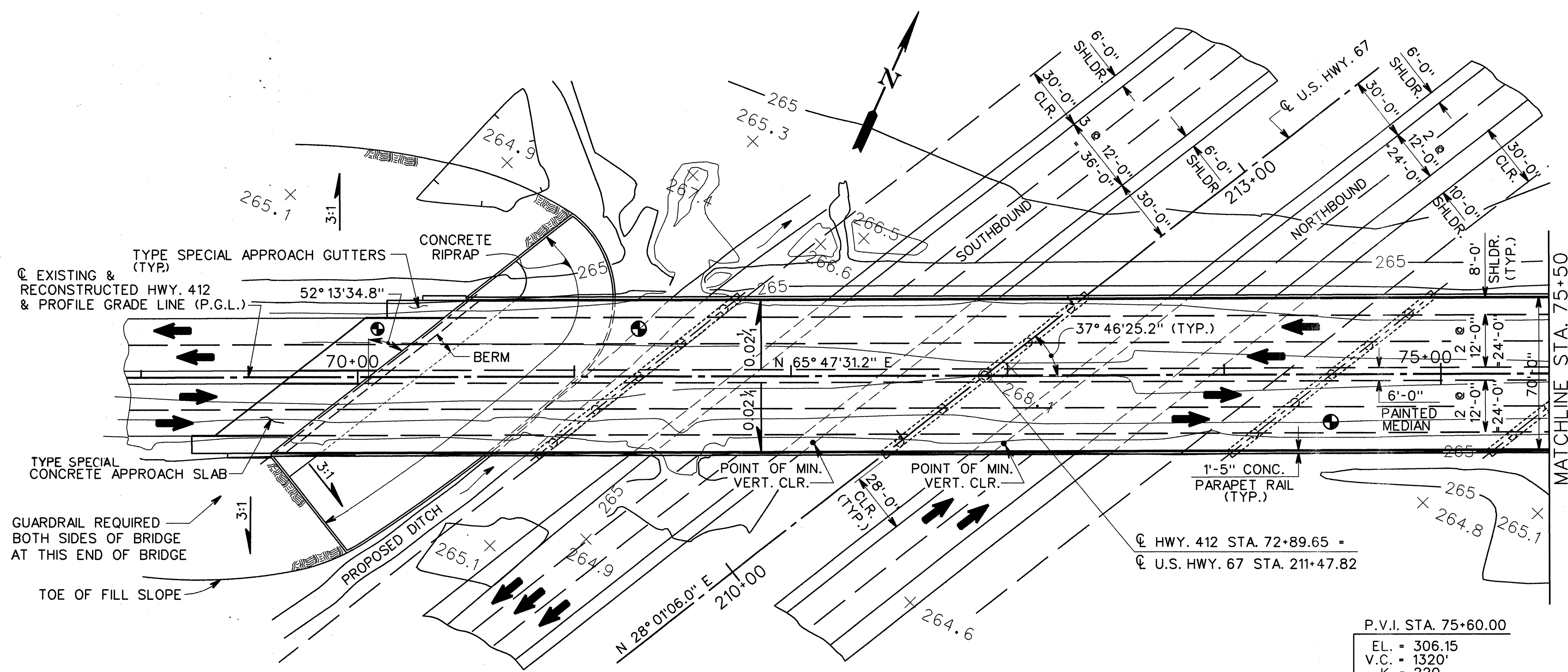
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK		47	306
				JOB NO.	R00106			
				BRIDGE NO.	TITLE			DWG. NO.
				6544	LAYOUT			35046

NOTES:

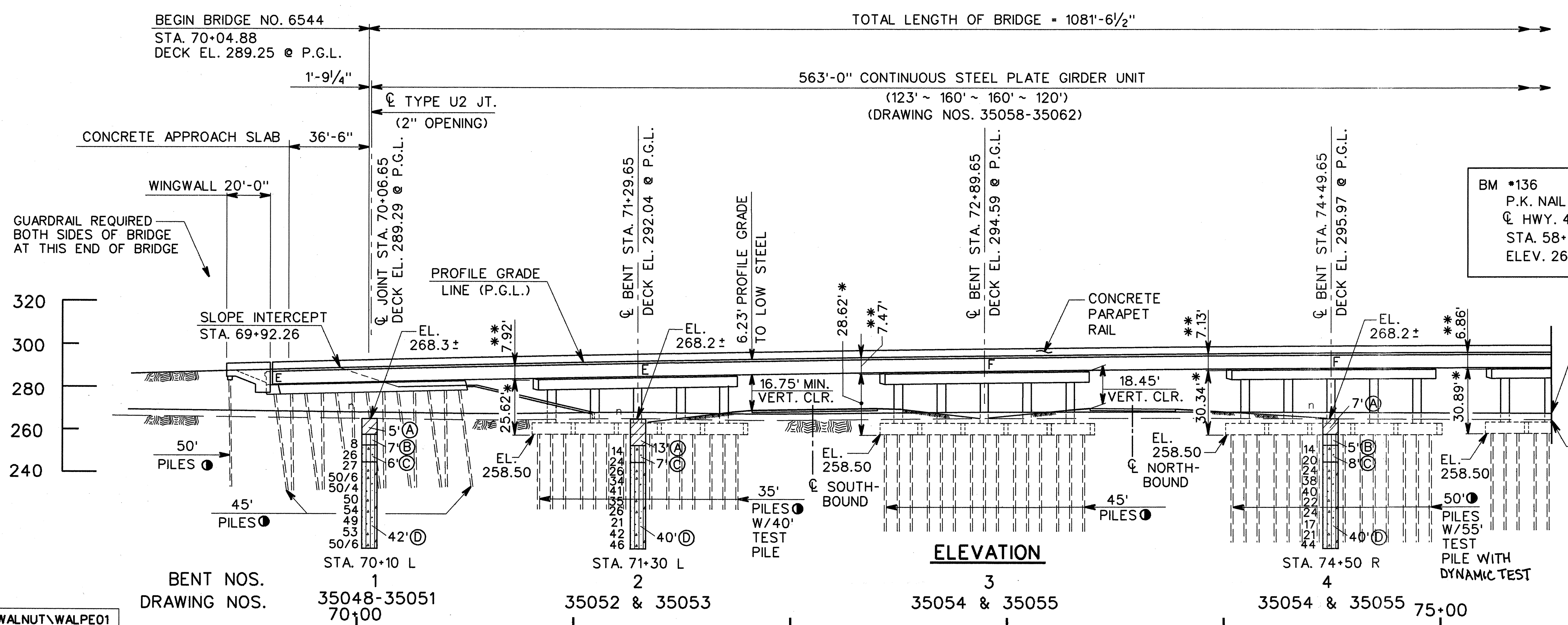
- * DIMENSION SHOWN IS MEASURED FROM THE TOP OF THE LOW END OF CAP TO THE BOTTOM OF FOOTING.
- ** DIMENSION SHOWN IS MEASURED FROM P.G.L. TO THE TOP OF THE LOW END OF CAP.
- ① LENGTH OF PILE SHOWN IS BASED ON 75 TON MINIMUM SAFE BEARING CAPACITY AND 16" DIA. CONCRETE FILLED STEEL SHELL PILING
- ALL DIMENSIONS AND STATIONS ARE ALONG U.S. HWY. 412.

LEGEND:

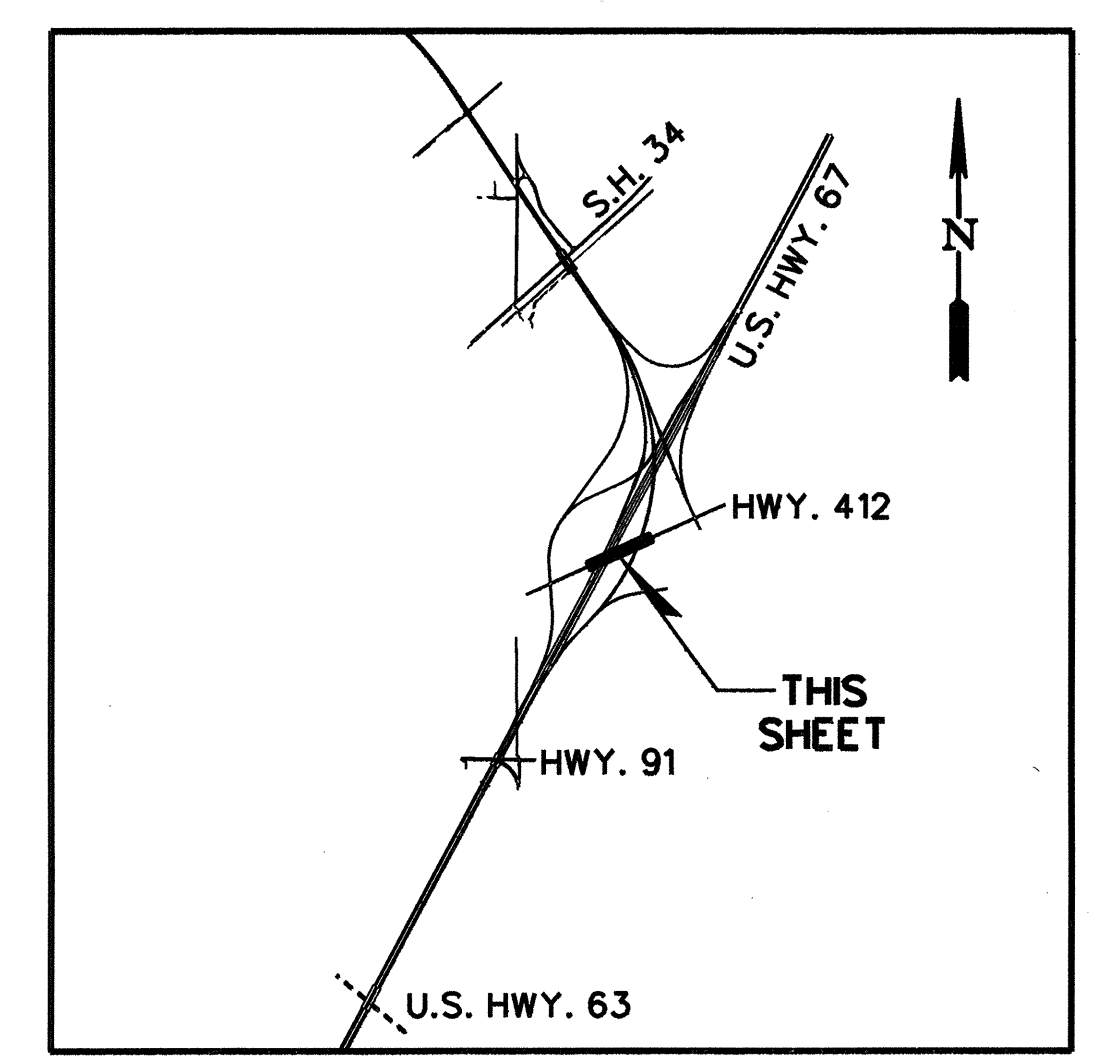
- BORING LOCATION
- Ⓐ SOFT TO STIFF TAN & GRAY CLAY AND SILTY CLAY
- Ⓑ LOOSE TO MEDIUM DENSE TAN & GRAY SANDY SILT
- Ⓒ LOOSE TO MEDIUM DENSE TAN, BROWN & GRAY SILTY FINE SAND
- Ⓓ MEDIUM DENSE TO DENSE GRAY SILTY FINE TO MEDIUM SAND W/SOME GRAVEL



PLAN



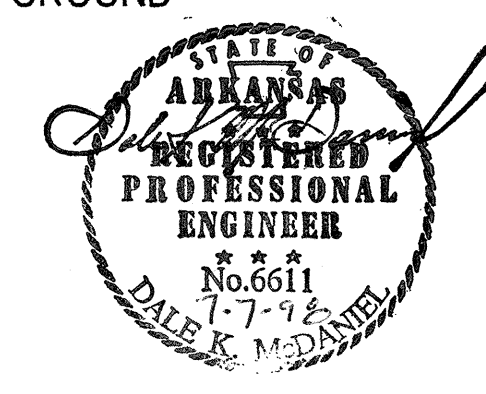
ELEVATION



SITE PLAN
N.T.S.

CTA/PB CRAFTON TULL & ASSOCIATES, INC.
PARSONS BRINCKERHOFF
A JOINT VENTURE ROGERS ARKANSAS

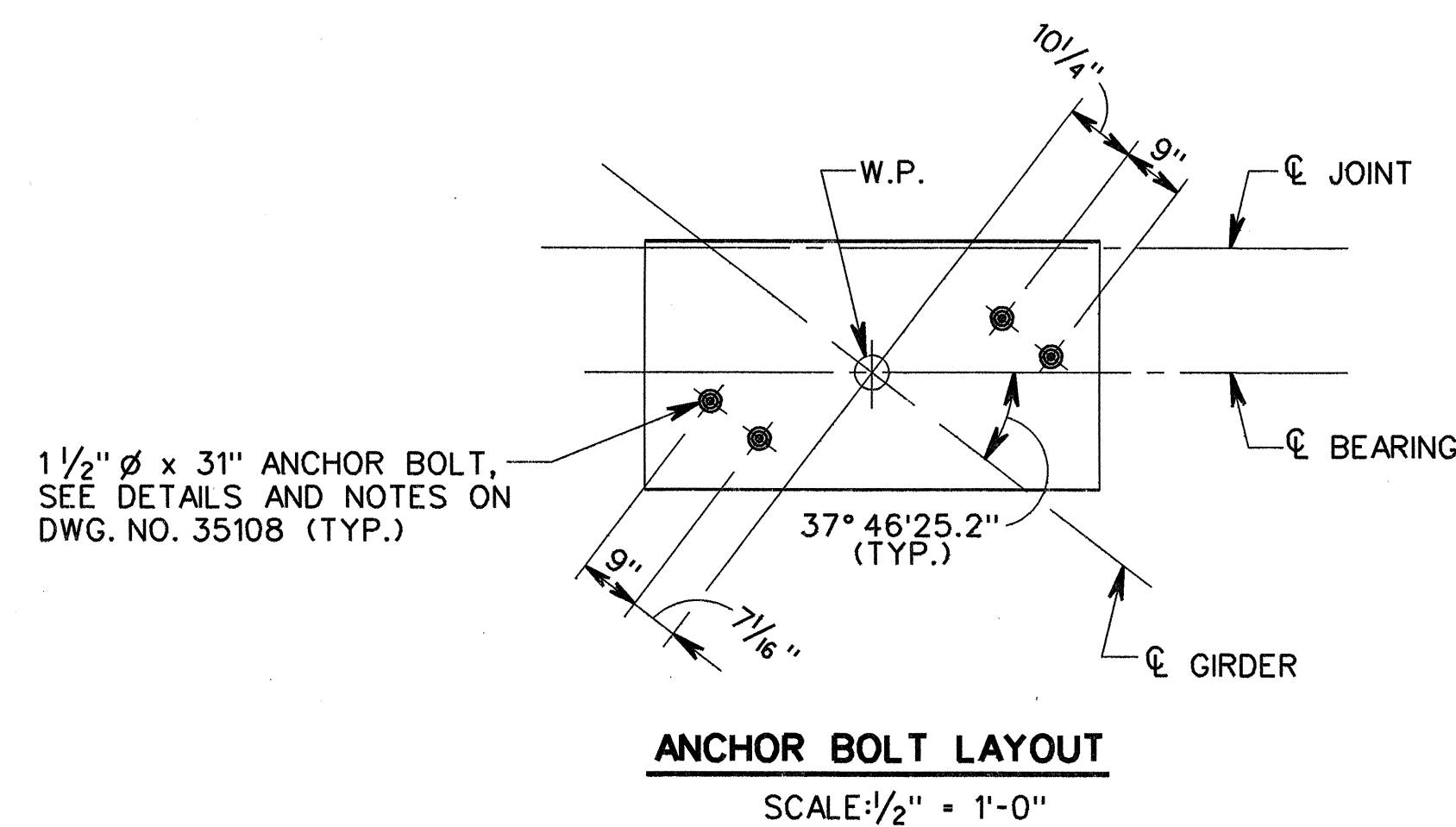
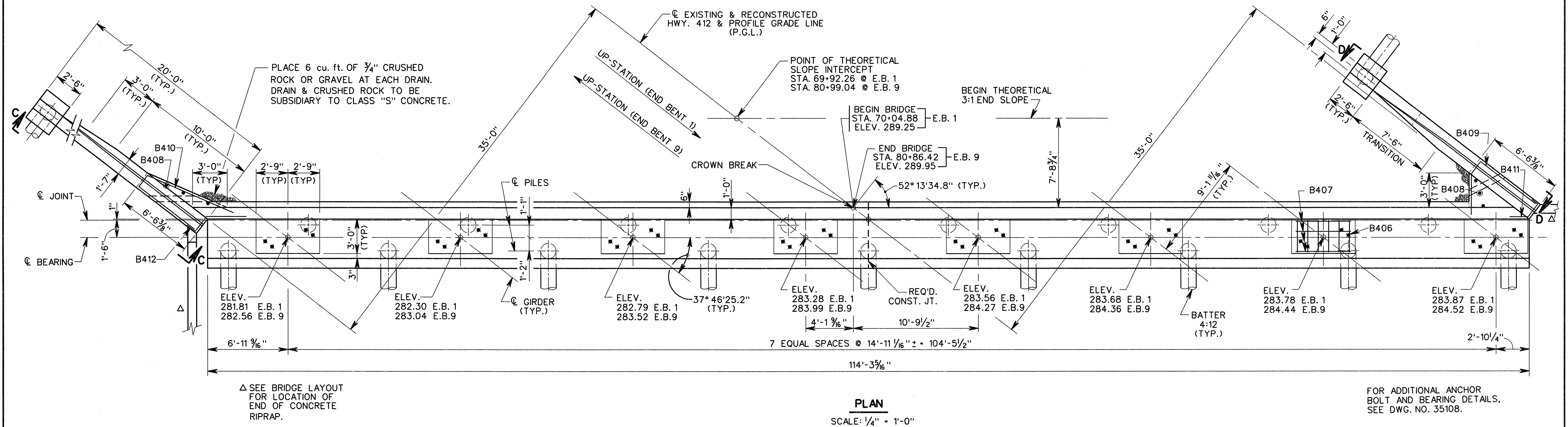
SHEET 1 OF 2
BRIDGE LAYOUT
HWY. 67/412 INTERCHANGE
U.S. HWY. 412 GRADE SEPARATION
LAWRENCE COUNTY
ROUTE 67 SEC. 16
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.
DRAWN BY: C.G.H. DATE: NOV. '95
CHECKED BY: D.K.M. DATE: OCT. '96
DESIGNED BY: D.K.M. DATE: JUN. '95
BRIDGE NO. 6544 DRAWING No. 35046



MICROFILMED
JAN 27 1999

WALNUT\WALPE01

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK		49	306
				JOB NO.	R00106			
				BRIDGE NO.	TITLE			DWG. NO.
				6544	END BENTS 1 & 9			35048



CTA/PB CRAFTON TULL & ASSOCIATES, INC.
PARSONS BRINCKERHOFF

A JOINT VENTURE ROGERS ARKANSAS

SHEET 1 OF 4

END BENTS 1 & 9

HWY. 67/412 INTERCHANGE
U.S. HWY. 412 GRADE SEPARATION
LAWRENCE COUNTY
ROUTE 67 SEC. 16

ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

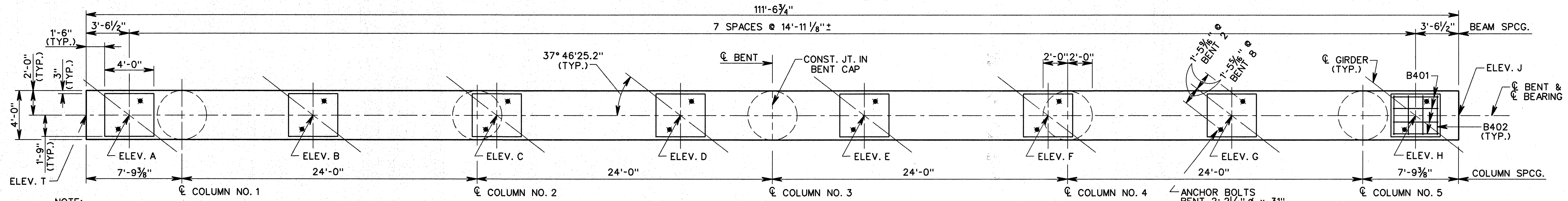
DRAWN BY: C.G.H. DATE: NOV. '95
CHECKED BY: D.K.M. DATE: OCT. '96
DESIGNED BY: A.B. DATE: JUN. '95

SCALE: AS SHOWN

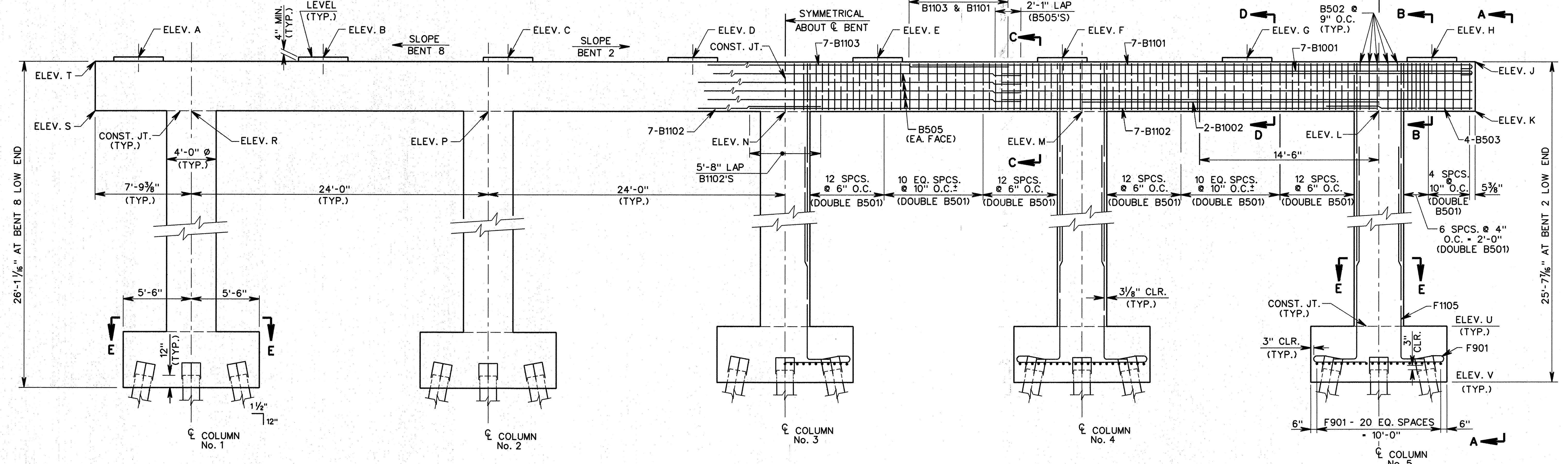
BRIDGE NO. 6544

DRAWING NO. 35048

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK		53	306
				JOB NO.	R00106			
				BRIDGE NO.	TITLE			DWG. NO.
				6544	BENTS 2 & 8			35052



PLAN
SCALE: 1/4" = 1'-0"



ELEVATION
SCALE: 1/4" = 1'-0"
(LOOKING AHEAD)

NOTES: FOR GENERAL NOTES, SEE DWG. NO. 35019
FOR VIEW A-A AND SECTIONS B-B, C-C, D-D & E-E, SEE DWG. NO. 35053
FOR BEARING DETAILS, SEE DWG. NO. 35107
FOR PILE ANCHOR DETAILS, SEE DWG. NO. 35111

CTA/PB CRAFTON TULL & ASSOCIATES, INC.
PARSONS BRINCKERHOFF
A JOINT VENTURE ROGERS ARKANSAS

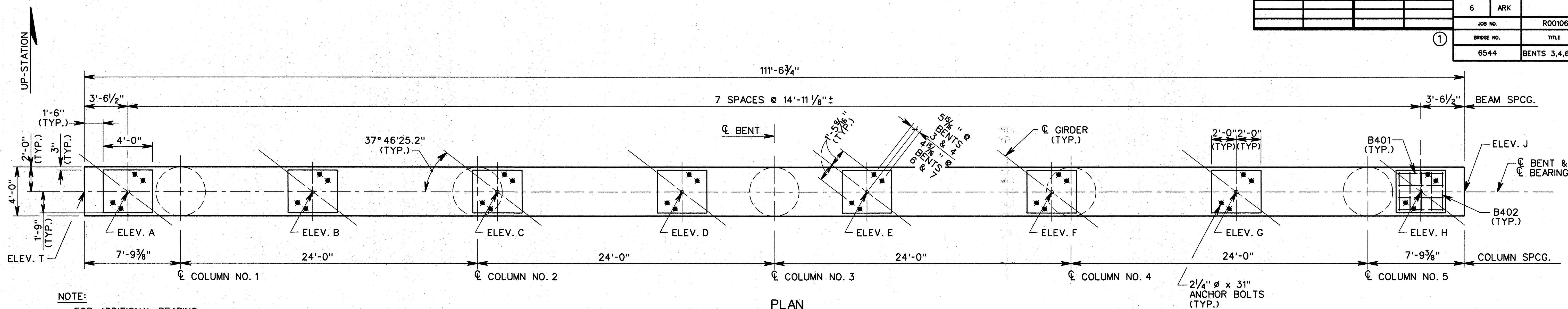
BENT VARIABLES

BENT	ELEV. A	ELEV. B	ELEV. C	ELEV. D	ELEV. E	ELEV. F	ELEV. G	ELEV. H	ELEV. J	ELEV. K	ELEV. L	ELEV. M	ELEV. N	ELEV. P	ELEV. R	ELEV. S	ELEV. T	ELEV. U	ELEV. V	SLOPE 1/2
2	286.15	286.13	286.09	286.05	285.82	285.40	284.97	284.54	284.12	280.12	280.24	280.61	280.99	281.36	281.73	281.85	285.85	263.00	258.50	0.0155
8	285.00	285.42	285.84	286.25	286.47	286.50	286.53	286.55	286.24	282.24	282.12	281.77	281.41	281.06	280.70	280.59	284.59	263.00	258.50	0.0148



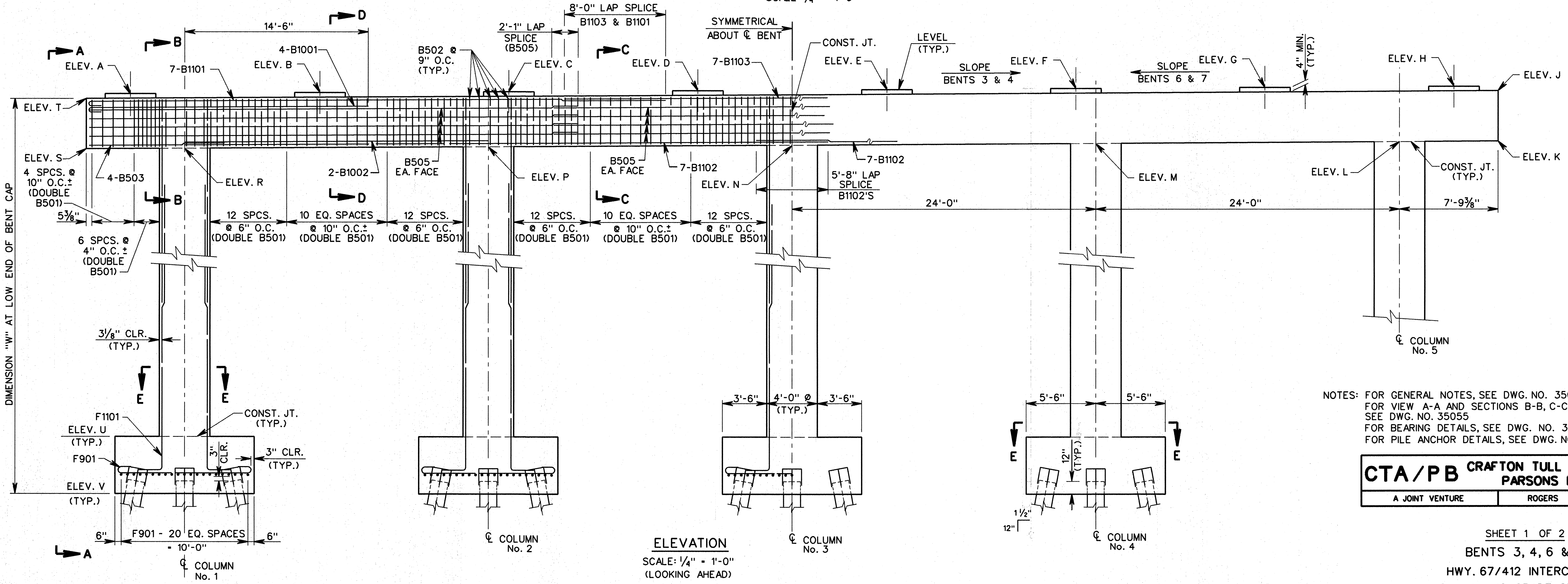
SHEET 1 OF 2
BENTS 2 & 8
HWY. 67/412 INTERCHANGE
U.S. HWY. 412 GRADE SEPARATION
LAWRENCE COUNTY
ROUTE 67 SEC. 16
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.
DRAWN BY: R.L.B. DATE: NOV. '95
CHECKED BY: D.K.M. DATE: OCT. '96
DESIGNED BY: K.M.M. DATE: JUN. '95
SCALE: AS SHOWN
BRIDGE NO. 6544 DRAWING NO. 35052

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AD PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK		55	306
				JOB NO.	R00106			
				BRIDGE NO.	TITLE			DWG. NO.
				6544	BENTS 3,4,6 & 7			35054



NOTE:
FOR ADDITIONAL BEARING
DETAILS, SEE DWG. NO. 35107

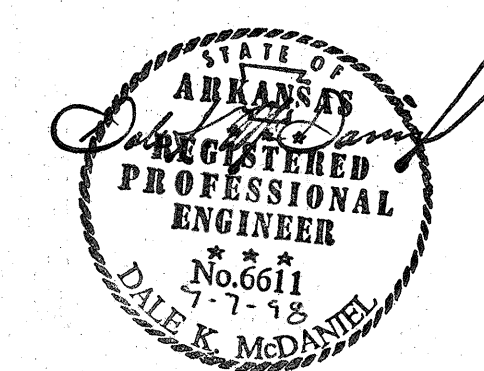
PLAN
SCALE: 1/4" = 1'-0"



NOTES: FOR GENERAL NOTES, SEE DWG. NO. 35019
FOR VIEW A-A AND SECTIONS B-B, C-C, D-D & E-E,
SEE DWG. NO. 35055
FOR BEARING DETAILS, SEE DWG. NO. 35107
FOR PILE ANCHOR DETAILS, SEE DWG. NO. 35111

CTA/PB CRAFTON TULL & ASSOCIATES, INC.
PARSONS BRINCKERHOFF
A JOINT VENTURE ROGERS ARKANSAS

SHEET 1 OF 2
BENTS 3, 4, 6 & 7
HWY. 67/412 INTERCHANGE
U.S. HWY. 412 GRADE SEPARATION
LAWRENCE COUNTY
ROUTE 67 SEC. 16
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.
DRAWN BY: C.G.H. DATE: NOV. '95
CHECKED BY: D.K.M. DATE: OCT. '96
DESIGNED BY: K.M.M. DATE: JUN. '95
SCALE: AS SHOWN
BRIDGE NO. 6544 DRAWING NO. 35054

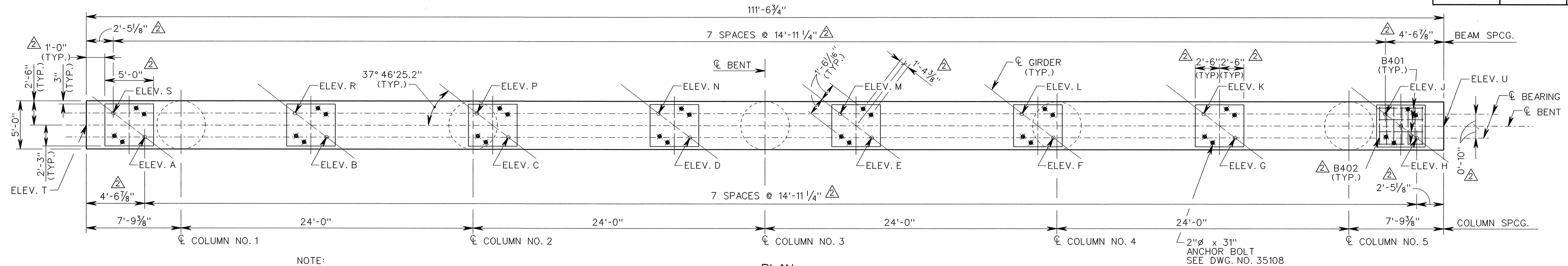


BENT VARIABLES

	ELEV. A	ELEV. B	ELEV. C	ELEV. D	ELEV. E	ELEV. F	ELEV. G	ELEV. H	ELEV. J	ELEV. K	ELEV. L	ELEV. M	ELEV. N	ELEV. P	ELEV. R	ELEV. S	ELEV. T	ELEV. U	ELEV. V	SLOPE 1/2	DIM. "W"
BENT 3	288.52	288.58	288.63	288.67	288.53	288.19	287.85	287.51	287.12	283.12	283.20	283.43	283.66	283.90	284.13	284.21	288.21	263.00	258.50	0.0097	28'-7 7/16"
BENT 4	289.61	289.75	289.88	290.01	289.96	289.71	289.45	289.19	288.84	284.84	284.87	284.96	285.06	285.15	285.25	285.28	289.28	263.00	258.50	0.0040	30'-4 1/16"
BENT 6	289.13	289.39	289.66	289.91	289.98	289.85	289.72	289.59	289.26	285.26	285.23	285.12	285.02	284.91	284.81	284.77	288.77	263.00	258.50	0.0044	30'-3 3/4"
BENT 7	287.57	287.92	288.26	288.59	288.73	288.69	288.63	288.58	288.26	284.26	284.18	283.95	283.72	283.49	283.26	283.19	287.19	263.00	258.50	0.0096	28'-8 1/4"

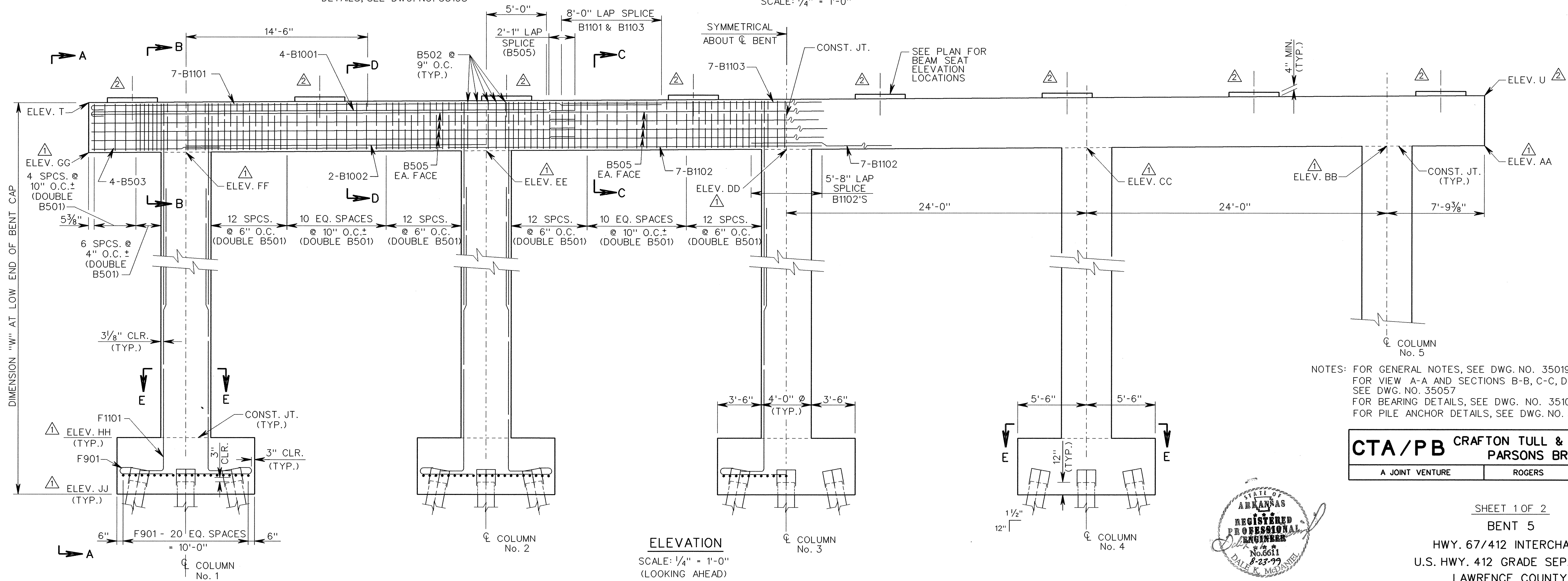
WALNUT\WALBD59

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
9-14-99	6-27-00			6	ARK		51	306
				JOB NO.	R00106			
				BRIDGE NO.	TITLE		DWG. NO.	
				6544	BENT 5		35056	



PLAN

SCALE: 1/4" = 1'-0"



ELEVATION

SCALE: 1/4" = 1'-0"
(LOOKING AHEAD)

BENT VARIABLES

BENT	ELEV. A	ELEV. B	ELEV. C	ELEV. D	ELEV. E	ELEV. F	ELEV. G	ELEV. H	ELEV. J	ELEV. K	ELEV. L	ELEV. M	ELEV. N	ELEV. P	ELEV. R	ELEV. S	ELEV. T	ELEV. U	EL. AA	EL. BB	EL. CC	EL. DD	EL. EE	EL. FF	EL. GG	EL. HH	EL. JJ
5	289.72	289.93	290.13	290.32	290.33	290.14	289.95	289.75	289.75	289.95	290.14	290.32	290.32	290.12	289.92	289.72	289.39	289.43	285.43	285.43	285.42	285.41	285.40	285.39	285.39	263.00	258.50

NOTES: FOR GENERAL NOTES, SEE DWG. NO. 35019
FOR VIEW A-A AND SECTIONS B-B, C-C, D-D & E-E,
SEE DWG. NO. 35057
FOR BEARING DETAILS, SEE DWG. NO. 35108
FOR PILE ANCHOR DETAILS, SEE DWG. NO. 35111

CTA/PB CRAFTON TULL & ASSOCIATES, INC.
PARSONS BRINCKERHOFF

A JOINT VENTURE ROGERS ARKANSAS

SHEET 1 OF 2

BENT 5

HWY. 67/412 INTERCHANGE
U.S. HWY. 412 GRADE SEPARATION
LAWRENCE COUNTY
ROUTE 67 SEC. 16

ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

DRAWN BY: R.L.B. DATE: NOV. '95
CHECKED BY: D.K.M. DATE: OCT. '96
DESIGNED BY: K.M.M. DATE: JUN. '95

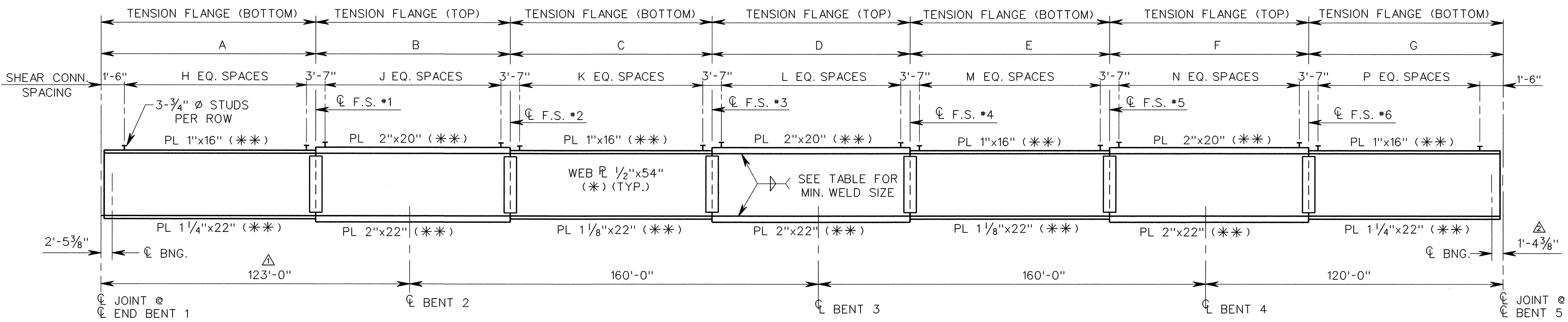
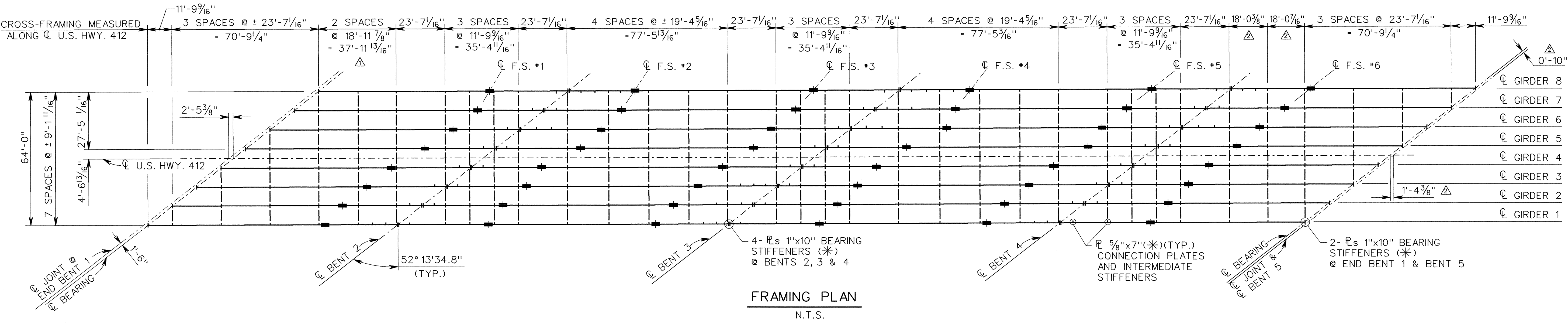
SCALE: AS SHOWN

BRIDGE NO. 6544

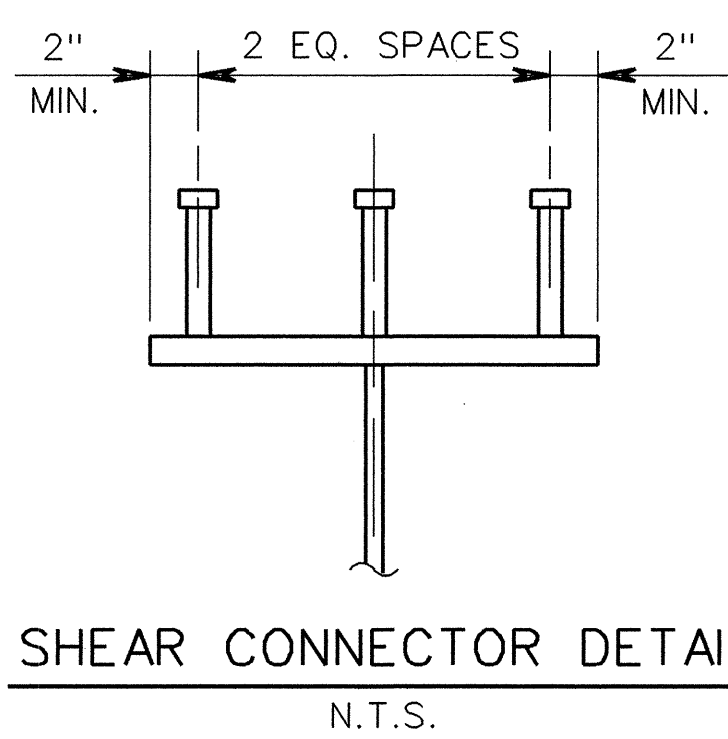
DRAWING NO. 35056

REVISED ELEVATION DESIGNATION 3-17-99 D.K.M.
REVISIONS BY D.K.M. 8-23-99 9-14-99

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
7-14-99	6-27-00			6	ARK		59	306
				JOB NO.	R00106			
				BRIDGE NO.	TITLE		DWG. NO.	
				6544	GIRDER DETAILS		35058	



	A	B	C	D	E	F	G	H	J	K	L	M	N	P
GIRDER #1	90'-9"	74'-0 3/8"	84'-11 5/8"	76'-3"	85'-3 3/4"	69'-11"	81'-9 1/4"	95	47	66	49	66	45	86
GIRDER #2	85'-3"	76'-1 1/2"	81'-9"	78'-3"	83'-11 1/8"	76'-0"	81'-7 5/8"	89	49	63	50	65	49	86
GIRDER #3	84'-0 3/8"	77'-4 1/4"	81'-9 5/8"	78'-2 3/4"	83'-2 1/8"	76'-10"	81'-6 1/8"	87	50	63	50	64	49	86
GIRDER #4	83'-1 1/4"	73'-6 3/4"	87'-8 1/2"	74'-5"	85'-10 1/8"	72'-6 5/8"	85'-9 3/4"	86	47	68	48	66	46	91
GIRDER #5	87'-2 1/2"	74'-2"	86'-0 3/4"	74'-7"	86'-5 3/4"	73'-6 3/4"	80'-11 1/4"	91	48	66	48	67	48	85
GIRDER #6	90'-7 1/4"	70'-5 5/8"	83'-6 1/2"	84'-4"	75'-8"	76'-6 3/4"	81'-9 7/8"	95	45	64	54	58	49	86
GIRDER #7	90'-7 1/4"	69'-9 7/8"	84'-2 1/4"	77'-6 3/4"	82'-5 1/2"	78'-2 3/8"	80'-2"	95	45	65	50	64	50	84
GIRDER #8	83'-0 5/8"	71'-6 5/8"	86'-0 1/2"	75'-9"	82'-11 3/4"	83'-3"	80'-4 1/2"	86	46	66	49	65	54	85



- NOTES:
1. FOR GENERAL NOTES, SEE DWG. NO. 35019.
 2. FOR FIELD SPLICE DETAILS, SEE DWG. NO. 35059.
 3. FOR CROSS-FRAME DETAILS, SEE DWG. NO. 35059.
 4. FOR BEARING PAD DETAILS, SEE DWG. NOS. 35107 & 35108.
 5. FOR JOINT DETAILS, SEE DWG. NO. 35110.
- * (AASHTO M270 - GRADE 36)
** (AASHTO M270 - GRADE 50)

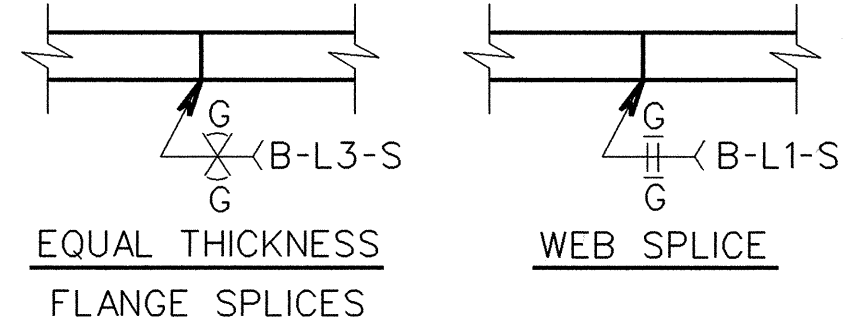
CTA/PB CRAFTON TULL & ASSOCIATES, INC.
PARSONS BRINCKERHOFF

A JOINT VENTURE	ROGERS	ARKANSAS
-----------------	--------	----------

SHEET 1 OF 2
UNIT 1-4 CONT. PL GIRDER DETAILS
HWY. 67/412 INTERCHANGE
U.S. HWY. 412 GRADE SEPARATION
LAWRENCE COUNTY
ROUTE 67 SEC. 16
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

DRAWN BY: C.G.H. DATE: NOV. 95
CHECKED BY: D.K.M. DATE: OCT. 96
DESIGNED BY: A.B. DATE: JUN. 95
SCALE: AS SHOWN

BRIDGE NO. 6544 DRAWING NO. 35058



DETAILS OF WELDED SHOP SPLICES

LOAD DISTRIBUTION TO:	INTERIOR GIRDER	EXTERIOR GIRDER
DEAD LOAD (NON-COMPOSITE)	1.299 K _{FT}	1.284 K _{FT}
DEAD LOAD (COMPOSITE)	0.280 K _{FT}	0.244 K _{FT}
LIVE LOAD TO COMPOSITE BEAM	1.662 WHEELS + IMPACT	1.686 WHEELS + IMPACT

NON-COMPOSITE DEAD LOAD INCLUDES APPROXIMATELY 0.338 K_{FT} STEEL WEIGHT.





TABLE FOR WELD		
MATERIAL THICKNESS OF THICKER PART JOINED (INCHES)	MINIMUM SIZE OF FILLET WELD (INCHES)	SINGLE PASS WELD MUST BE USED
INCLUSIVE TO 3/4"	1/4"	
OVER 3/4"	5/16"	

NOTE: WHEN A FILLET WELD SIZE, AS SHOWN ON THE PLANS, IS LARGER THAN THE MINIMUM, THE FIRST PASS SHALL BE THAT SPECIFIED FOR MINIMUM SIZE OF FILLET WELD.

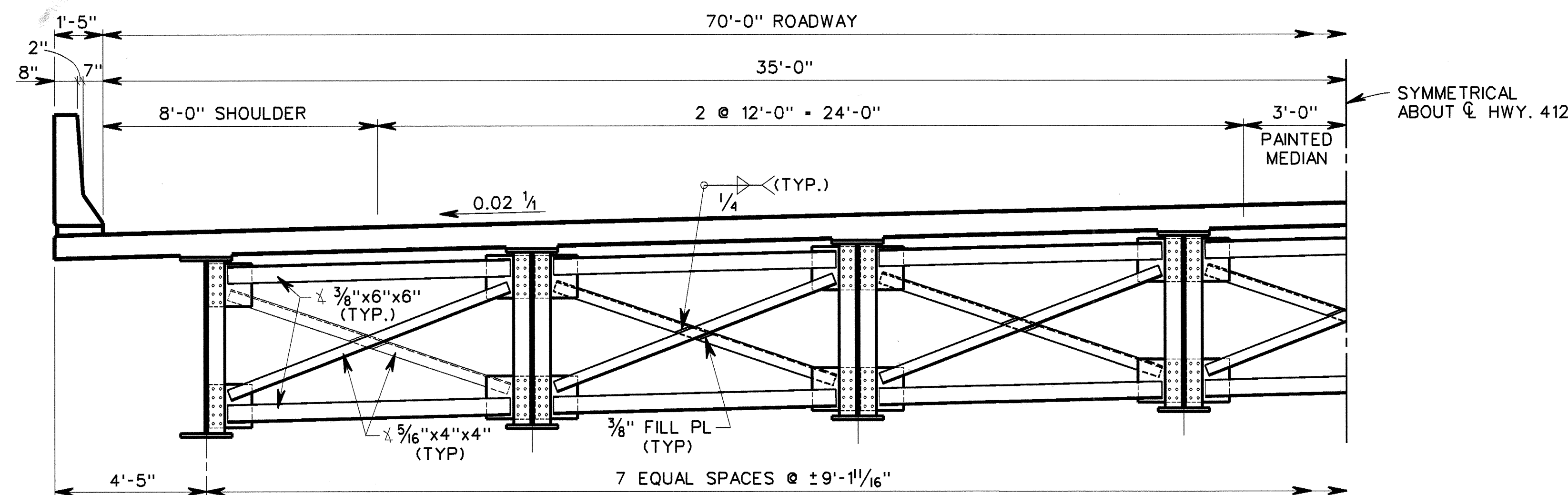
REVISIONS BY D.K.M. 08-23-99 9-14-99
REVISED DIMENSIONS AND NUMBER OF SPACES 04-12-99 D.K.M.

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
5-24-99	5-27-99			6	ARK	NH-0038(22)	60	306
				JOB NO.	R00106			
				BRIDGE NO.	TITLE			DWG. NO.
				6544	GIRDER DETAILS			35059

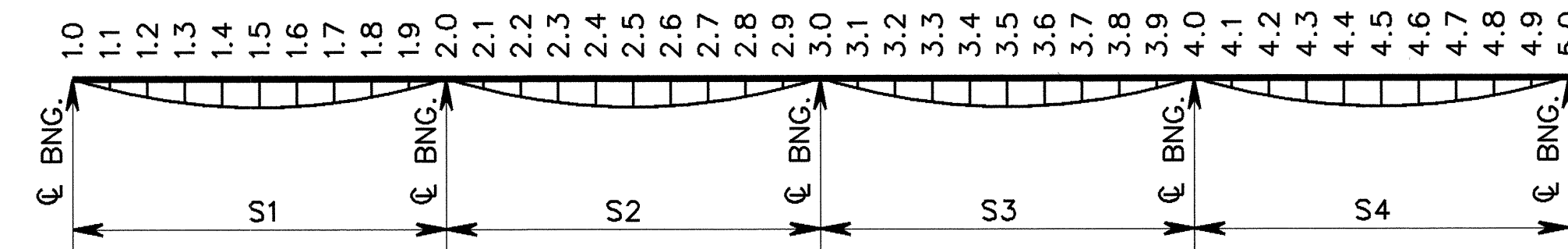
TABLE OF DEFLECTIONS (IN.)

SPAN POINT		1.0	1.1	1.2	1.3	1.4	1.5	1.6	1.7	1.8	1.9	2.0	2.1	2.2	2.3	2.4	2.5	2.6	2.7	2.8	2.9	3.0	3.1	3.2	3.3	3.4	3.5	3.6	3.7	3.8	3.9	4.0	4.1	4.2	4.3	4.4	4.5	4.6	4.7	4.8	4.9	5.0
INTERIOR GIRDERS	STRUCTURAL STEEL	0.000	-0.141	-0.258	-0.334	-0.362	-0.340	-0.269	-0.187	-0.095	-0.023	0.000	-0.082	-0.230	-0.391	-0.500	-0.546	-0.493	-0.357	-0.199	-0.059	0.000	-0.081	-0.233	-0.403	-0.524	-0.583	-0.538	-0.407	-0.244	-0.087	0.000	-0.023	-0.096	-0.189	-0.281	-0.348	-0.359	-0.332	-0.266	-0.146	0.000
	STRUCTURAL STEEL & SLAB 	0.000	-0.735	-1.351	-1.740	-1.883	-1.770	-1.389	-0.978	-0.510	-0.136	0.000	-0.389	-1.080	-1.846	-2.375	-2.608	-2.364	-1.727	-0.974	-0.302	0.000	-0.337	-1.008	-1.784	-2.354	-2.641	-2.454	-1.859	-1.120	-0.409	0.000	-0.071	-0.361	-0.749	-1.143	-1.445	-1.512	-1.410	-1.140	-0.627	0.000
	STRUCTURAL STEEL SLAB & PARAPET 	0.000	-0.806	-1.478	-1.900	-2.052	-1.923	-1.505	-1.054	-0.543	-0.139	0.000	-0.441	-1.216	-2.066	-2.648	-2.898	-2.619	-1.907	-1.072	-0.329	0.000	-0.383	-1.135	-1.996	-2.623	-2.932	-2.716	-2.053	-1.234	-0.449	0.000	-0.082	-0.404	-0.832	-1.264	-1.592	-1.663	-1.547	-1.249	-0.686	0.000
EXTERIOR GIRDERS	STRUCTURAL STEEL	0.000	-0.164	-0.301	-0.392	-0.427	-0.404	-0.332	-0.229	-0.126	-0.049	0.000	-0.074	-0.223	-0.399	-0.538	-0.594	-0.537	-0.400	-0.226	-0.072	0.000	-0.077	-0.233	-0.419	-0.573	-0.648	-0.601	-0.471	-0.291	-0.115	0.000	-0.012	-0.080	-0.175	-0.274	-0.352	-0.384	-0.361	-0.270	-0.149	0.000
	STRUCTURAL STEEL & SLAB 	0.000	-0.842	-1.542	-2.010	-2.189	-2.075	-1.699	-1.179	-0.649	-0.256	0.000	-0.349	-1.047	-1.881	-2.555	-2.854	-2.588	-1.948	-1.122	-0.377	0.000	-0.308	-0.991	-1.841	-2.571	-2.946	-2.755	-2.170	-1.354	-0.544	0.000	-0.012	-0.271	-0.659	-1.078	-1.423	-1.586	-1.509	-1.134	-0.629	0.000
	STRUCTURAL STEEL SLAB & PARAPET 	0.000	-0.978	-1.791	-2.336	-2.547	-2.421	-1.991	-1.388	-0.769	-0.306	0.000	-0.397	-1.208	-2.181	-2.973	-3.332	-3.036	-2.301	-1.340	-0.459	0.000	-0.350	-1.145	-2.136	-2.990	-3.432	-3.220	-2.545	-1.594	-0.644	0.000	-0.134	-0.317	-0.772	-1.262	-1.664	-1.854	-1.764	-1.326	-0.736	0.000

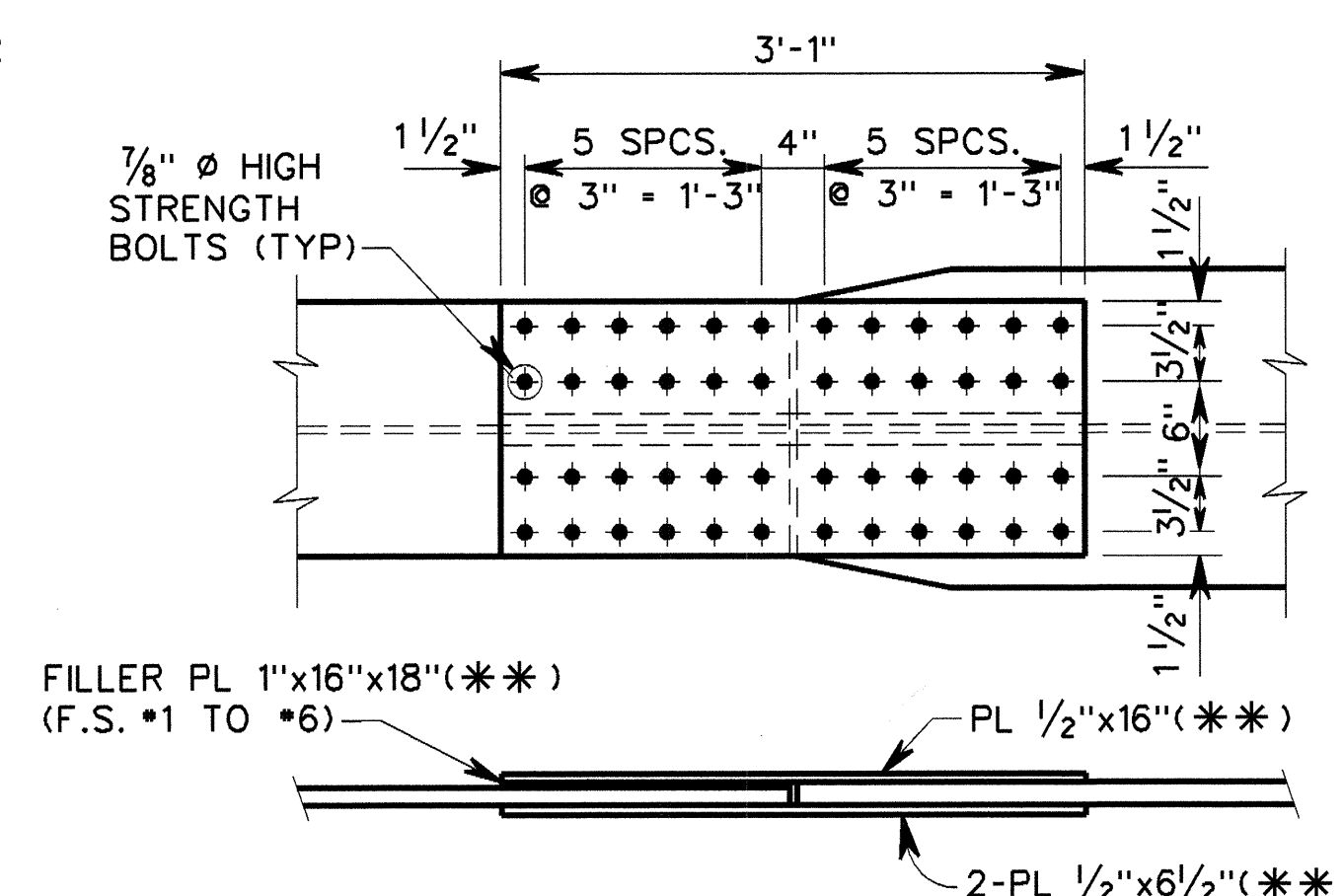
NOTE:
SIGN CONVENTIONS FOR DEFLECTIONS IS AS FOLLOWS:
(+) IS UPWARD
(-) IS DOWNWARD



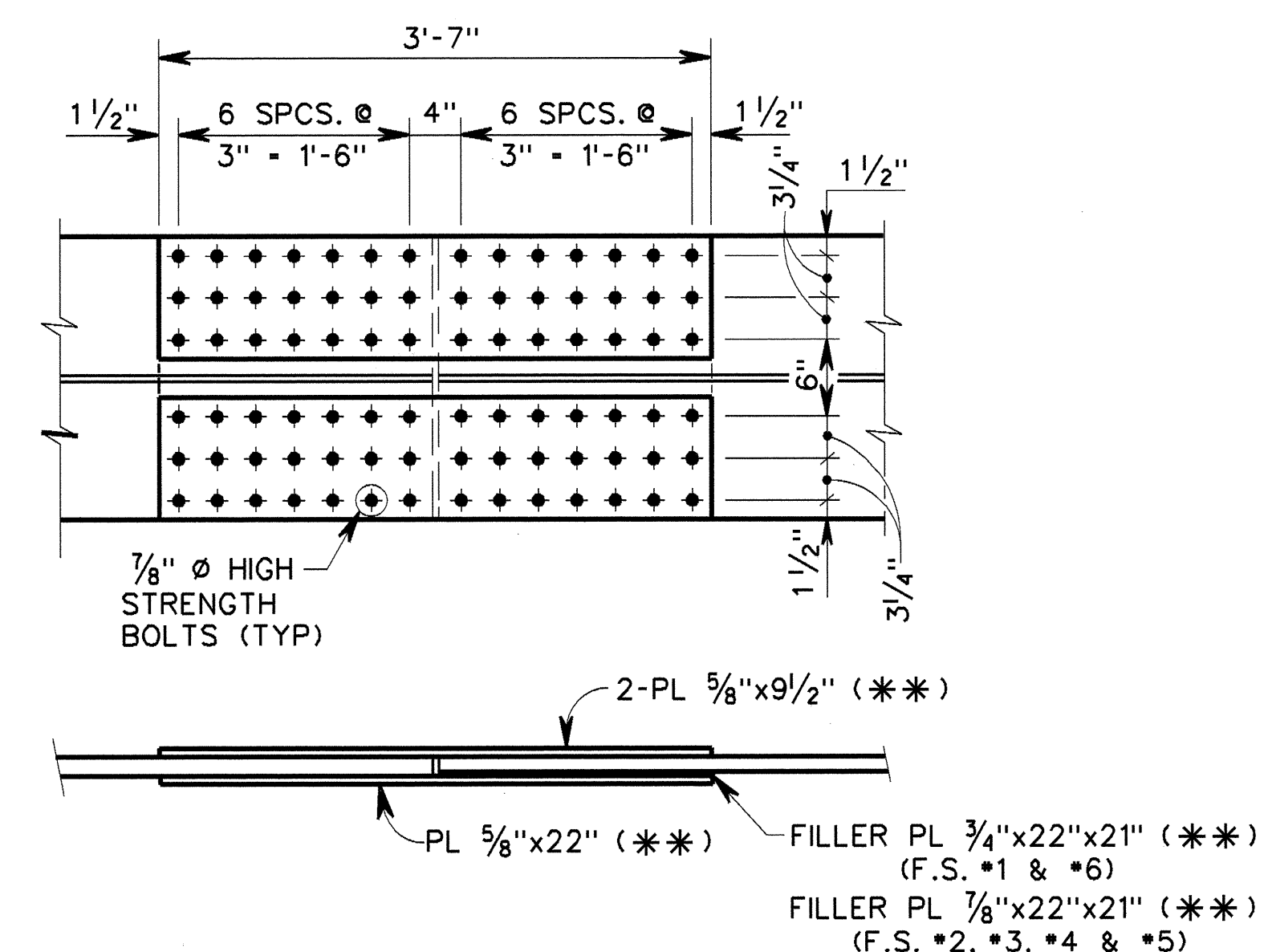
ROADWAY SECTION
SCALE: 3/8" = 1'-0"
(LOOKING UPSTATION)



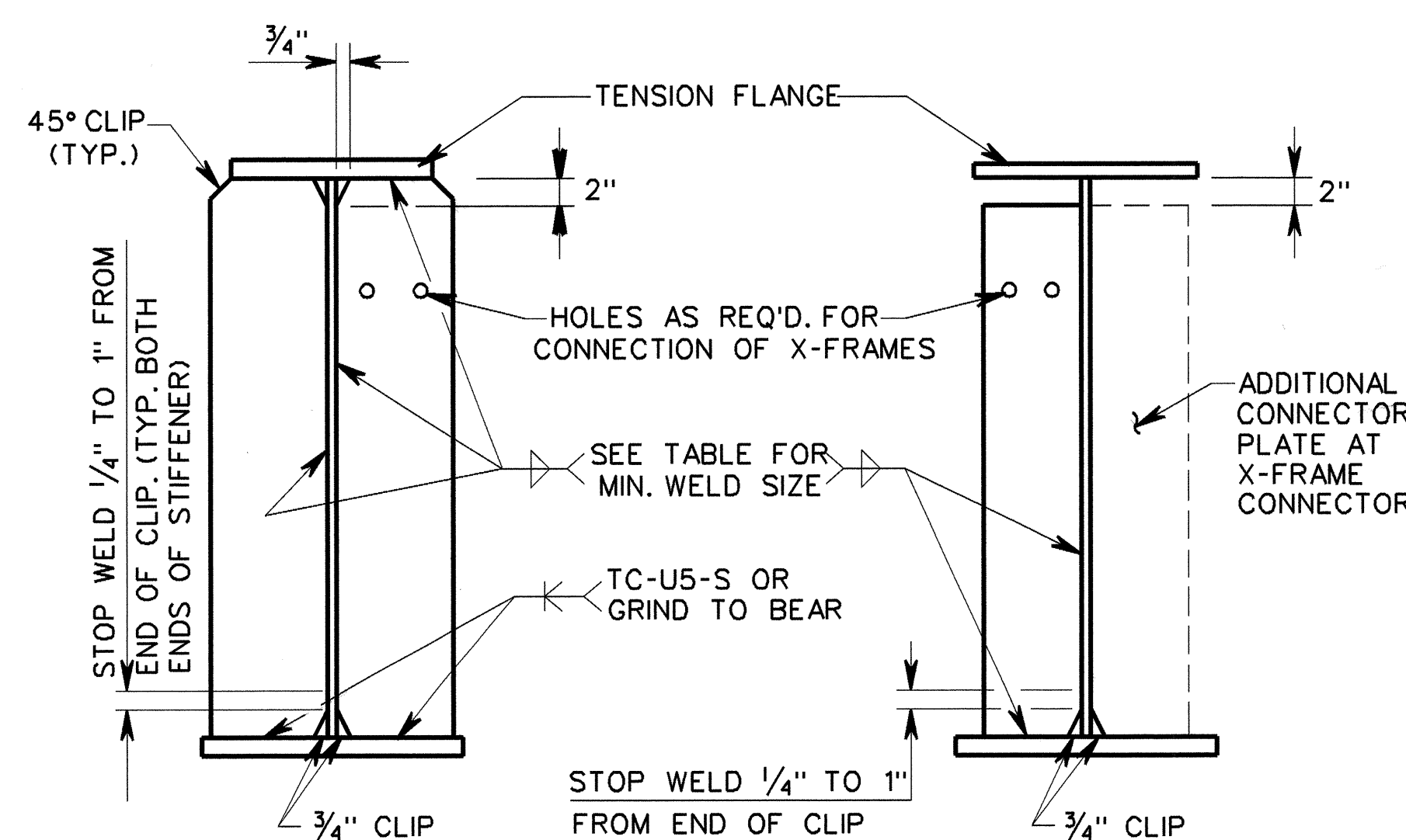
DEAD LOAD DEFLECTION DIAGRAM



TYPICAL TOP FLANGE SPLICE PLATE
(F.S. #1 TO #6)
SCALE: 1" = 1'-0"

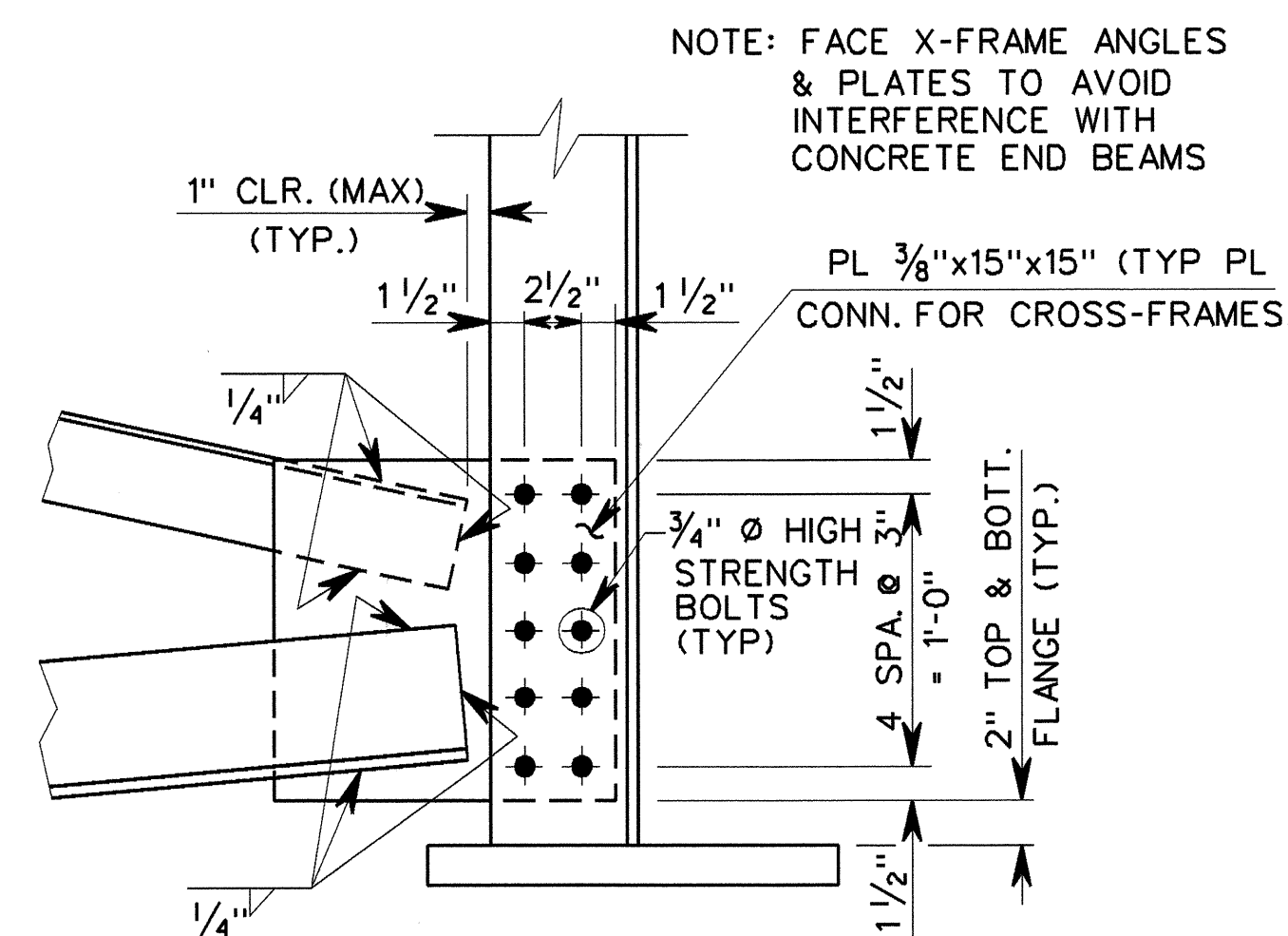


TYPICAL BOTTOM FLANGE SPLICE PLATE
(F.S. #1 TO #6)
SCALE: 1" = 1'-0"

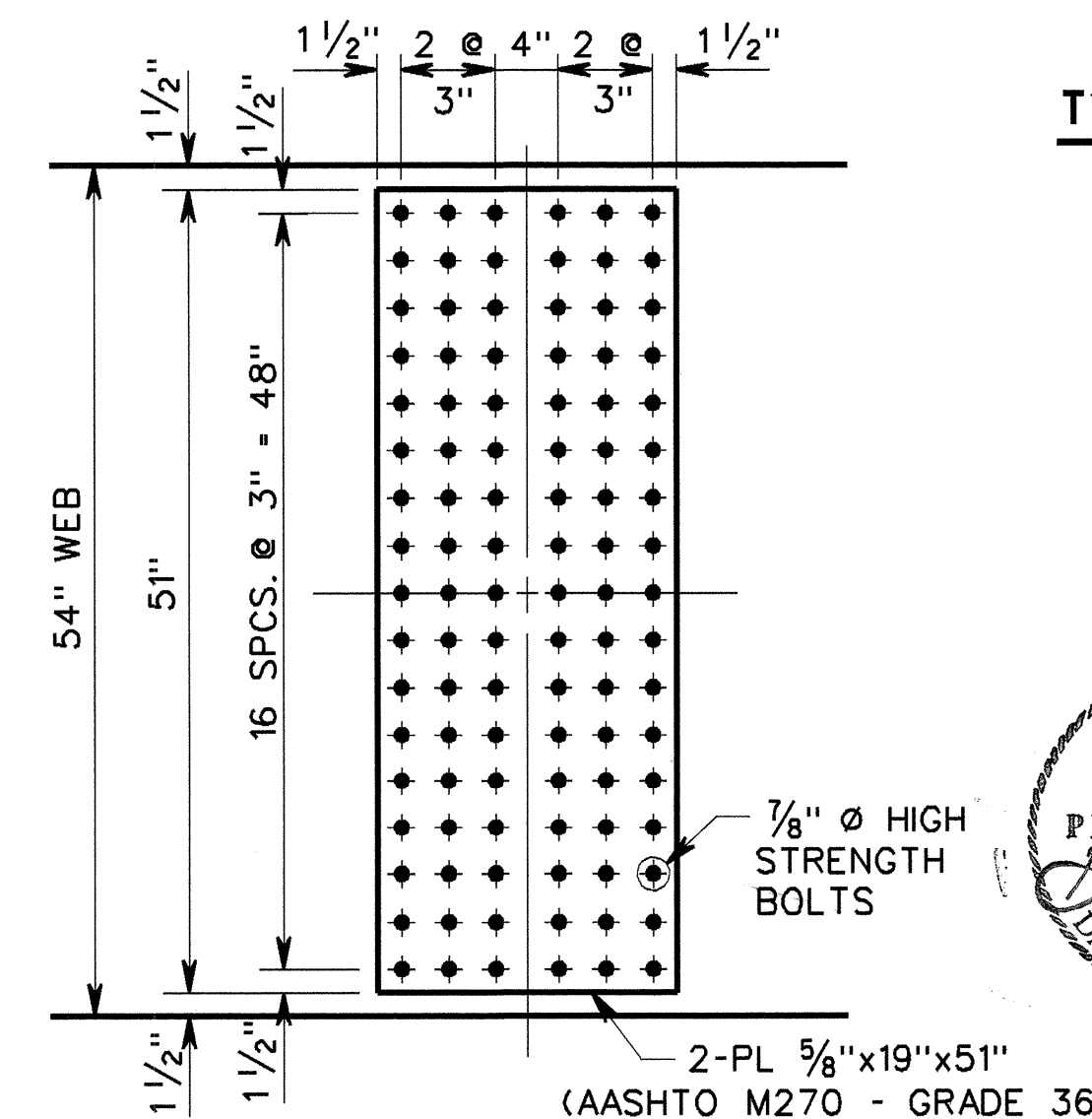


BEARING STIFFENERS

INTERMEDIATE STIFFENERS
AND CONNECTION PLATES

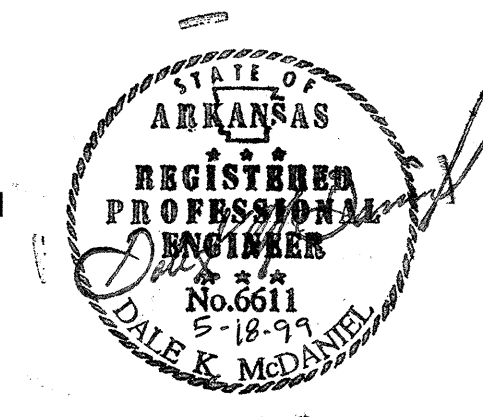


CONNECTION DETAIL (TYP)
SCALE: 1 1/2" = 1'-0"



WEB SPLICE (TYP)
SCALE: 1" = 1'-0"

CTA/PB CRAFTON TULL & ASSOCIATES, INC.
PARSONS BRINCKERHOFF
A JOINT VENTURE ROGERS ARKANSAS



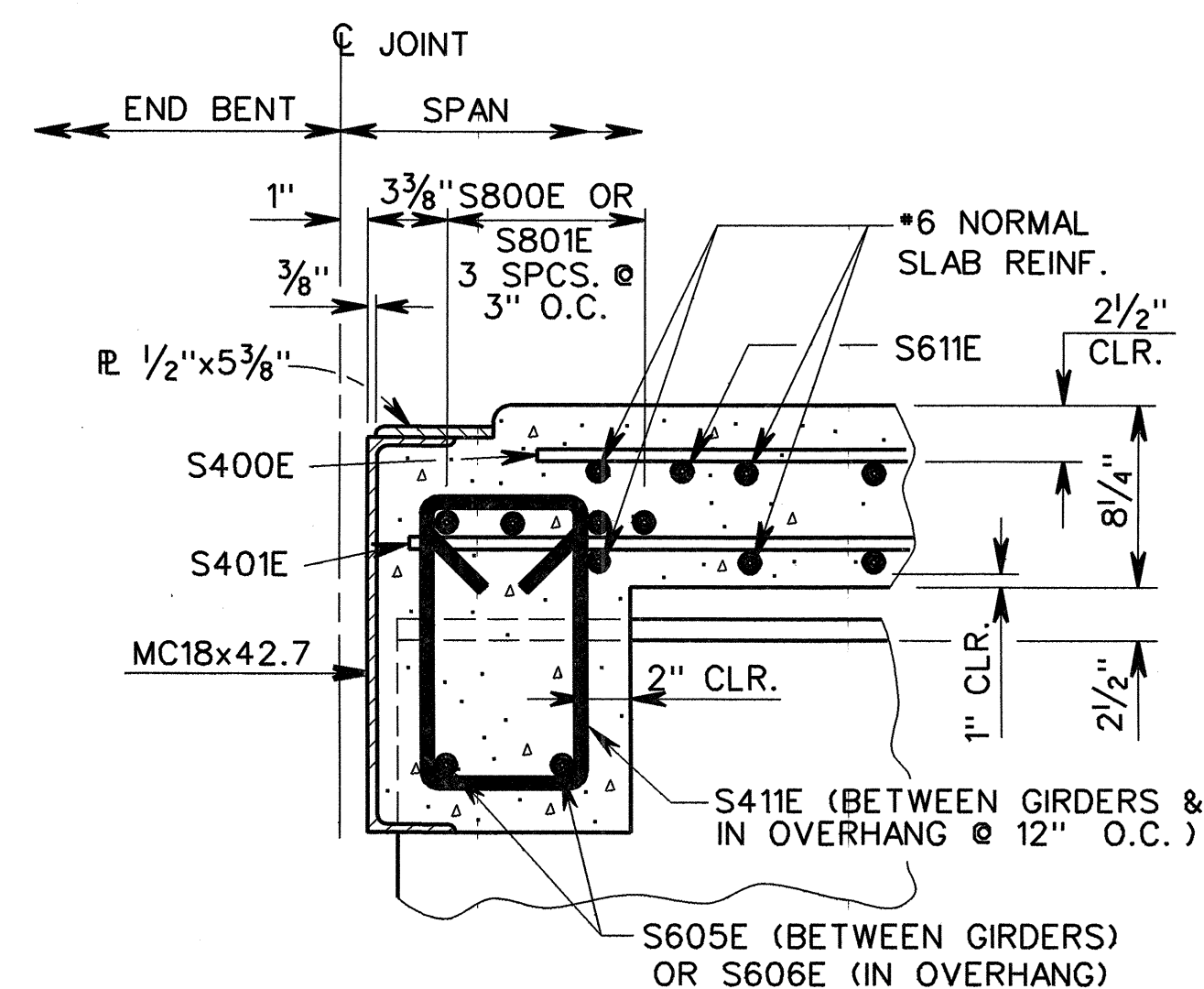
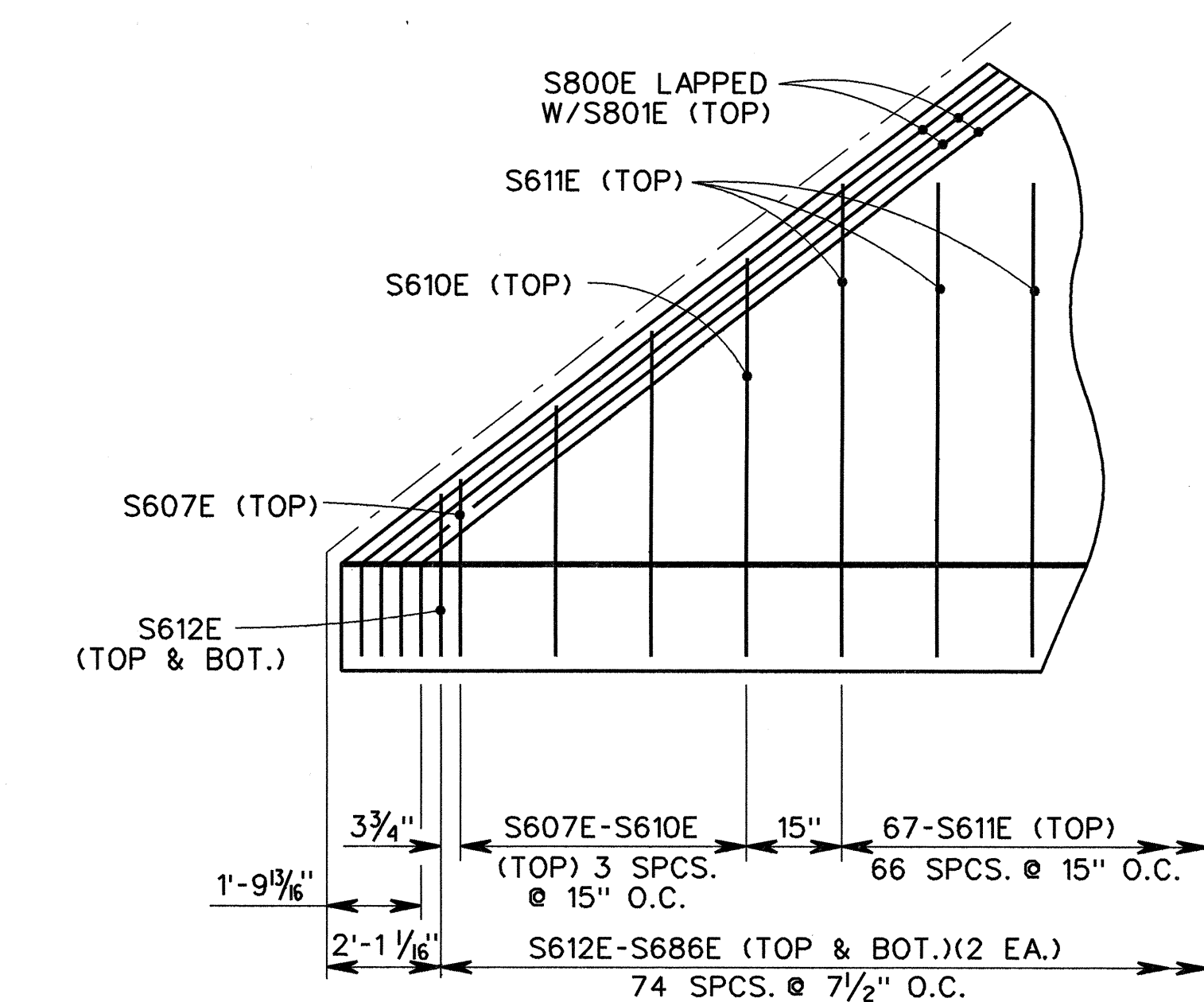
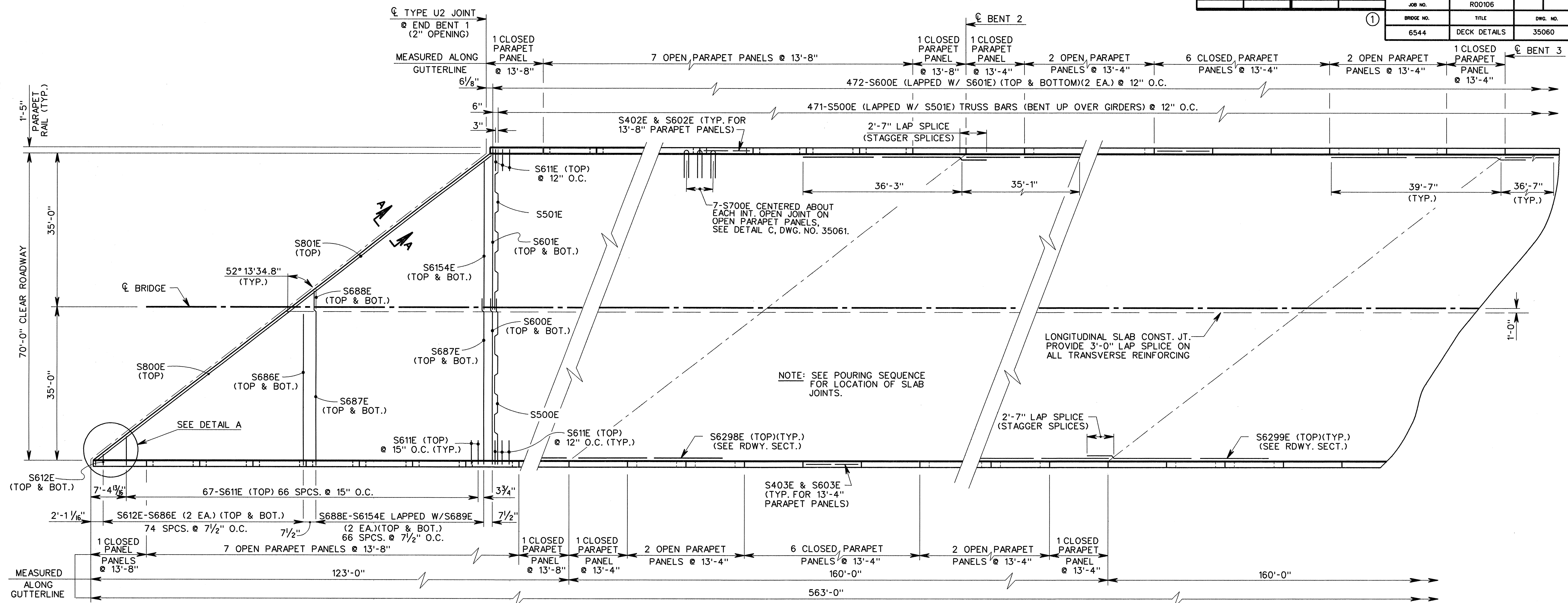
SHEET 2 OF 2
UNIT 1-4 CONT. R GIRDER DETAILS
HWY. 67/412 INTERCHANGE
U.S. HWY. 412 GRADE SEPARATION
LAWRENCE COUNTY
ROUTE 67 SEC. 16
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

DRAWN BY: C.G.H. DATE: NOV. '95
CHECKED BY: D.K.M. DATE: OCT. '96
DESIGNED BY: A.B. DATE: JUN. '95
SCALE: AS SHOWN

REVISD DEFLECTIONS AT SPAN POINT 1.1 THRU 1.9 05-18-99 D.K.M.
REVISD DEFLECTIONS AT SPAN POINT 1.1 THRU 1.9 04-12-99 D.K.M.

BRIDGE NO. 6544 DRAWING NO. 35059

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK		61	300
				JOB NO.	R00106			
				BRIDGE NO.	TITLE		DWG. NO.	
				6544	DECK DETAILS		35060	



NOTES:

- FOR ROADWAY SECTION AND ADDITIONAL DETAILS, SEE DWG. NO. 35062.
- FOR GENERAL NOTES, SEE DWG. NO. 35019.
- FOR PARAPET RAIL REINFORCING & DETAILS, SEE DWG. NO. 35112.
- FOR TYPE U2 JOINT DETAILS, SEE DWG. NO. 35110.
- THE ROADWAY SURFACE AND FRONT FACE AND TOP OF PARAPET RAILING TO BE TREATED WITH CLASS 1 PROTECTIVE SURFACE TREATMENT.
- AT THE CONTRACTORS OPTION, FOUR STRAIGHT #5 BARS MAY BE SUBSTITUTED IN LIEU OF BARS S500E & S501E. BARS SHALL BE EPOXY COATED. PAYMENT WILL BE BASED ON THE WEIGHT OF BARS S500E & S501E.

CTA/PB CRAFTON TULL & ASSOCIATES, INC.
PARSONS BRINCKERHOFF

A JOINT VENTURE ROGERS ARKANSAS



SHEET 1 OF 3

UNIT 1-4 DECK DETAILS
HWY. 67/412 INTERCHANGE
U.S. HWY. 412 GRADE SEPARATION
LAWRENCE COUNTY
ROUTE 67 SEC. 16

ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

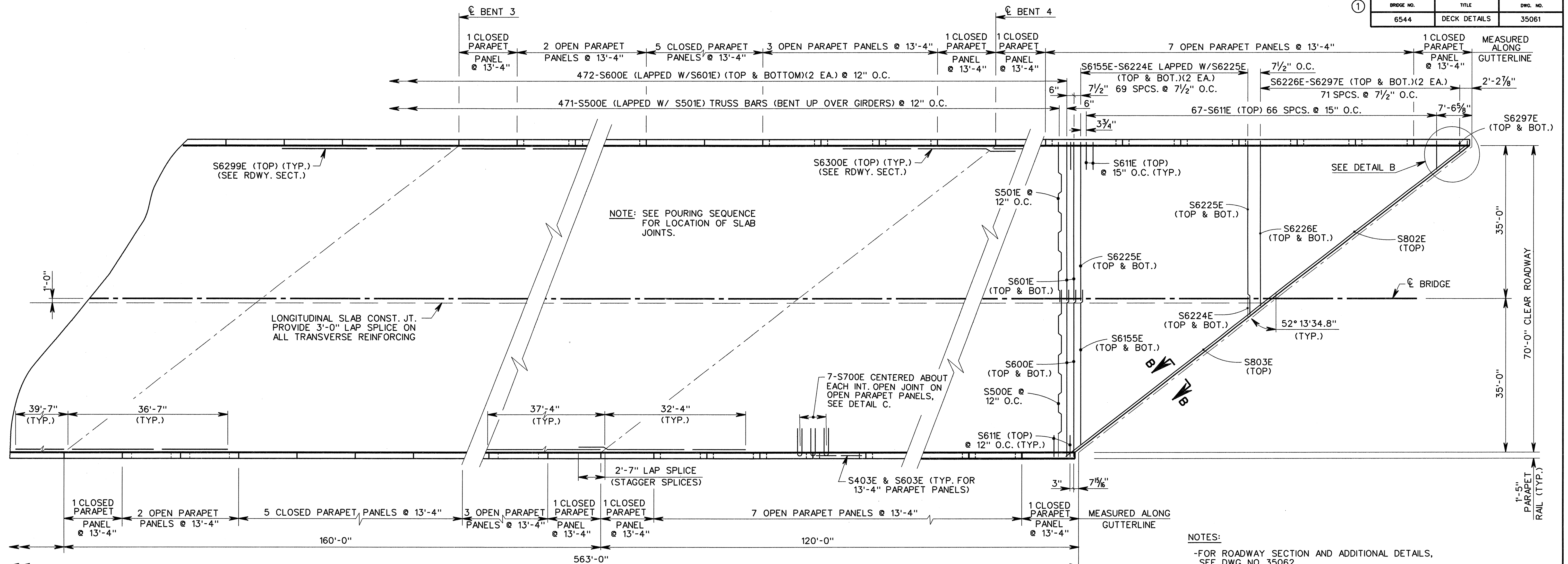
DRAWN BY: C.G.H. DATE: NOV. '95
CHECKED BY: D.K.M. DATE: OCT. '96
DESIGNED BY: D.K.M. DATE: JUN. '95

SCALE: AS SHOWN

BRIDGE NO. 6544

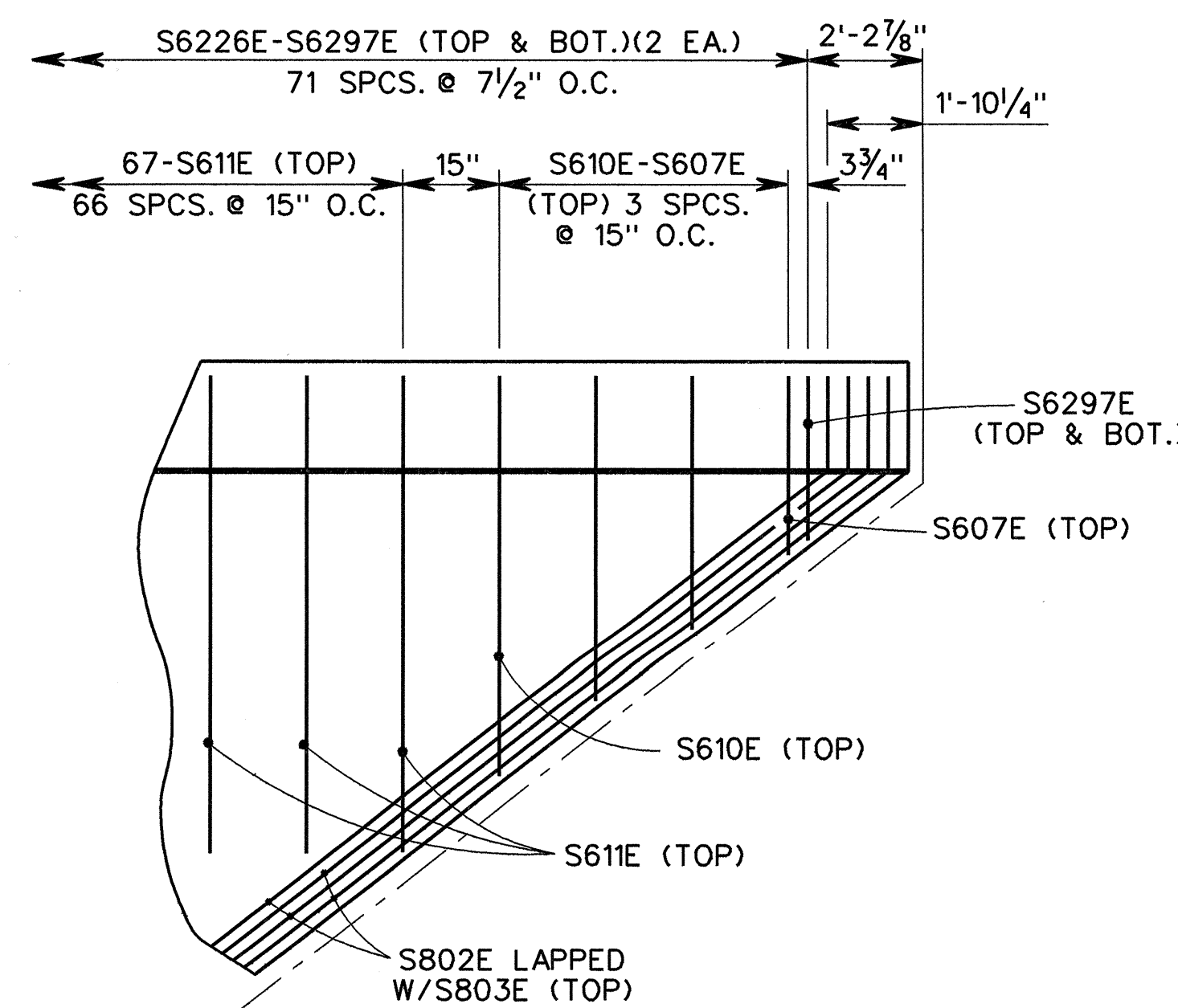
DRAWING NO. 35060

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK		62	306
				JOB NO.	R00106			
				BRIDGE NO.	TITLE			DWG. NO.
				6544	DECK DETAILS			35061

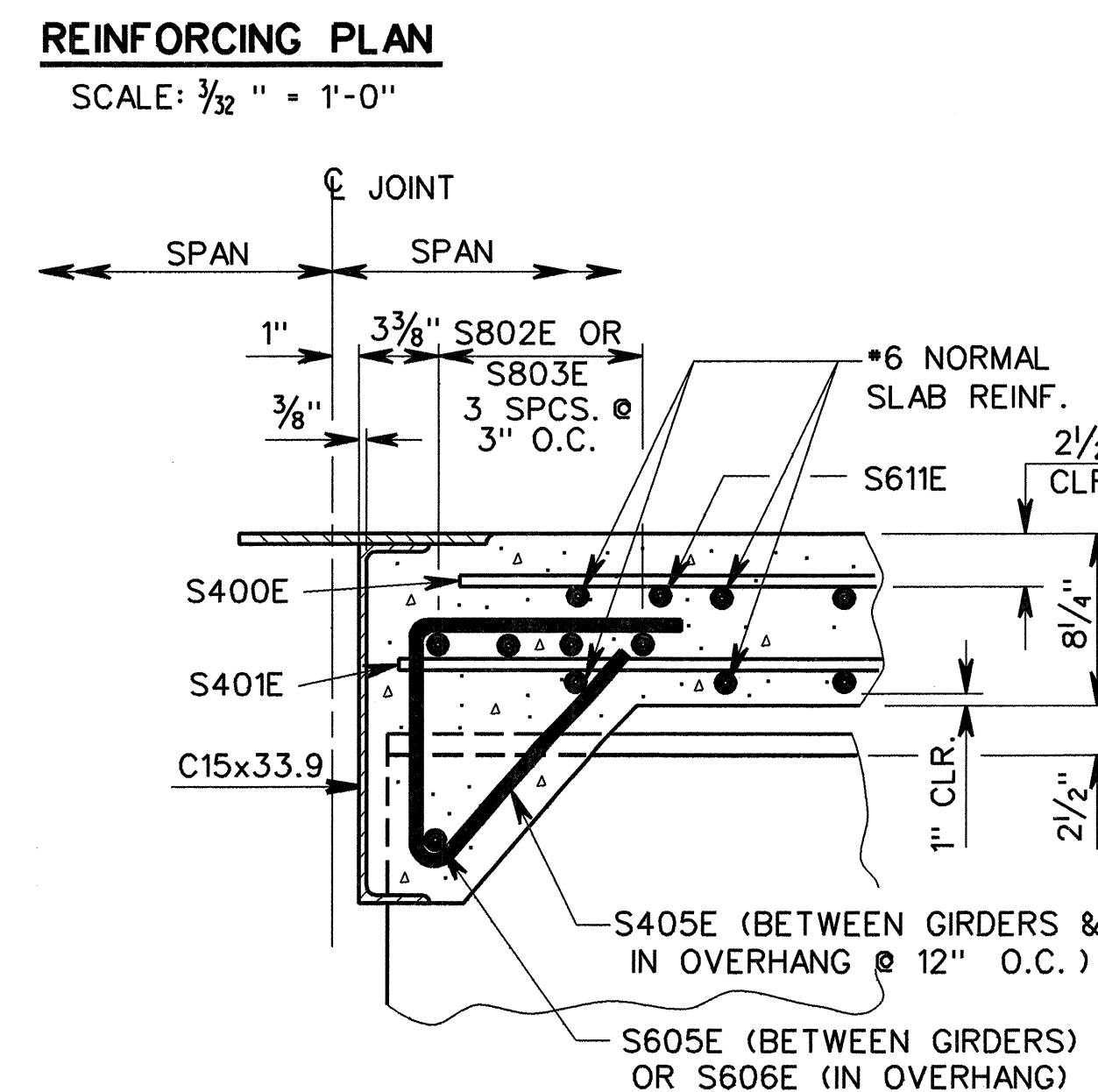


NOTES:

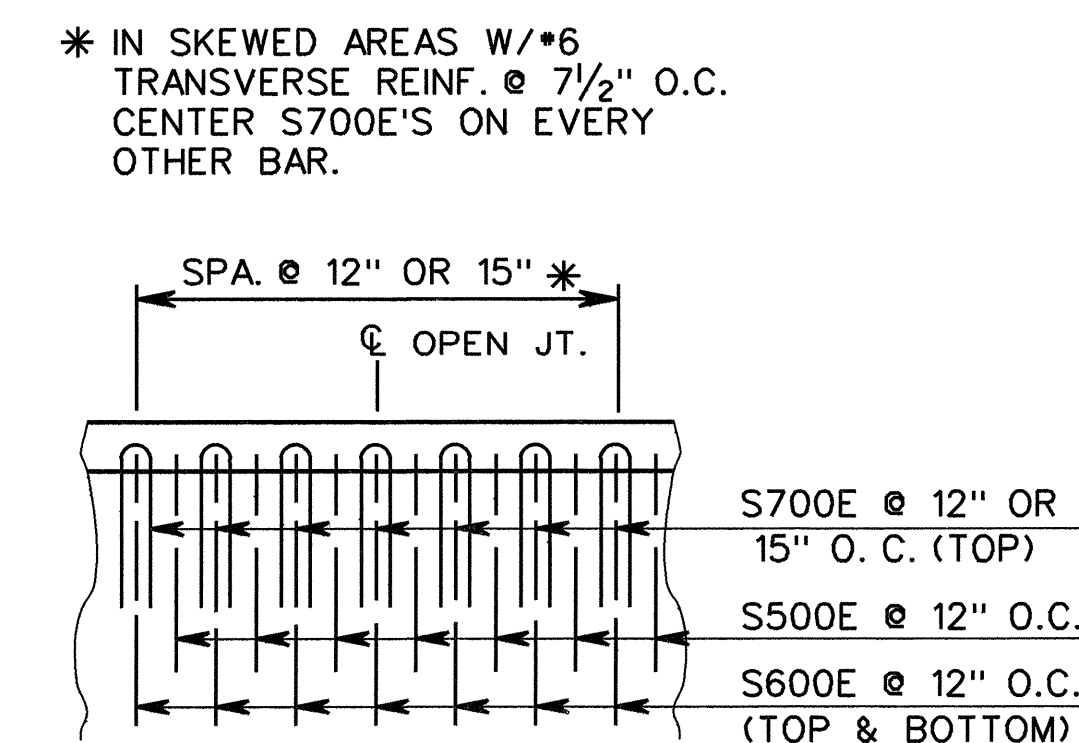
- FOR ROADWAY SECTION AND ADDITIONAL DETAILS, SEE DWG. NO. 35062.
- FOR GENERAL NOTES, SEE DWG. NO. 35019.
- FOR PARAPET RAIL REINFORCING & DETAILS, SEE DWG. NO. 35112.
- FOR TYPE U2 JOINT DETAILS, SEE DWG. NO. 35110.
- THE ROADWAY SURFACE AND FRONT FACE AND TOP OF PARAPET RAILING TO BE TREATED WITH CLASS 1 PROTECTIVE SURFACE TREATMENT.
- AT THE CONTRACTORS OPTION, FOUR STRAIGHT #5 BARS MAY BE SUBSTITUTED IN LIEU OF BARS S500E & S501E. BARS SHALL BE EPOXY COATED. PAYMENT WILL BE BASED ON THE WEIGHT OF BARS S500E & S501E.



DETAIL B
SCALE: 1/2" = 1'-0"



SECTION B-B
SCALE: 1 1/2" = 1'-0"



DETAIL C
N.T.S.

CTA / P B			CRAFTON TULL & ASSOCIATES, INC.		
			PARSONS BRINCKERHOFF		
A JOINT VENTURE			ROGERS	ARKANSAS	

SHEET 2 OF 3

UNIT 1-4 DECK DETAILS
HWY. 67/412 INTERCHANGE
U.S. HWY. 412 GRADE SEPARATION
LAWRENCE COUNTY
ROUTE 67 SEC. 16

ARKANSAS STATE HIGHWAY COMMISSION

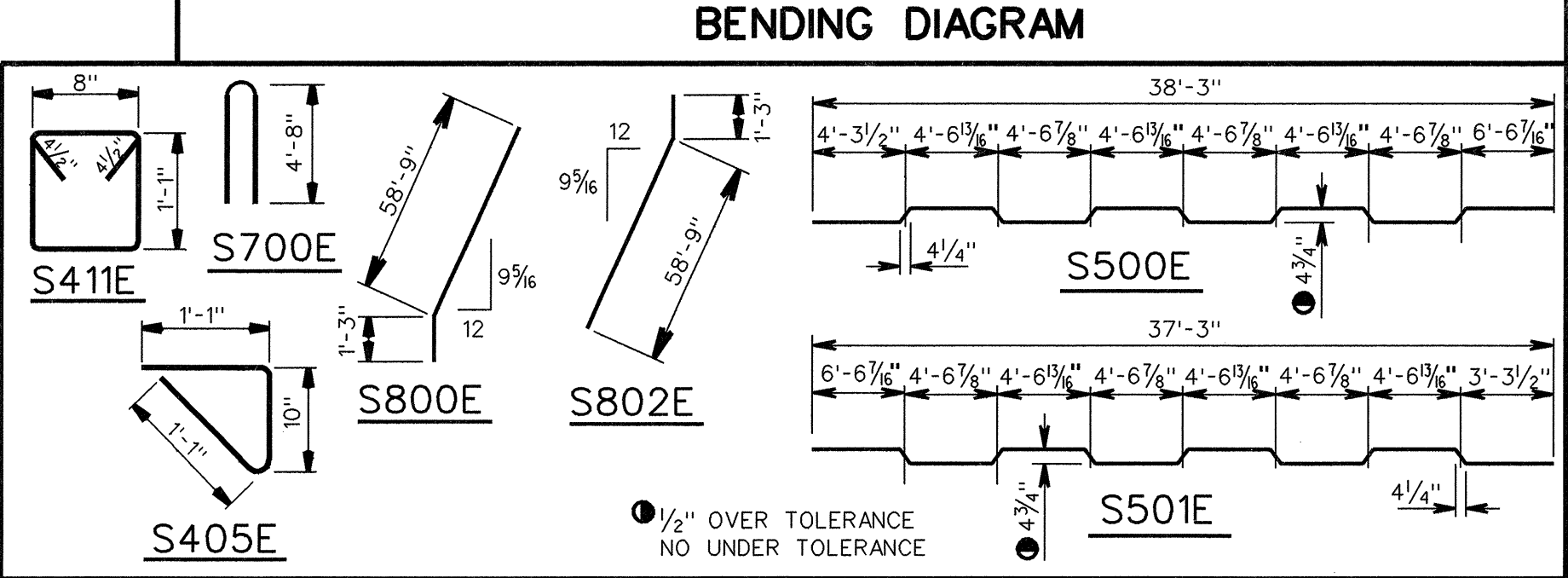
DRAWN BY: C.G.H. DATE: NOV. '95
CHECKED BY: D.K.M. DATE: OCT. '96 SCALE: AS SHOWN
DESIGNED BY: D.K.M. DATE: JUN. '95

BRIDGE NO. 6544 DRAWING NO. 35061

\WALNUT\WALDR84B

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AD PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK		63	306
				JOB NO.	R00106			
				BRIDGE NO.	TITLE		DWG. NO.	
				6544	DECK DETAILS		35062	

REINFORCING SCHEDULE - UNIT 1-4							
MARK	NO.	LENGTH	PIN DIA.	MARK	NO.	LENGTH	PIN DIA.
S800E	4	60'-0"	6"	S6226E	2 EA.	VARIES 36'-11" TO 2'-6"	STR.
S801E	4	59'-10"	STR.	S6297E	224	38'-3"	STR.
S802E	4	60'-0"	6"	S6298E	224	40'-8"	STR.
S803E	4	59'-10"	STR.	S6299E	224	37'-5"	STR.
				S6300E	471	39'-2"	3"
S700E	238	9'-8"	6 1/2"	S500E	471	38'-2"	3"
				S501E	885	39'-0"	STR.
S600E	946	38'-2"	STR.	S400E	1905	39'-0"	STR.
S601E	946	37'-2"	STR.	S401E	92	2'-11"	3"
S605E	21	14'-1"	STR.	S411E	92	3'-9"	2"
S606E	6	4'-3"	STR.	S602E	70	13'-4"	STR.
S607E	2 EA.	VARIES 2'-8" TO 5'-7"	STR.	S603E	160	13'-0"	STR.
S610E	2 EA.	VARIES 2'-5" TO 38'-3"	STR.	S402E	56	13'-4"	STR.
S611E	1078	5'-9"	STR.	S403E	128	13'-0"	STR.
S612E	2 EA.	VARIES 3'-6" TO 38'-3"	STR.	S406E	368	5'-6"	2"
S686E	2 EA.	VARIES 3'-6" TO 35'-6"	STR.	S408E	368	6'-6"	2"
S687E	65	38'-3"	STR.	S409E	368	3'-2"	2"
S688E	2 EA.	VARIES 36'-6" TO 3'-2"	STR.	S410E	368	6'-0"	2"
S6154E	2 EA.	VARIES 36'-6" TO 3'-2"	STR.	S402E	32	13'-4"	STR.
S6155E	2 EA.	VARIES 36'-6" TO 3'-2"	STR.	S403E	272	13'-0"	STR.
S6224E	70	37'-3"	STR.	S406E	532	5'-6"	2"
S6225E	70	37'-3"	STR.	S407E	532	5'-8"	2"



BENDING NOTES:

- DIMENSIONS OF BARS ARE OUT TO OUT.
- FOR PARAPET RAIL BAR BENDING DIAGRAMS, SEE DWG. NO. 35112.
- BARS TO BE EPOXY COATED, SEE SECTION 804 OF THE STANDARD SPECIFICATIONS FOR EPOXY COATED REINFORCING STEEL (GR. 60)
- NOTES:
- FOR GENERAL NOTES, SEE DWG. NO. 35019.
- FOR PARAPET RAIL REINFORCING & DETAILS, SEE DWG. NO. 35112.
- FOR TYPE U2 JOINT DETAILS, SEE DWG. NO. 35110.
- THE ROADWAY SURFACE AND FRONT FACE AND TOP OF PARAPET RAILING TO BE TREATED WITH CLASS 1 PROTECTIVE SURFACE TREATMENT.
- AT THE CONTRACTORS OPTION, FOUR STRAIGHT #5 BARS MAY BE SUBSTITUTED IN LIEU OF BARS S500E & S501E. BARS SHALL BE EPOXY COATED. PAYMENT WILL BE BASED ON THE WEIGHT OF BARS S500E & S501E.

CTA/PB CRAFTON TULL & ASSOCIATES, INC. PARSONS BRINCKERHOFF

A JOINT VENTURE ROGERS ARKANSAS

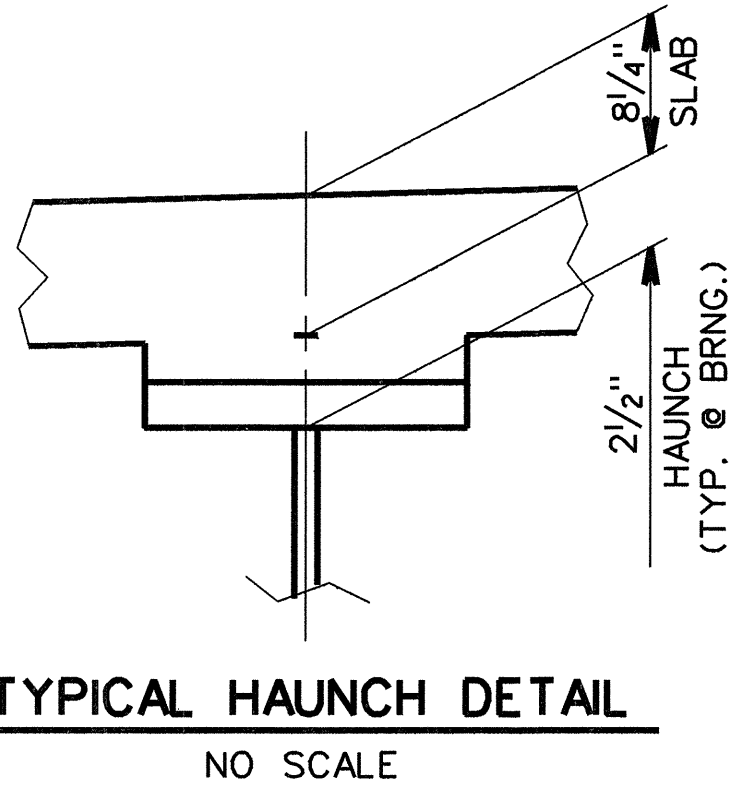
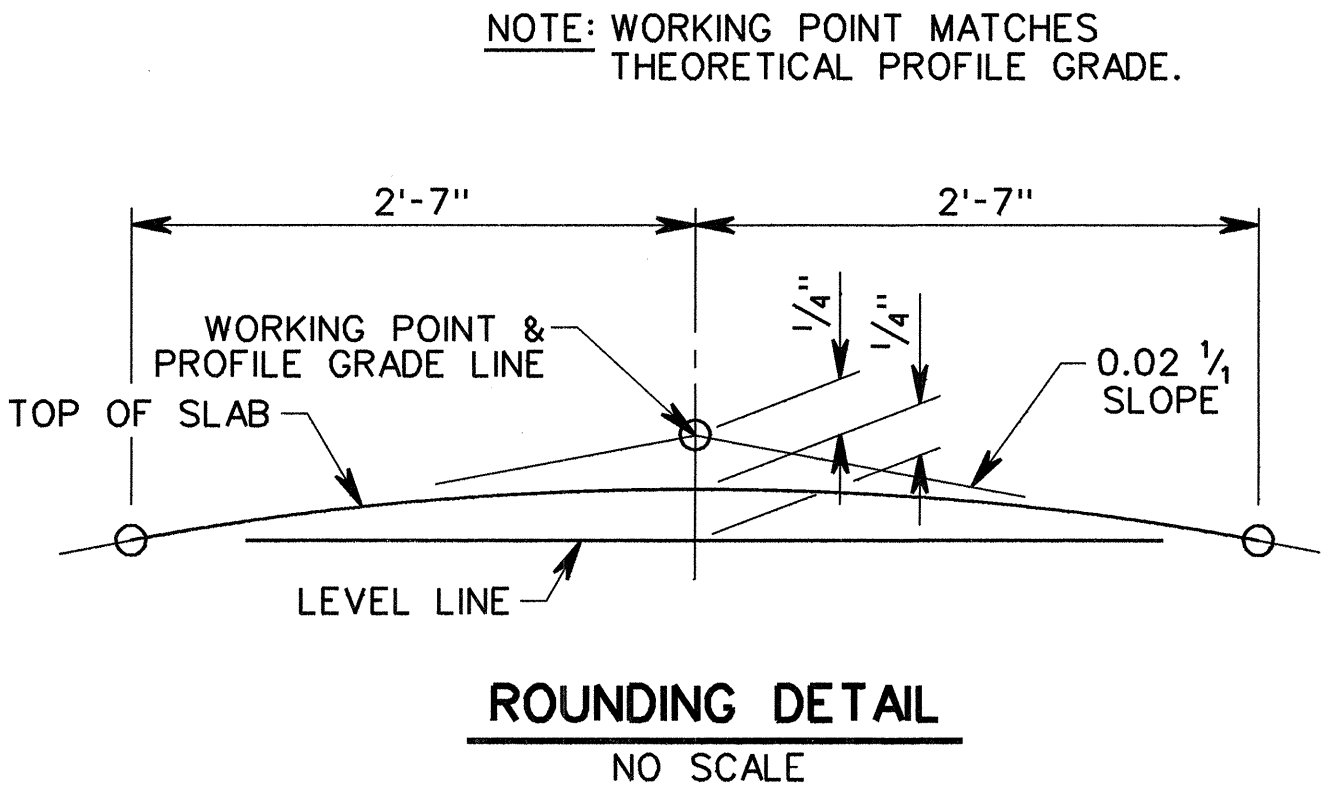
SHEET 3 OF 3

UNIT 1-4 DECK DETAILS
HWY. 67/412 INTERCHANGE
U.S. HWY. 412 GRADE SEPARATION
LAWRENCE COUNTY
ROUTE 67 SEC. 16

ARKANSAS STATE HIGHWAY COMMISSION

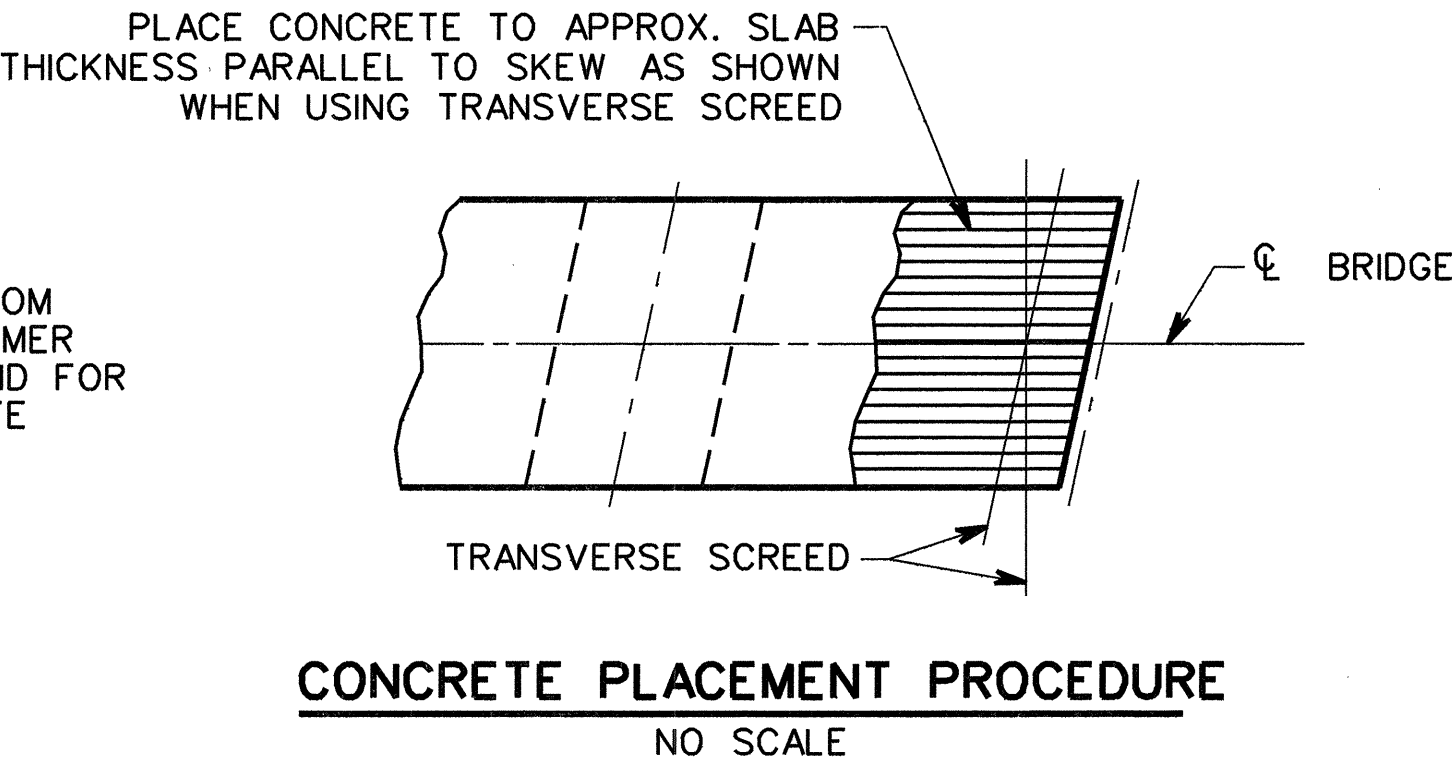
LITTLE ROCK, ARK.
DRAWN BY: C.G.H. DATE: NOV. '92
CHECKED BY: D.K.M. DATE: OCT. '96
DESIGNED BY: D.K.M. DATE: JUN. '99

BRIDGE NO. 6544 DRAWING NO. 35062

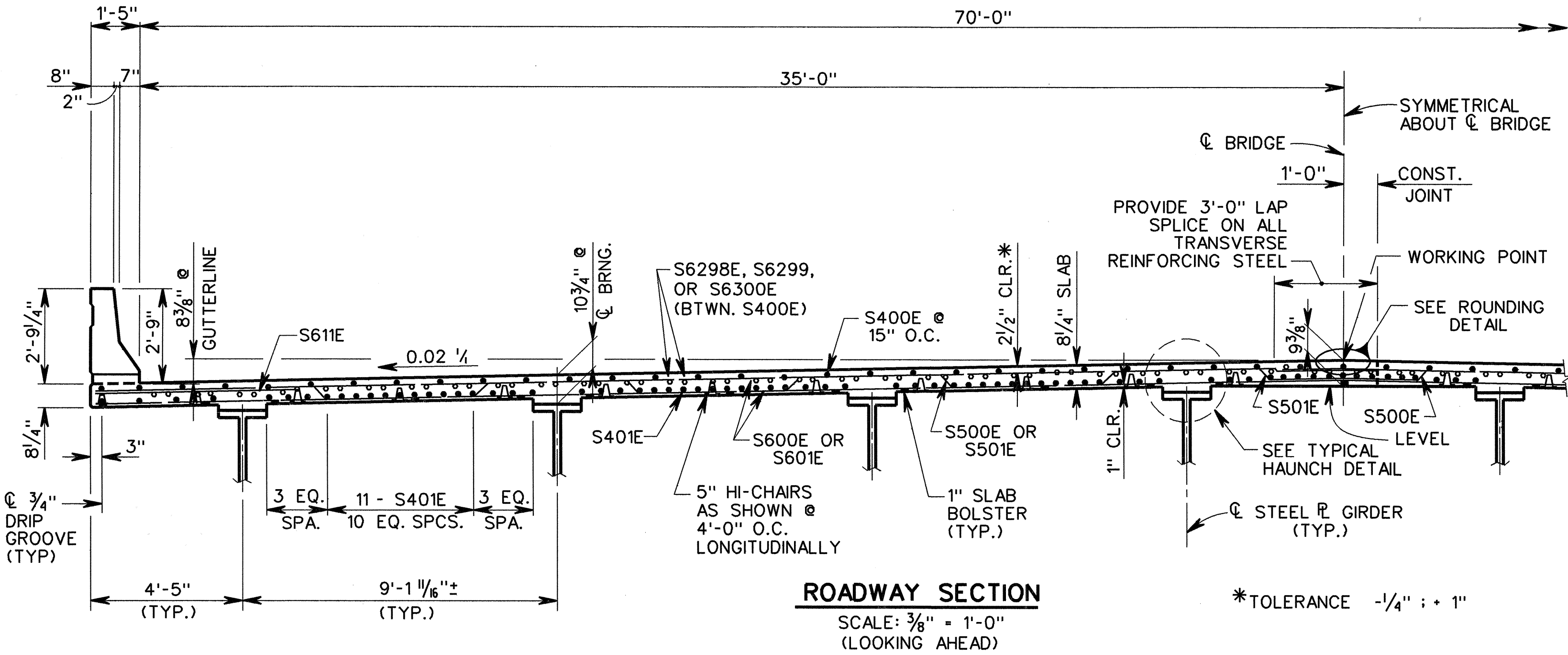


NOTE: TOLERANCE FOR SLAB THICKNESS IS MINUS 1/4" AND PLUS 1". PROVIDE HAUNCH AS REQ'D TO MEET SLAB TOLERANCES. NO ADJUSTMENT FOR INCREASE IN CONCRETE AND STRUCTURAL STEEL QUANTITIES WILL BE MADE FOR THICKENING SLAB OR DEEPENING HAUNCH TO MEET TOLERANCES.

THE SUPERSTRUCTURE DETAILS SHOWN ARE FOR USE WHEN REMOVABLE DECK FORMING IS USED AND ARE THE BASIS FOR MEASUREMENT OF CLASS S(AE) CONCRETE. SEE STANDARD DRAWING 14991 FOR ALLOWABLE MODIFICATIONS AND FOR TOLERANCES WHEN PERMANENT STEEL BRIDGE DECK FORMS ARE USED.

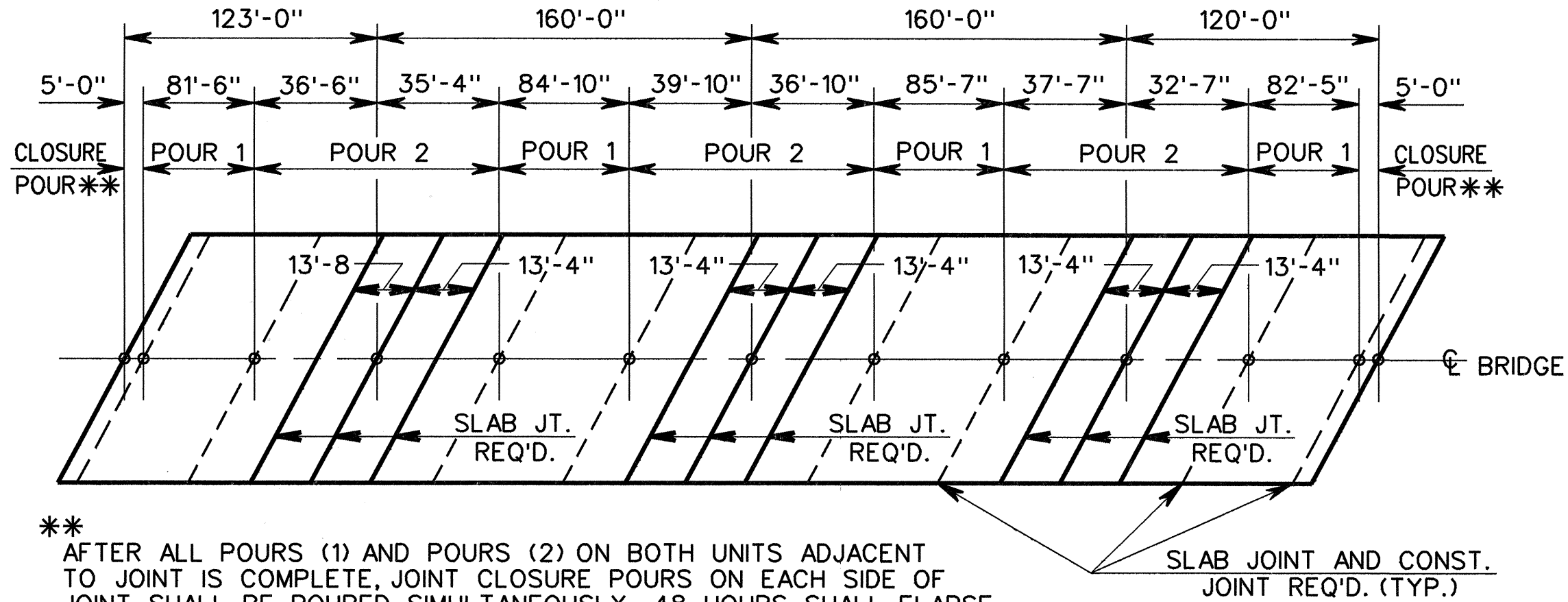
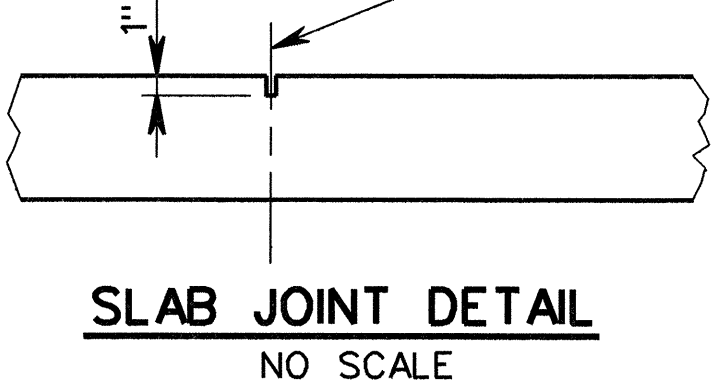


NOTE: AT THE CONTRACTOR'S OPTION, THE TRANSVERSE SCREED MAY BE PLACED ON A RADIAL LINE OR PARALLEL TO THE SKEW. SCREED RAIL SUPPORT DETAIL SHOWN ON DWG. 35076 SHALL APPLY TO THIS BRIDGE



LONGITUDINAL SLAB REINFORCING:
S400E(TOP) & S401E (BOTTOM) SPACED AS SHOWN. S6298E, S6299E & S6300E (TOP) OVER INT. BENTS AS SHOWN HERE AND ON REINFORCING PLAN.

1/2" x 1" TYPE 6 JOINT SEALER. SEE SECTIONS 501.02(h) AND 501.05(j) OF THE STANDARD SPECIFICATIONS. JOINT SEALER SHALL BE MEASURED AND PAID FOR AS CLASS S(AE) CONCRETE-BRIDGE. SLAB JOINTS SHALL EXTEND TO THE OUTSIDE EDGE OF THE DECK SLAB. SLAB JOINTS SHALL BE INSTALLED BEFORE THE PARAPET RAILING IS POURED. IF SLAB JOINTS ARE TO BE SAWED, THEY SHALL BE SAWED BEFORE ANY VEHICULAR TRAFFIC IS ALLOWED ON THE UNIT.



** AFTER ALL POURS (1) AND POURS (2) ON BOTH UNITS ADJACENT TO JOINT IS COMPLETE, JOINT CLOSURE POURS ON EACH SIDE OF JOINT SHALL BE POURED SIMULTANEOUSLY. 48 HOURS SHALL ELAPSE BETWEEN END OF LAST POUR (2) AND START OF JOINT CLOSURE POUR.

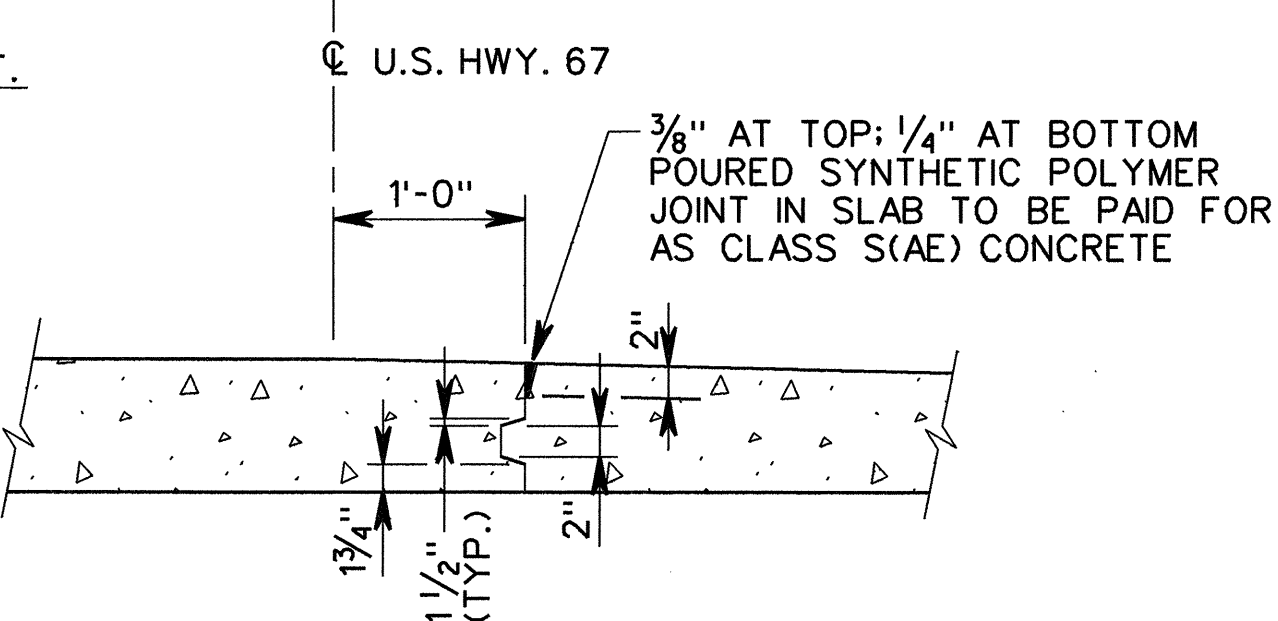
NOTES:

POURS WITH THE SAME NUMBER MAY BE PLACED SIMULTANEOUSLY OR SEPARATELY. ALL POURS (1) MUST BE PLACED BEFORE POURS (2) CAN BE PLACED. 48 HOURS SHALL ELAPSE BETWEEN POURS AND 72 HOURS SHALL ELAPSE BETWEEN ADJACENT POURS. ANY RAILING POURS MADE BEFORE ENTIRE SLAB UNIT HAS BEEN PLACED MUST BE APPROVED BY THE BRIDGE ENGINEER.

ALL CONCRETE IN BRIDGE SUPERSTRUCTURE SHALL BE PLACED, CONSOLIDATED AND SCREEDED OFF FOR THE ENTIRE POUR BEFORE ANY CONCRETE HAS TAKEN ITS INITIAL SET. THIS MAY REQUIRE THE USE OF A RETARDING AGENT. THE CONCRETE DECK SHALL BE FINISHED IN ACCORDANCE WITH SECTION 802.19 OF THE STANDARD SPECIFICATIONS. MOVEMENT OF THE FINISHING MACHINE ACROSS NEW CONCRETE SHALL BE ON PLANKS PLACED ON THE SURFACE AND SHALL BE PROHIBITED FOR 72 HOURS AFTER FINISHING THE POUR.

THE CONTRACTOR MUST OBTAIN APPROVAL FROM THE BRIDGE ENGINEER FOR ANY DEVIATIONS FROM THE POURING SEQUENCE SHOWN.

LONGITUDINAL CONST. JT. DETAIL

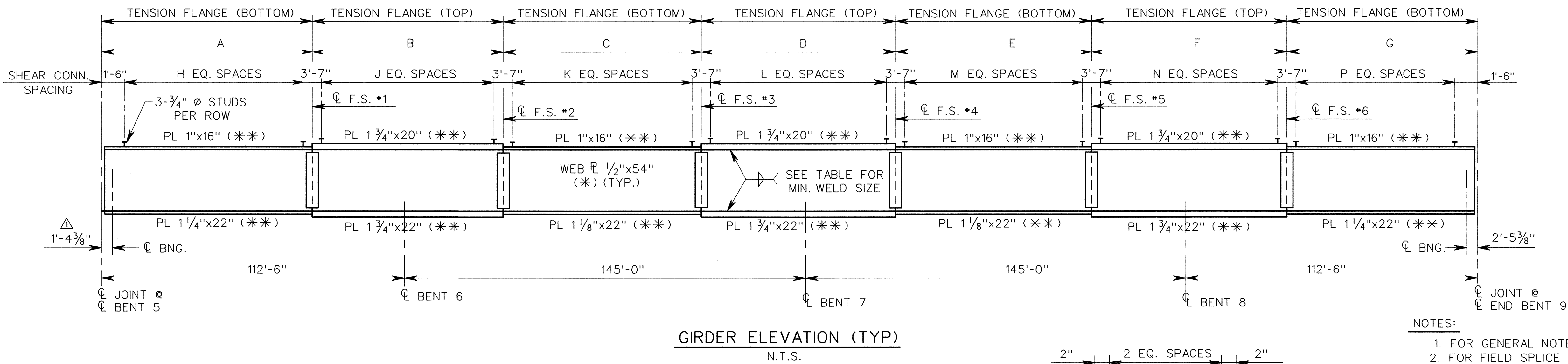
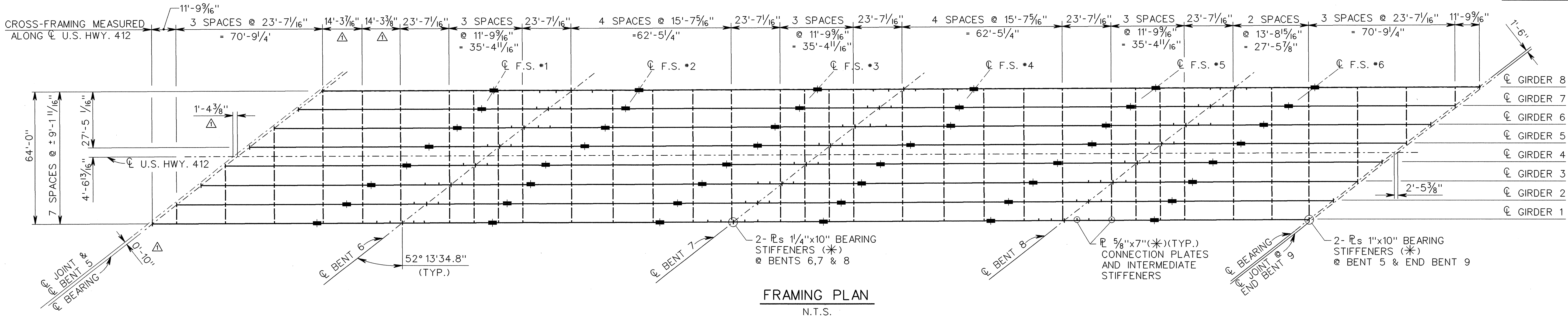


WALNUT WALDR84C

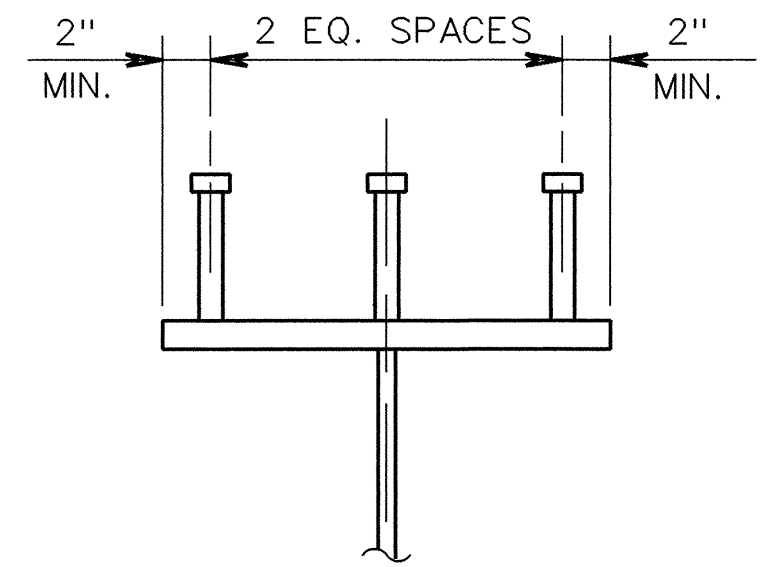
MICROFILMED
JAN 27 1999

10-08-28-99

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
9-14-99	6-27-00			6	ARK		64	306
				JOB NO.	R00106			
				BRIDGE NO.	TITLE		DWG. NO.	
				6544	GIRDER DETAILS		35063	



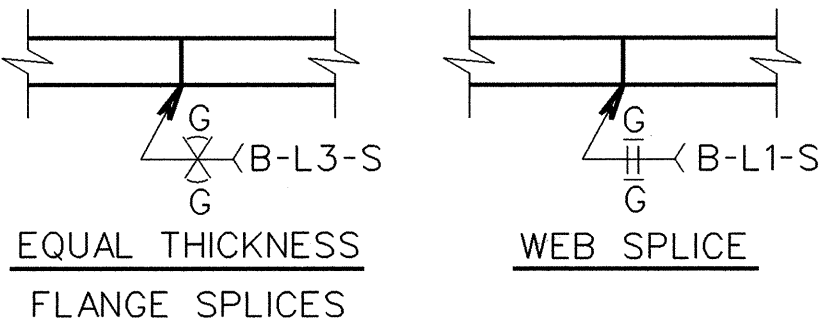
	A	B	C	D	E	F	G	H	J	K	L	M	N	P
GIRDER #1	80'-4 $\frac{1}{2}$ "	70'-1 $\frac{5}{8}$ "	78'-9 $\frac{1}{8}$ "	66'-7 $\frac{1}{4}$ "	78'-4 $\frac{7}{8}$ "	66'-7 $\frac{1}{4}$ "	74'-1 $\frac{3}{8}$ "	86	45	57	43	57	43	79
GIRDER #2	78'-3 $\frac{3}{8}$ "	66'-4 $\frac{1}{8}$ "	77'-0 $\frac{7}{8}$ "	68'-2 $\frac{3}{8}$ "	78'-4 $\frac{7}{8}$ "	63'-8 $\frac{1}{2}$ "	83'-0 $\frac{1}{8}$ "	84	42	56	44	57	41	89
GIRDER #3	77'-7 $\frac{1}{2}$ "	65'-0 $\frac{1}{8}$ "	78'-8 $\frac{7}{8}$ "	66'-10"	79'-5 $\frac{1}{2}$ "	64'-1 $\frac{1}{2}$ "	83'-2 $\frac{1}{2}$ "	83	41	57	43	57	41	89
GIRDER #4	80'-1 $\frac{1}{4}$ "	63'-6 $\frac{3}{4}$ "	75'-5 $\frac{1}{4}$ "	70'-7 $\frac{1}{8}$ "	74'-5"	69'-7 $\frac{7}{8}$ "	81'-2 $\frac{3}{4}$ "	86	40	54	45	54	45	87
GIRDER #5	80'-1 $\frac{1}{2}$ "	70'-9"	74'-5"	70'-7"	75'-7"	63'-2 $\frac{1}{8}$ "	80'-4 $\frac{3}{8}$ "	86	45	54	45	55	40	86
GIRDER #6	82'-0 $\frac{1}{8}$ "	65'-5 $\frac{7}{8}$ "	79'-2"	67'-0 $\frac{1}{2}$ "	78'-9"	64'-2"	78'-4 $\frac{1}{2}$ "	88	42	57	43	57	41	84
GIRDER #7	81'-8 $\frac{1}{4}$ "	65'-2 $\frac{1}{8}$ "	78'-3"	67'-10 $\frac{1}{8}$ "	77'-5"	65'-5 $\frac{5}{8}$ "	79'-1 $\frac{1}{8}$ "	88	42	57	43	56	42	85
GIRDER #8	74'-1 $\frac{3}{8}$ "	66'-7 $\frac{1}{4}$ "	78'-4 $\frac{7}{8}$ "	72'-7 $\frac{1}{4}$ "	72'-4 $\frac{7}{8}$ "	68'-10 $\frac{1}{2}$ "	81'-11 $\frac{1}{8}$ "	79	43	57	47	52	44	88



- NOTES:
1. FOR GENERAL NOTES, SEE DWG. NO. 35019.
 2. FOR FIELD SPLICE DETAILS, SEE DWG. NO. 35064.
 3. FOR CROSS-FRAME DETAILS, SEE DWG. NO. 35064.
 4. FOR BEARING PAD DETAILS, SEE DWG. NOS. 35107 & 35108.
 5. FOR JOINT DETAILS, SEE DWG. NO. 35110.
- * (AASHTO M270 - GRADE 36)
** (AASHTO M270 - GRADE 50)

CTA/PB CRAFTON TULL & ASSOCIATES, INC.
PARSONS BRINCKERHOFF

A JOINT VENTURE	ROGERS	ARKANSAS
-----------------	--------	----------



DETAILS OF WELDED SHOP SPLICES

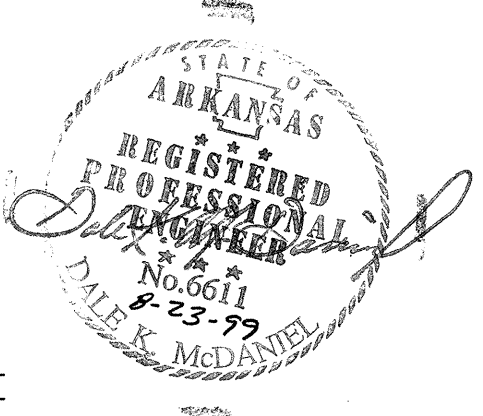
LOAD DISTRIBUTION TO:	INTERIOR GIRDER	EXTERIOR GIRDER
DEAD LOAD (NON-COMPOSITE)	1.292 k_{FT}	1.276 k_{FT}
DEAD LOAD (COMPOSITE)	0.276 k_{FT}	0.244 k_{FT}
LIVE LOAD TO COMPOSITE BEAM	1.662 WHEELS + IMPACT	1.686 WHEELS + IMPACT

NON-COMPOSITE DEAD LOAD INCLUDES APPROXIMATELY 0.321 k_{FT} STEEL WEIGHT.

TABLE FOR WELD		
MATERIAL THICKNESS OF THICKER PART JOINED (INCHES)	MINIMUM SIZE OF FILLET WELD (INCHES)	SINGLE PASS WELD MUST BE USED
INCLUSIVE TO $\frac{3}{4}$ "	$\frac{1}{4}$ "	
OVER $\frac{3}{4}$ "	$\frac{5}{16}$ "	

NOTE: WHEN A FILLET WELD SIZE, AS SHOWN ON THE PLANS, IS LARGER THAN THE MINIMUM, THE FIRST PASS SHALL BE THAT SPECIFIED FOR MINIMUM SIZE OF FILLET WELD.

STUD SHEAR CONNECTORS SHOWN SHALL BE $\frac{3}{4}$ " ϕ x 5" LONG GRANULAR FLUX FILLED, SOLID FLUXED OR EQUAL AND AUTOMATICALLY END WELDED TO GIRDER FLANGE IN ACCORDANCE WITH RECOMMENDATIONS OF THE MANUFACTURER. THE $\frac{3}{4}$ " ϕ STUDS ARE ESTIMATED AT 61.5 lb. per 100 AS THE BASIS OF PAYMENT. THE $\frac{1}{8}$ " ϕ STUDS MAY BE SUBSTITUTED FOR THE $\frac{3}{4}$ " ϕ STUDS SHOWN AT THE RATIO OF 0.73- $\frac{1}{8}$ " STUDS IN PLACE OF 1- $\frac{3}{4}$ " STUD.



SHEET 1 OF 2

UNIT 5-8 CONT. ϕ GIRDER DETAILS
HWY. 67/412 INTERCHANGE
U.S. HWY. 412 GRADE SEPARATION
LAWRENCE COUNTY
ROUTE 67 SEC. 16
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

DRAWN BY: C.G.H. DATE: NOV. '95
CHECKED BY: D.K.M. DATE: OCT. '96
DESIGNED BY: A.B. DATE: JUN. '95
SCALE: AS SHOWN

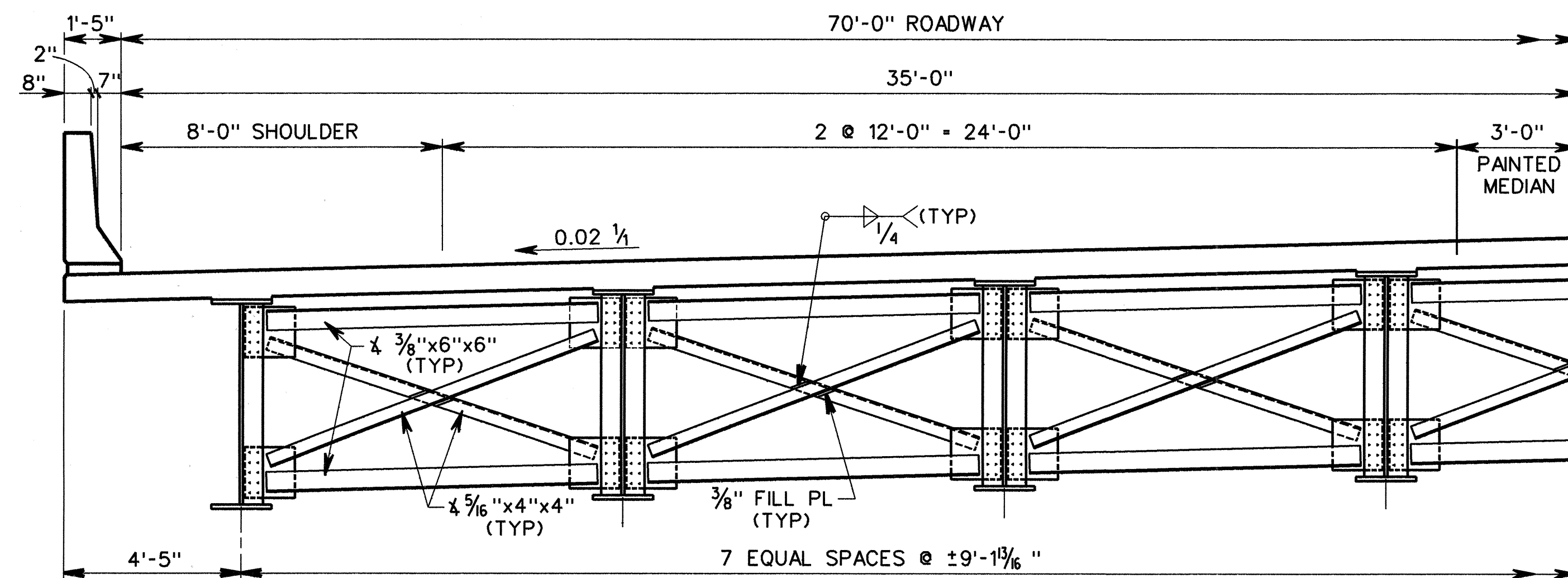
BRIDGE NO. 6544 DRAWING NO. 35063

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK		65	306
				JOB NO.	R00106			
				BRIDGE NO.	TITLE			DWG. NO.
				6544	GIRDER DETAILS			35064

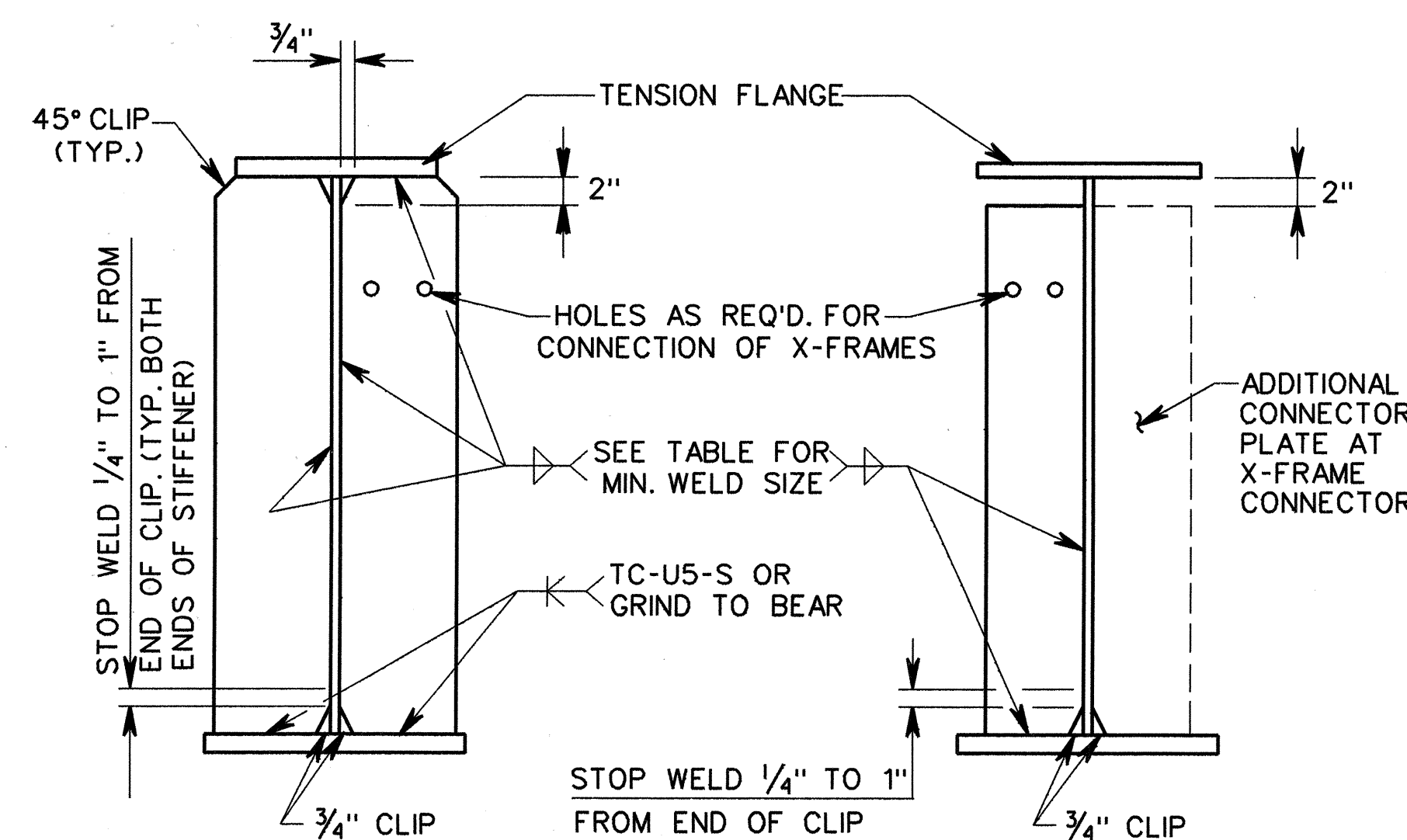
TABLE OF DEFLECTIONS (IN.)

SPAN POINT		1.0	1.1	1.2	1.3	1.4	1.5	1.6	1.7	1.8	1.9	2.0	2.1	2.2	2.3	2.4	2.5	2.6	2.7	2.8	2.9	3.0	3.1	3.2	3.3	3.4	3.5	3.6	3.7	3.8	3.9	4.0	4.1	4.2	4.3	4.4	4.5	4.6	4.7	4.8	4.9	5.0
INTERIOR GIRDERS	STRUCTURAL STEEL	0.000	-0.113	-0.206	-0.268	-0.291	-0.275	-0.225	-0.157	-0.084	-0.024	0.000	-0.054	-0.157	-0.269	-0.354	-0.380	-0.342	-0.253	-0.141	-0.043	0.000	-0.057	-0.165	-0.284	-0.377	-0.410	-0.377	-0.288	-0.171	-0.061	0.000	-0.021	-0.077	-0.147	-0.215	-0.265	-0.279	-0.259	-0.203	-0.112	0.000
	STRUCTURAL STEEL & SLAB	0.000	-0.536	-0.978	-1.266	-1.370	-1.288	-1.044	-0.726	-0.383	-0.110	0.000	-0.240	-0.707	-1.225	-1.630	-1.760	-1.598	-1.187	-0.669	-0.211	0.000	-0.237	-0.715	-1.252	-1.683	-1.842	-1.700	-1.300	-0.775	-0.279	0.000	-0.075	-0.307	-0.607	-0.906	-1.132	-1.208	-1.131	-0.894	-0.493	0.000
	STRUCTURAL STEEL SLAB & PARAPET	0.000	-0.594	-1.083	-1.400	-1.511	-1.418	-1.147	-0.795	-0.417	-0.117	0.000	-0.272	-0.793	-1.366	-1.810	-1.949	-1.762	-1.305	-0.732	-0.228	0.000	-0.269	-0.802	-1.396	-1.869	-2.038	-1.875	-1.430	-0.850	-0.304	0.000	-0.085	-0.342	-0.672	-0.999	-1.244	-1.324	-1.237	-0.976	-0.538	0.000
EXTERIOR GIRDERS	STRUCTURAL STEEL	0.000	-0.129	-0.237	-0.308	-0.335	-0.317	-0.262	-0.183	-0.102	-0.036	0.000	-0.051	-0.157	-0.279	-0.380	-0.420	-0.382	-0.288	-0.160	-0.047	0.000	-0.055	-0.166	-0.295	-0.403	-0.450	-0.418	-0.328	-0.197	-0.072	0.000	-0.016	-0.072	-0.147	-0.222	-0.281	-0.305	-0.286	-0.221	-0.123	0.000
	STRUCTURAL STEEL & SLAB	0.000	-0.626	-1.145	-1.489	-1.620	-1.536	-1.269	-0.890	-0.499	-0.179	0.000	-0.209	-0.683	-1.249	-1.730	-1.938	-1.787	-1.363	-0.769	-0.235	0.000	-0.225	-0.717	-1.305	-1.815	-2.053	-1.928	-1.522	-0.928	-0.348	0.000	-0.042	-0.263	-0.576	-0.905	-1.170	-1.292	-1.224	-0.953	-0.534	0.000
	STRUCTURAL STEEL SLAB & PARAPET	0.000	-0.743	-1.360	-1.769	-1.929	-1.835	-1.523	-1.076	-0.608	-0.222	0.000	-0.238	-0.791	-1.454	-2.022	-2.271	-2.103	-1.615	-0.922	-0.289	0.000	-0.257	-0.831	-1.518	-2.118	-2.401	-2.262	-1.794	-1.099	-0.415	0.000	-0.047	-0.307	-0.673	-1.058	-1.368	-1.510	-1.431	-1.115	-0.625	0.000

NOTE:
SIGN CONVENTIONS FOR DEFLECTIONS IS AS FOLLOWS:
(+) IS UPWARD
(-) IS DOWNWARD

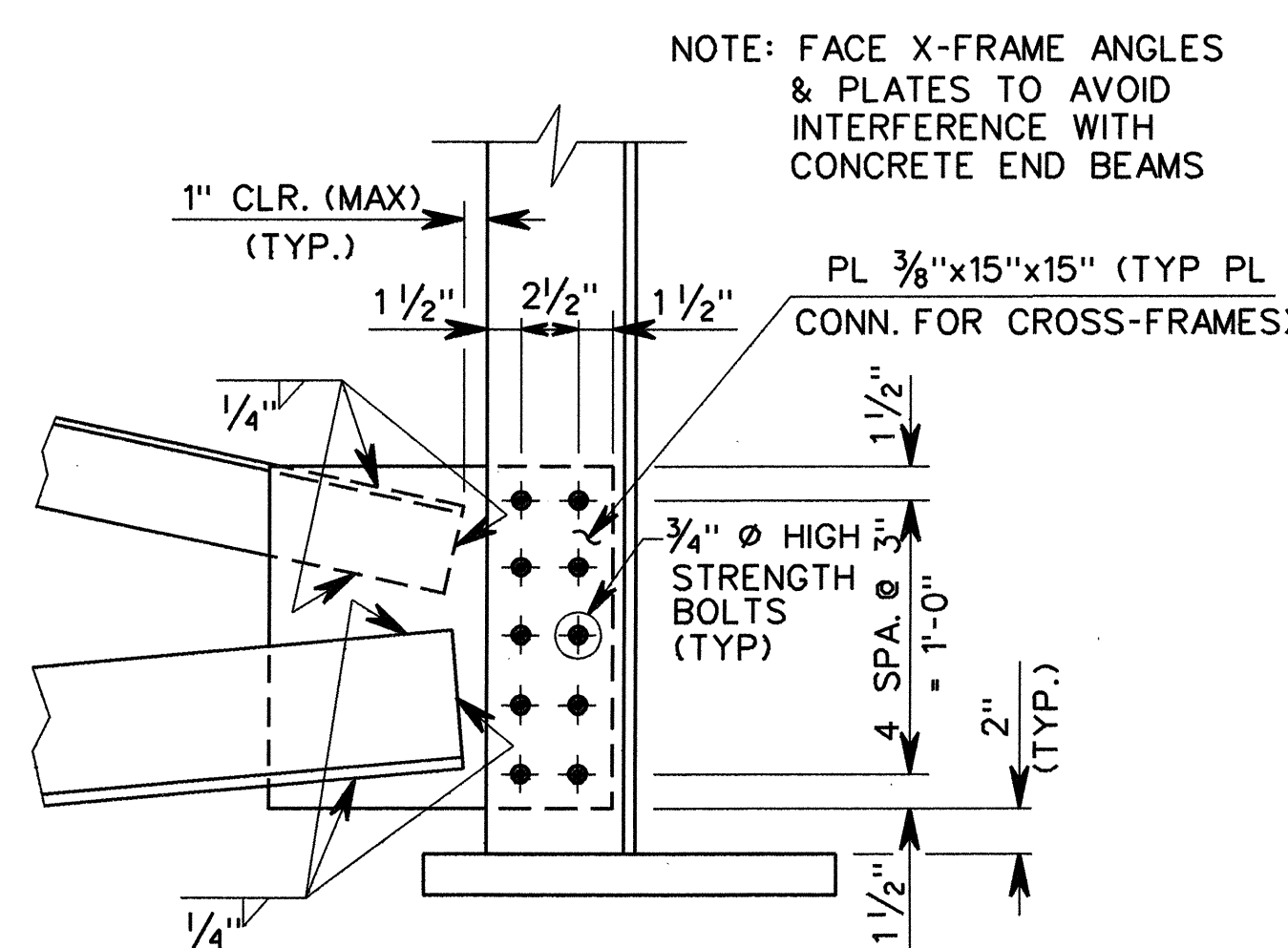


ROADWAY SECTION
SCALE: 3/8" = 1'-0"
(LOOKING UPSTATION)

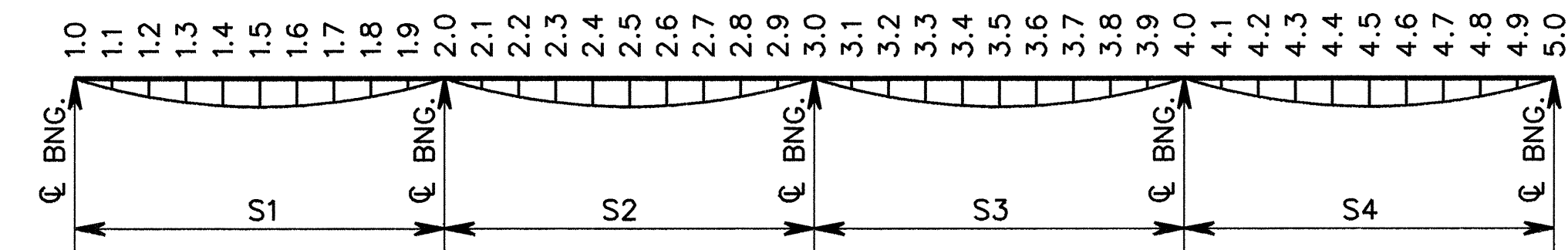


BEARING STIFFENERS

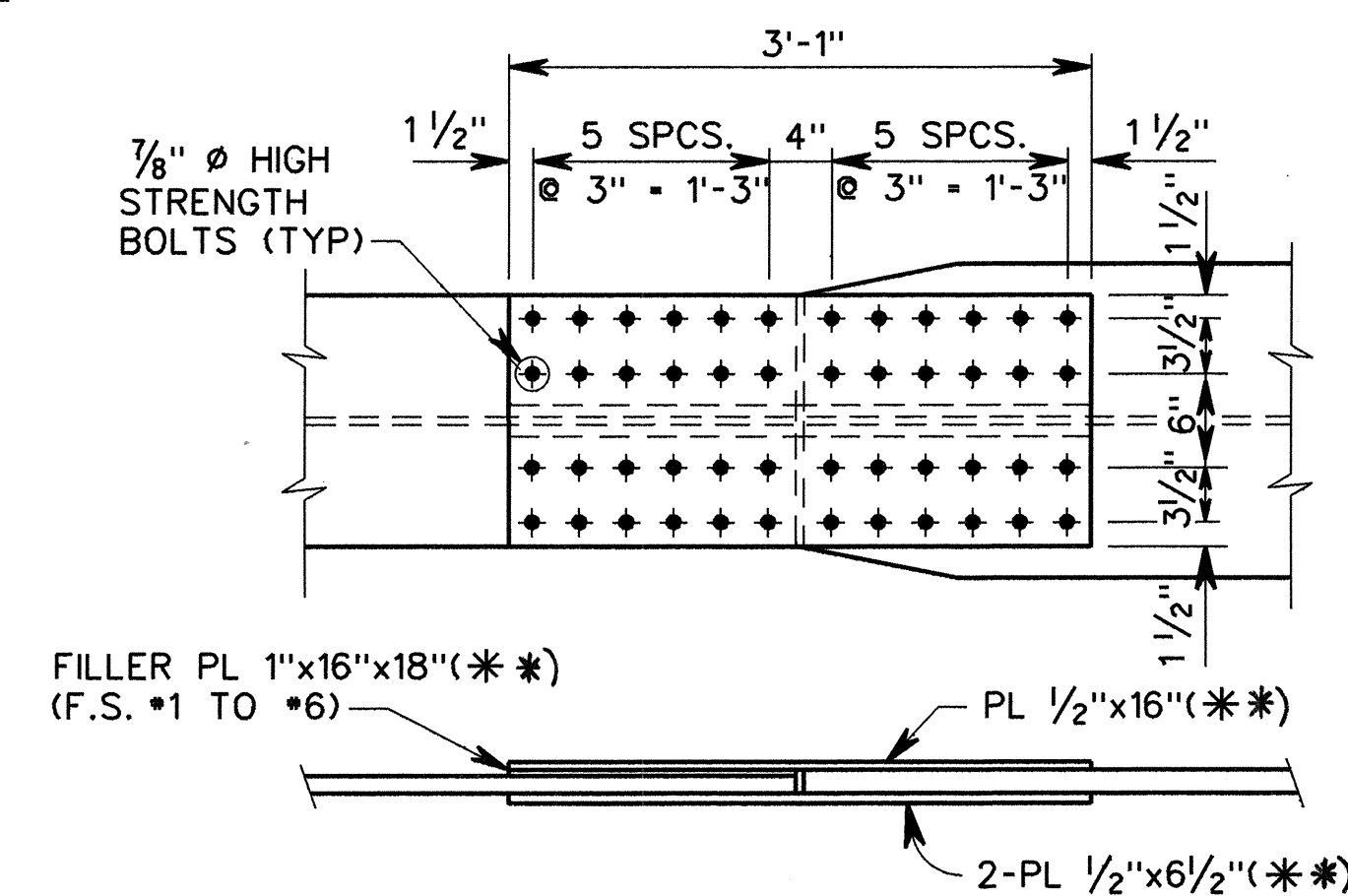
INTERMEDIATE STIFFENERS
AND CONNECTION PLATES



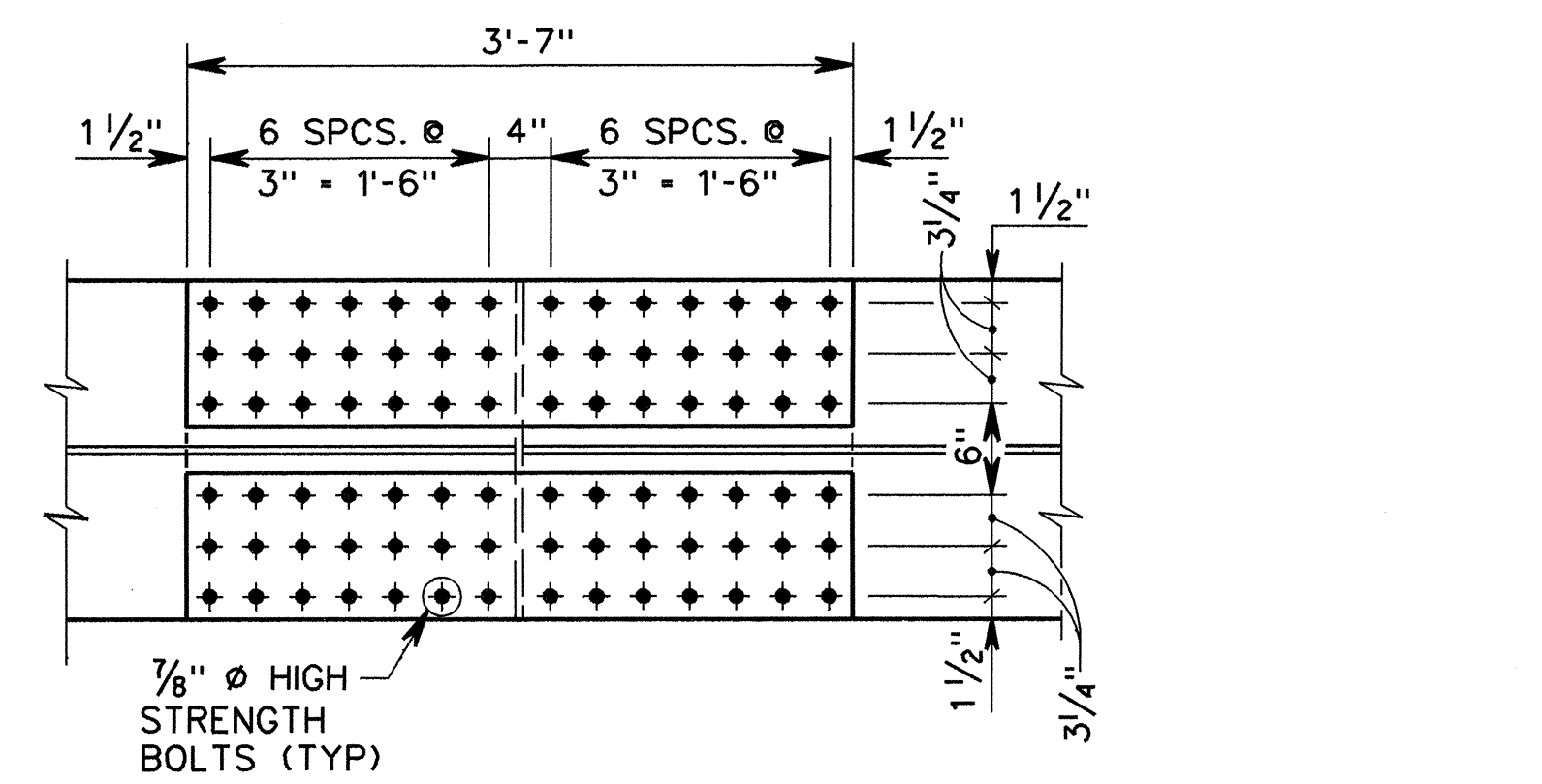
CONNECTION DETAIL (TYP)
SCALE: 1 1/2" = 1'-0"



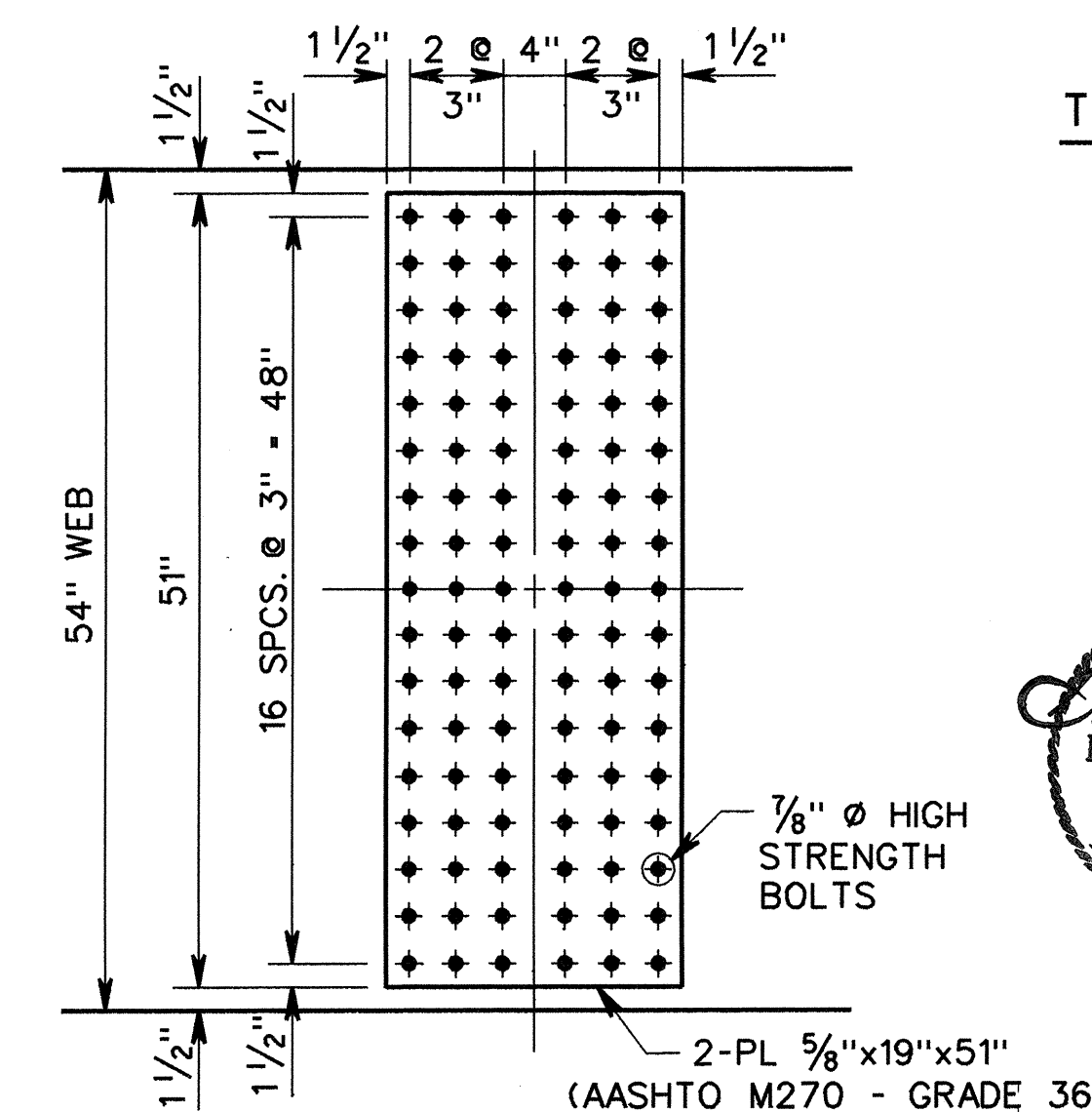
DEAD LOAD DEFLECTION DIAGRAM



TYPICAL TOP FLANGE SPLICE PLATE
(F.S. #1 TO #6)
SCALE: 1" = 1'-0"



TYPICAL BOTTOM FLANGE SPLICE PLATE
(F.S. #1 TO #6)
SCALE: 1" = 1'-0"



WEB SPLICE (TYP)
SCALE: 1" = 1'-0"

** (AASHTO M270 - GRADE 50)

CTA/PB CRAFTON TULL & ASSOCIATES, INC.
A JOINT VENTURE ROGERS ARKANSAS
PARSONS BRINCKERHOFF



SHEET 2 OF 2

UNIT 5-8 CONT. R GIRDER DETAILS
HWY. 67/412 INTERCHANGE
U.S. HWY. 412 GRADE SEPARATION
LAWRENCE COUNTY
ROUTE 67 SEC. 16

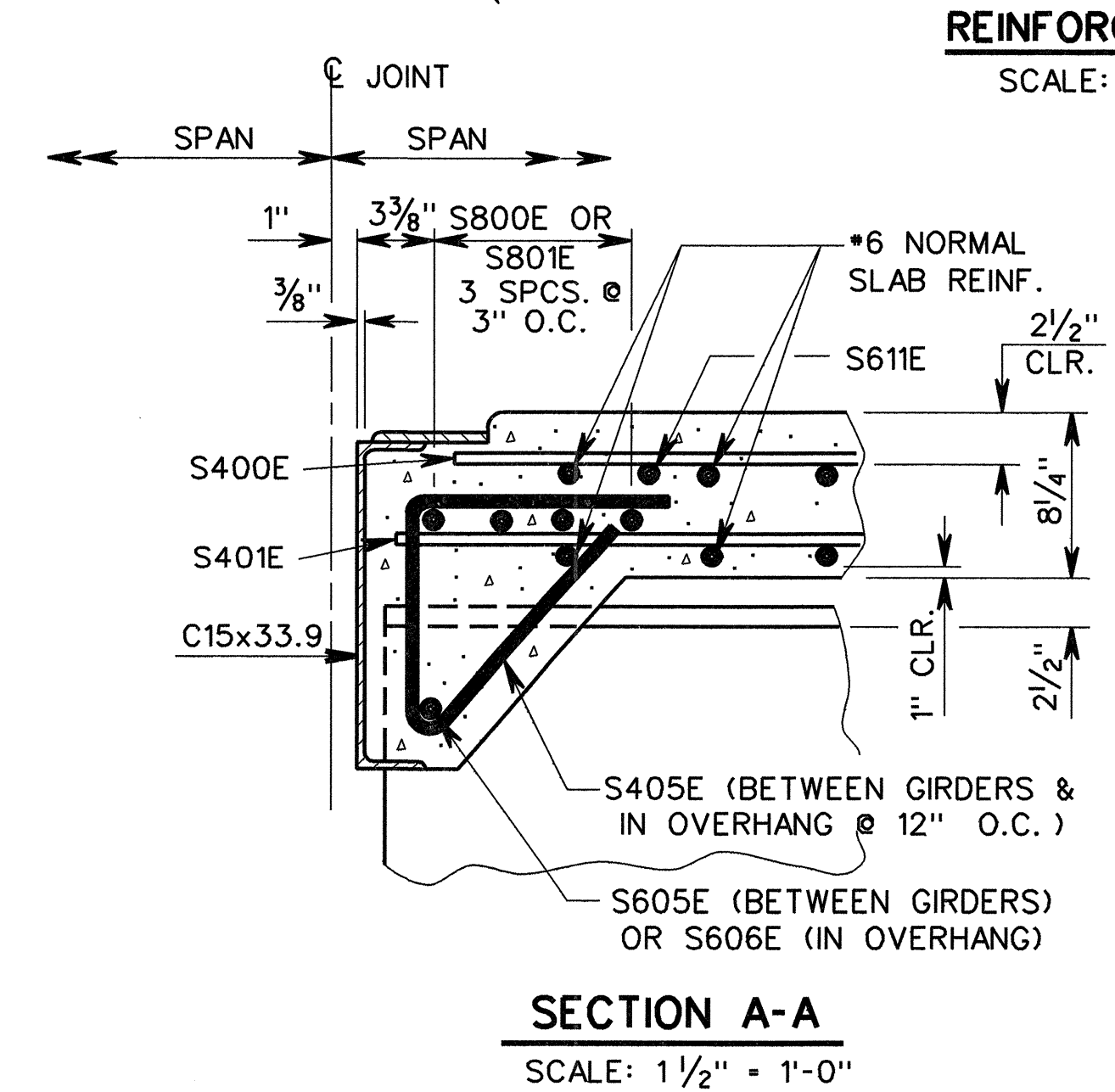
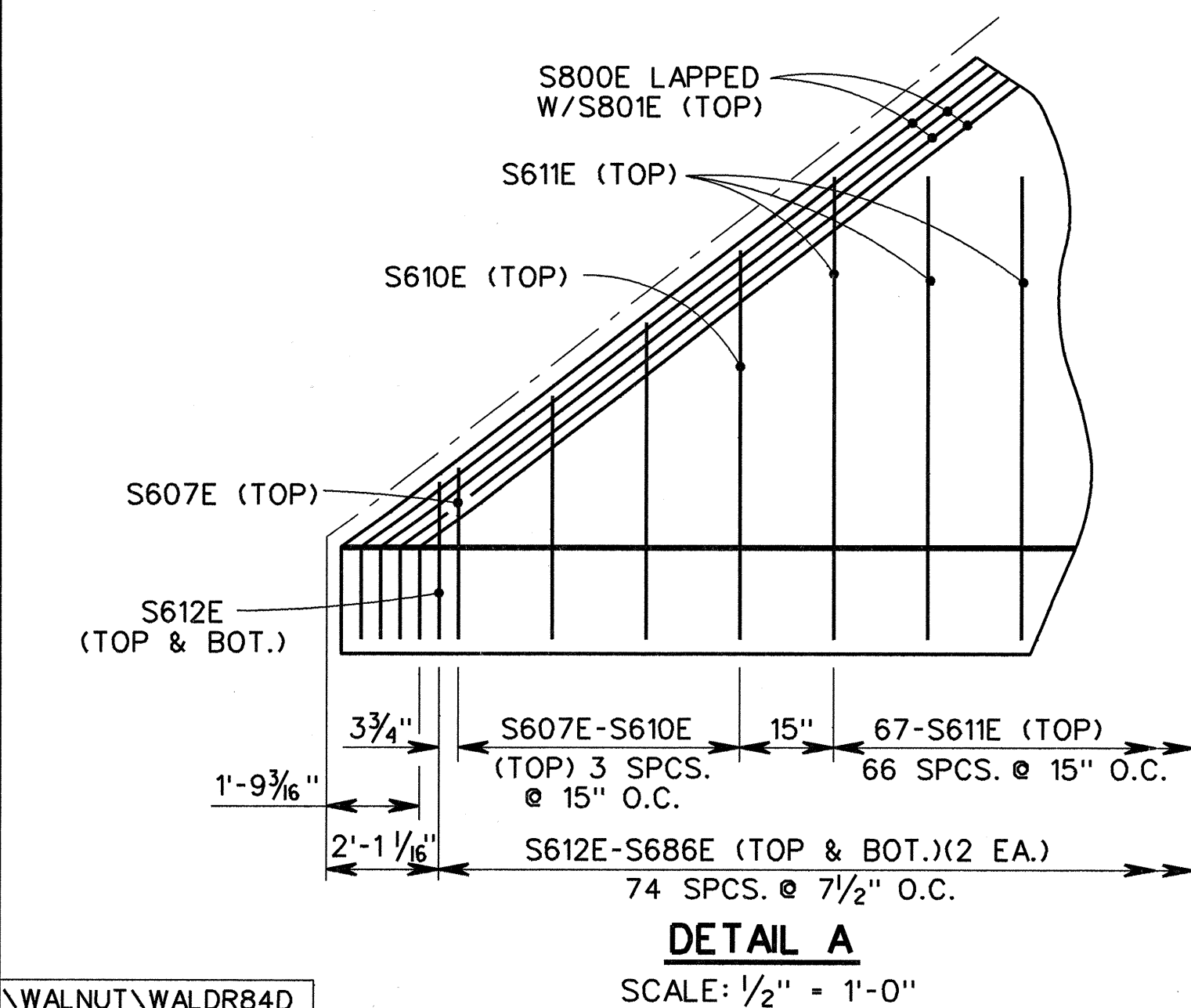
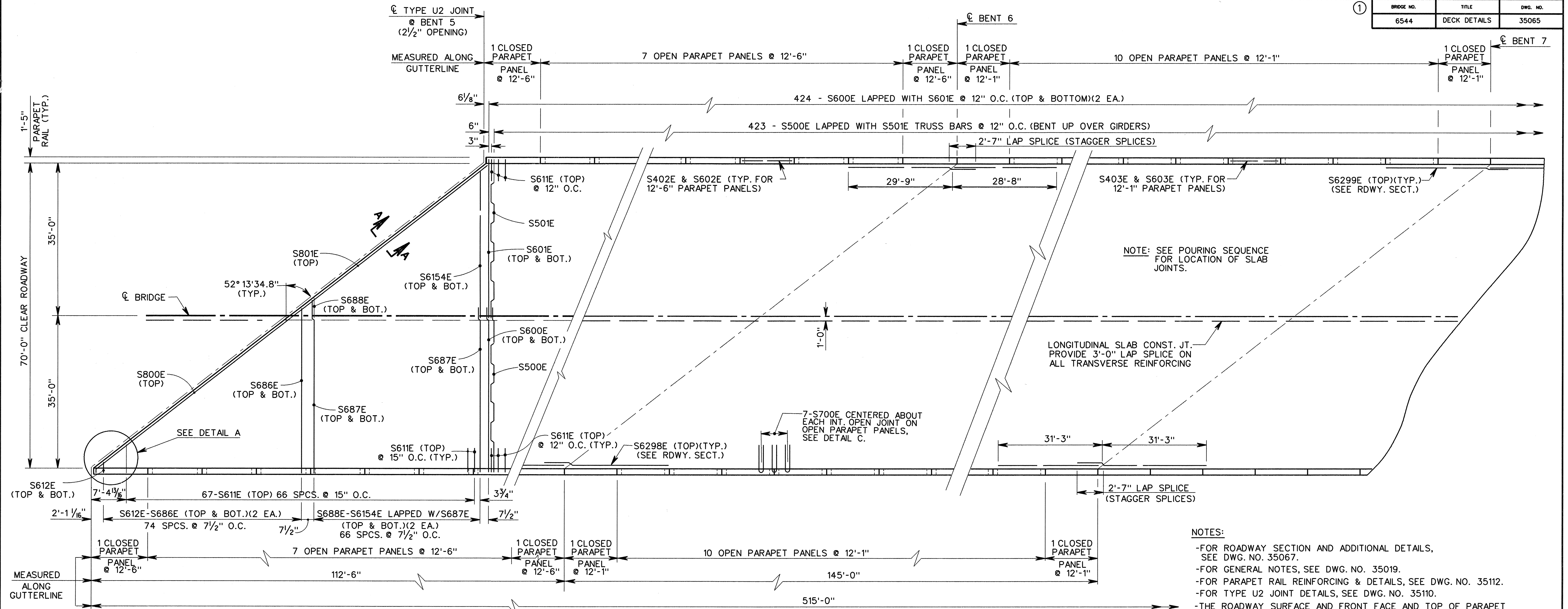
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

DRAWN BY: C.G.H. DATE: NOV. '95
CHECKED BY: D.K.M. DATE: OCT. '96
DESIGNED BY: A.A.B. DATE: JUN. '95
SCALE: AS SHOWN

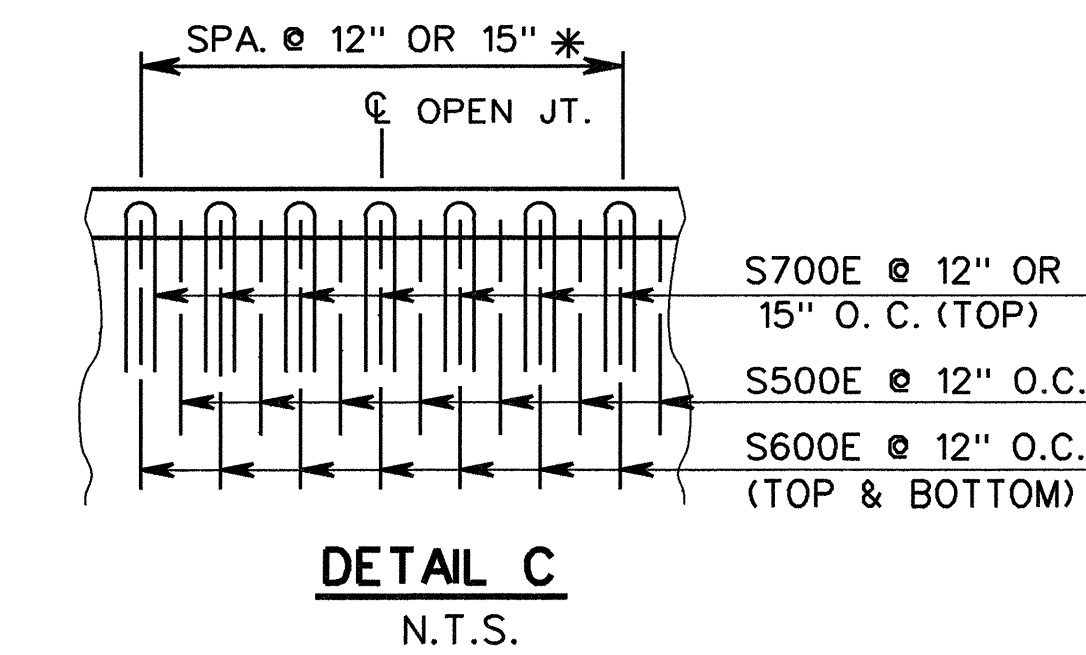
BRIDGE NO. 6544

DRAWING NO. 35064

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK		66	300
				JOB NO.	R00106			
				BRIDGE NO.	TITLE		DWG. NO.	
				6544	DECK DETAILS		35065	



* IN SKEWED AREAS W/ #6 TRANSVERSE REINF. @ $\frac{7}{2}$ " O.C. CENTER S700E'S ON EVERY OTHER BAR.



CTA/PB CRAFTON TULL & ASSOCIATES, INC. PARSONS BRINCKERHOFF

A JOINT VENTURE ROGERS ARKANSAS



SHEET 1 OF 3

UNIT 5-8 DECK DETAILS

HWY. 67/412 INTERCHANGE

U.S. HWY. 412 GRADE SEPARATION

LAWRENCE COUNTY

ROUTE 67 SEC. 16

ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

DRAWN BY: C.G.H. DATE: NOV. '95

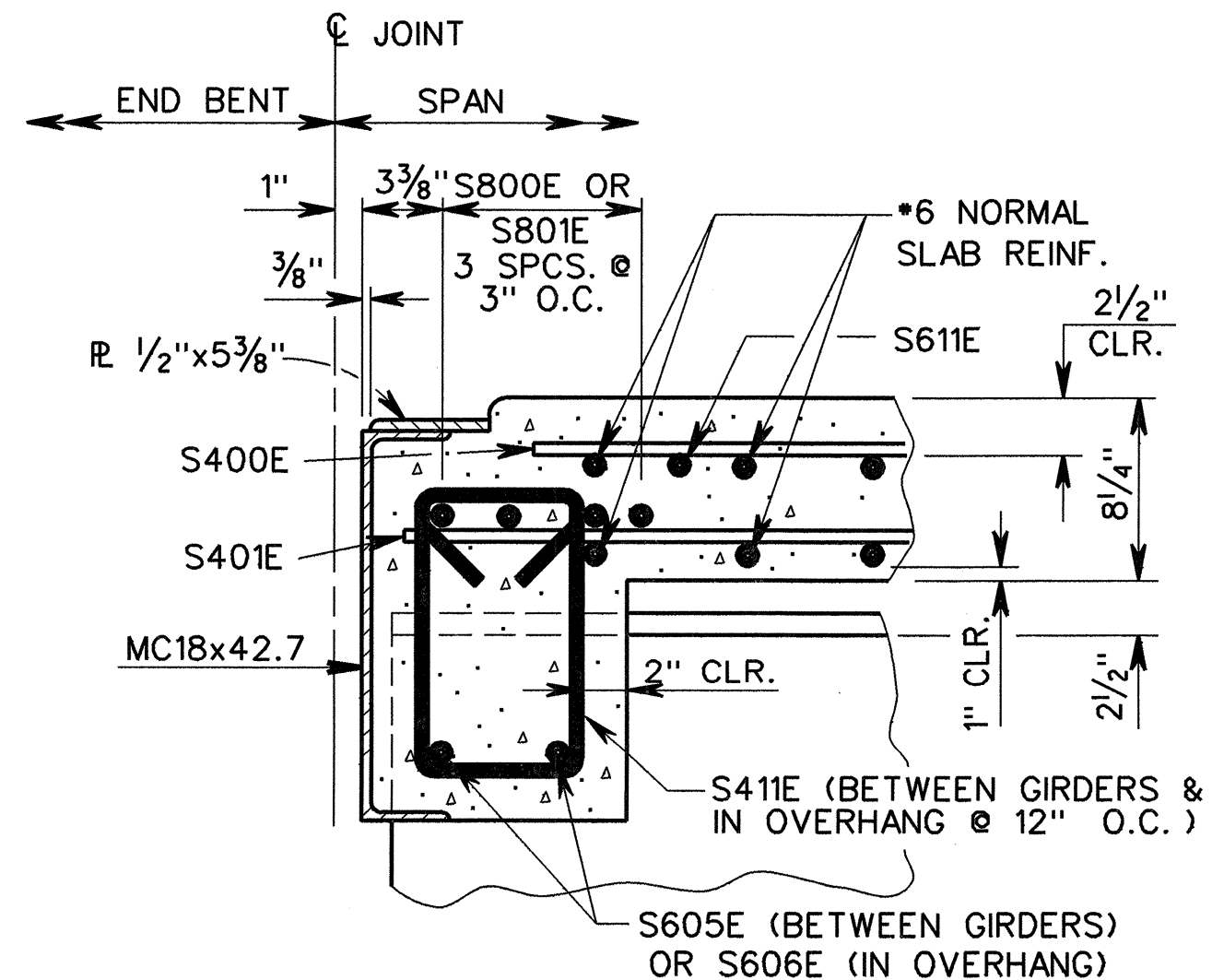
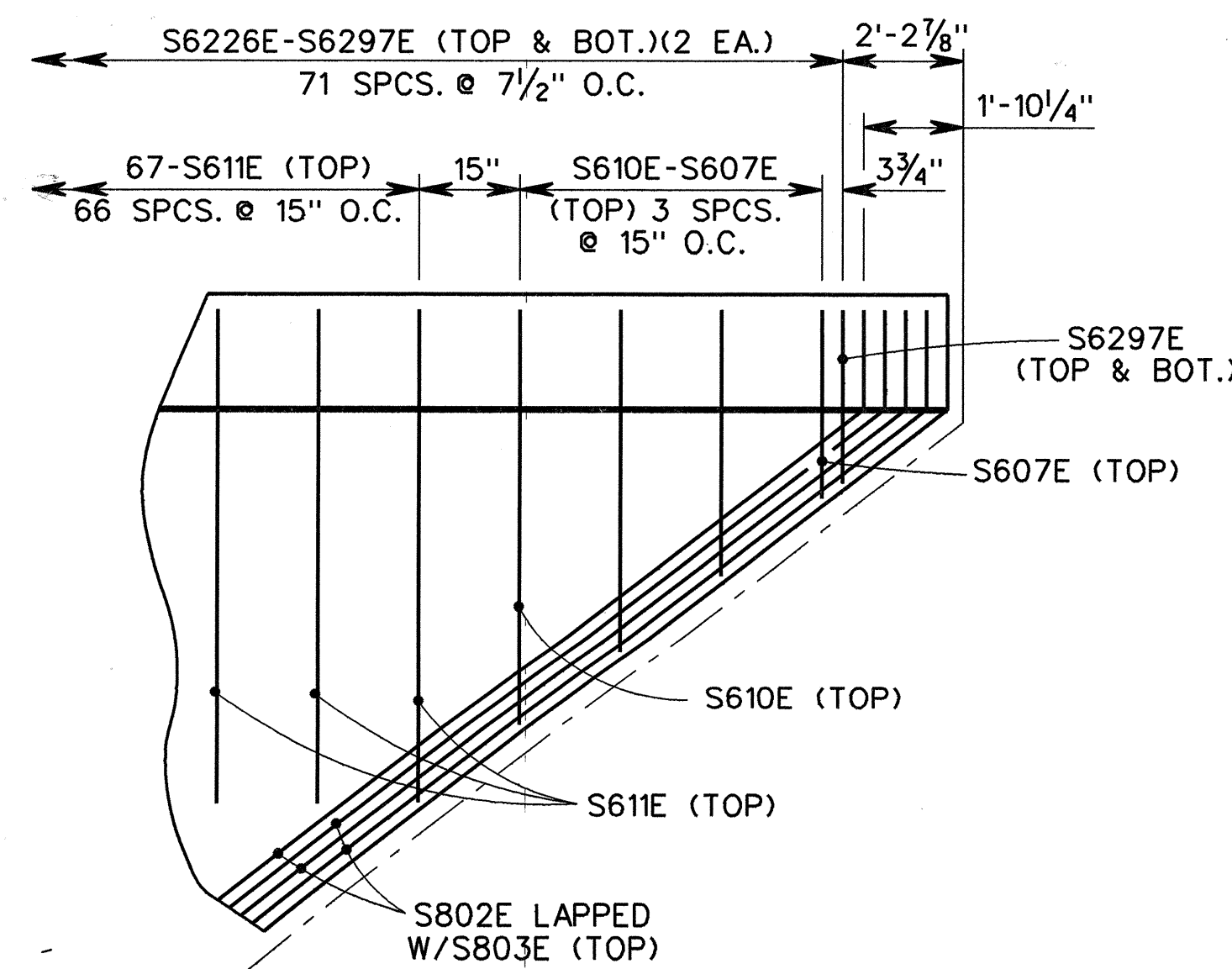
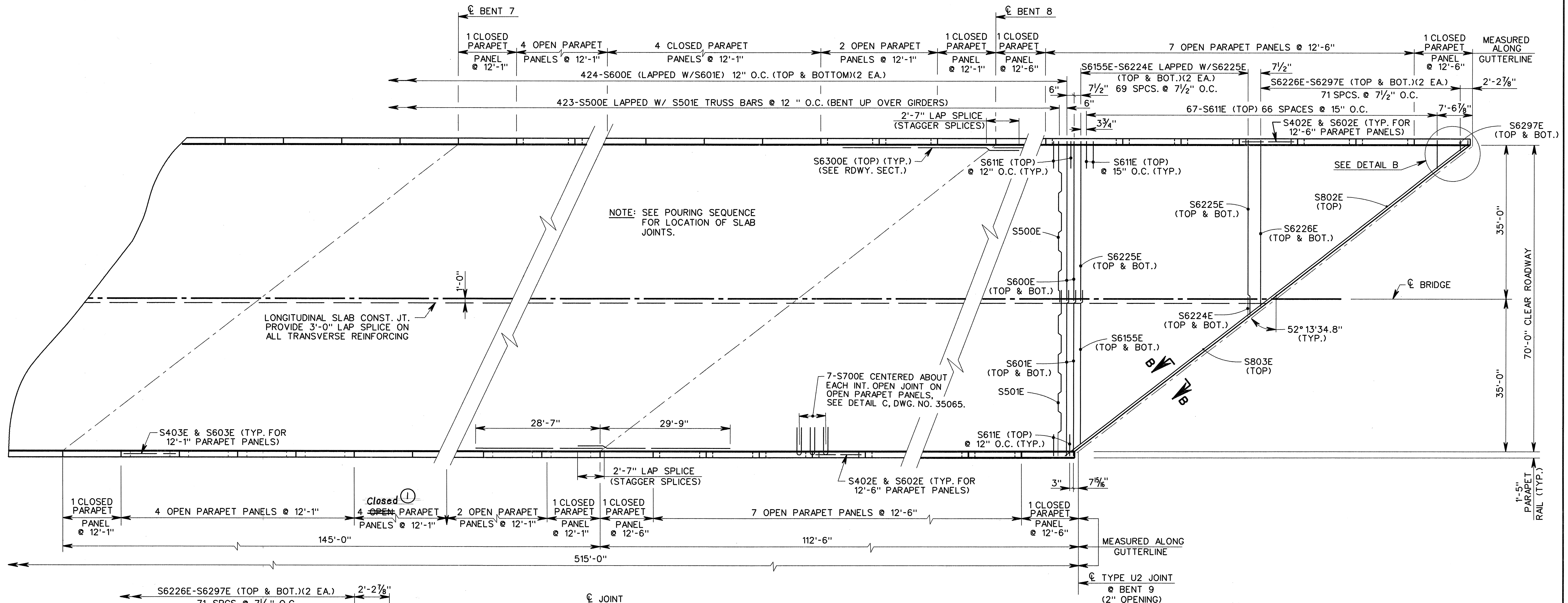
CHECKED BY: D.K.M. DATE: OCT. '96

DESIGNED BY: D.K.M. DATE: JUN. '95

SCALE: AS SHOWN

BRIDGE NO. 6544 DRAWING NO. 35065

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
7-14-99	9-5-99			6	ARK		67	306
				JOB NO.	R00106			
				BRIDGE NO.	TITLE		DWG. NO.	
				6544	DECK DETAILS		35066	



NOTES:

- FOR ROADWAY SECTION AND ADDITIONAL DETAILS, SEE DWG. NO. 35067.
- FOR GENERAL NOTES, SEE DWG. NO. 35019.
- FOR PARAPET RAIL REINFORCING & DETAILS, SEE DWG. NO. 35112.
- FOR TYPE U2 JOINT DETAILS, SEE DWG. NO. 35110.
- THE ROADWAY SURFACE AND FRONT FACE AND TOP OF PARAPET RAILING TO BE TREATED WITH CLASS 1 PROTECTIVE SURFACE TREATMENT.
- AT THE CONTRACTORS OPTION, FOUR STRAIGHT #5 BARS MAY BE SUBSTITUTED IN LIEU OF BARS S500E & S501E. BARS SHALL BE EPOXY COATED. PAYMENT WILL BE BASED ON THE WEIGHT OF BARS S500E & S501E.

① Revised: Parapet Rail. A. May, 7-14-99

CTA/PB CRAFTON TULL & ASSOCIATES, INC.
PARSONS BRINCKERHOFF

A JOINT VENTURE ROGERS ARKANSAS

SHEET 2 OF 3

UNIT 5-8 DECK DETAILS

HWY. 67/412 INTERCHANGE

U.S. HWY. 412 GRADE SEPARATION

LAWRENCE COUNTY

ROUTE 67 SEC. 16

ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

DRAWN BY: C.G.H. DATE: NOV. 95

CHECKED BY: D.K.M. DATE: OCT. 96

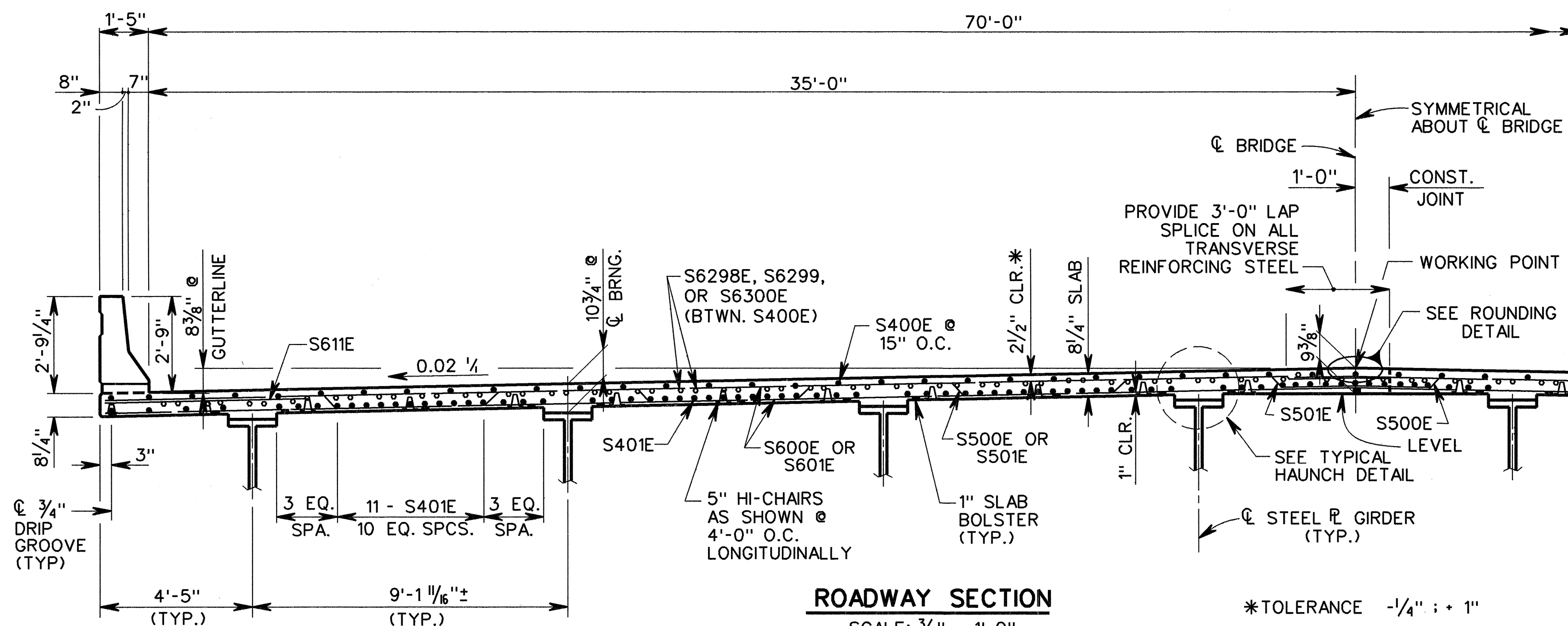
DESIGNED BY: D.K.M. DATE: JUN. 95

SCALE: AS SHOWN

BRIDGE NO. 6544 DRAWING NO. 35066



DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK		68	306
				JOB NO.	R00106			
				BRIDGE NO.	TITLE		DWG. NO.	
				6544	DECK DETAILS		35067	



TRANSVERSE SLAB REINFORCING:
 S600E & S601E (TOP & BOTTOM) @ 12" O.C. — ALTERNATE
 S500E & S501E TRUSS @ 12" O.C.
 S611E @ 12" O.C. (IN OVERHANGS)

ROADWAY SECTION
 SCALE: 3/8" = 1'-0"
 (LOOKING AHEAD)

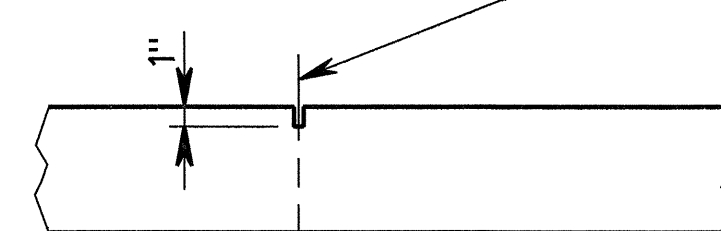
LONGITUDINAL SLAB REINFORCING:
 S400E(TOP) & S401E (BOTTOM) SPACED AS SHOWN.
 S6298E, S6299E & S6300E (TOP) OVER INT. BENTS
 AS SHOWN HERE AND ON REINFORCING PLAN.

*TOLERANCE -1/4" ; + 1"

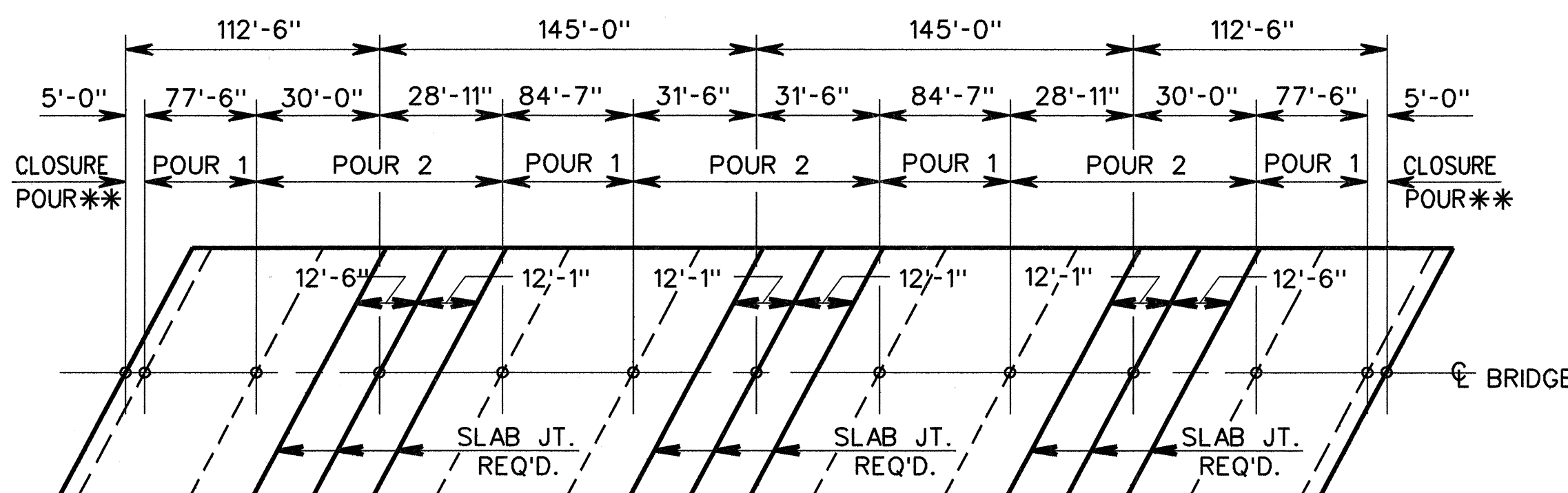
FOR EXPLANATION OF PARAPET VARIABLES SEE DWG. NO. 35112.

TABLE OF PARAPET VARIABLES							
f	OPEN PARAPET					CLOSED PARAPET	
	a	b	c	k	m	g	n
12'-6"	2'-9"	7'-0"	8"	6 1/2"	6	9"	11
12'-1"	2'-6 1/2"	7'-0"	8"	4"	6	6 1/2"	11

1/2" x 1" TYPE 6 JOINT SEALER. SEE SECTIONS 501.02(h) AND 501.05(j) OF THE STANDARD SPECIFICATIONS. JOINT SEALER SHALL BE MEASURED AND PAID FOR AS CLASS S(AE) CONCRETE-BRIDGE. SLAB JOINTS SHALL EXTEND TO THE OUTSIDE EDGE OF THE DECK SLAB. SLAB JOINTS SHALL BE INSTALLED BEFORE THE PARAPET RAILING IS POURED. IF SLAB JOINTS ARE TO BE SAWED, THEY SHALL BE SAWED BEFORE ANY VEHICULAR TRAFFIC IS ALLOWED ON THE UNIT.



SLAB JOINT DETAIL
 NO SCALE



** AFTER ALL POURS (1) AND POURS (2) ON BOTH UNITS ADJACENT TO JOINT IS COMPLETE, JOINT CLOSURE POURS ON EACH SIDE OF JOINT SHALL BE POURED SIMULTANEOUSLY. 48 HOURS SHALL ELAPSE BETWEEN END OF LAST POUR (2) AND START OF JOINT CLOSURE POUR.

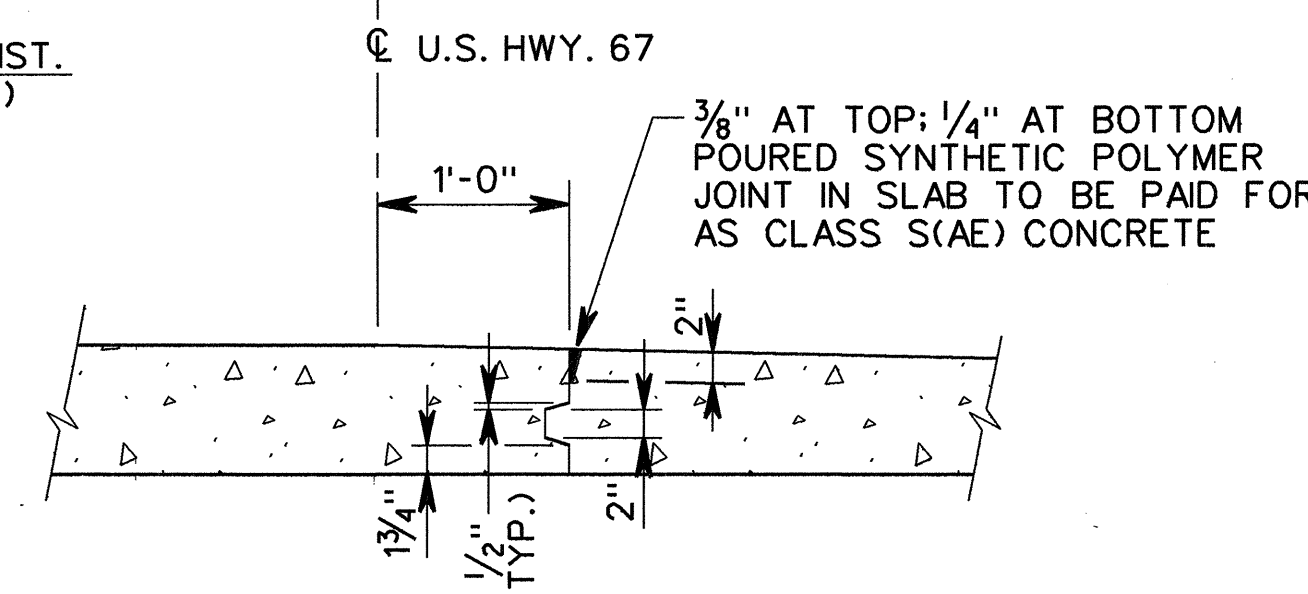
POURING SEQUENCE
 NO SCALE

NOTES:

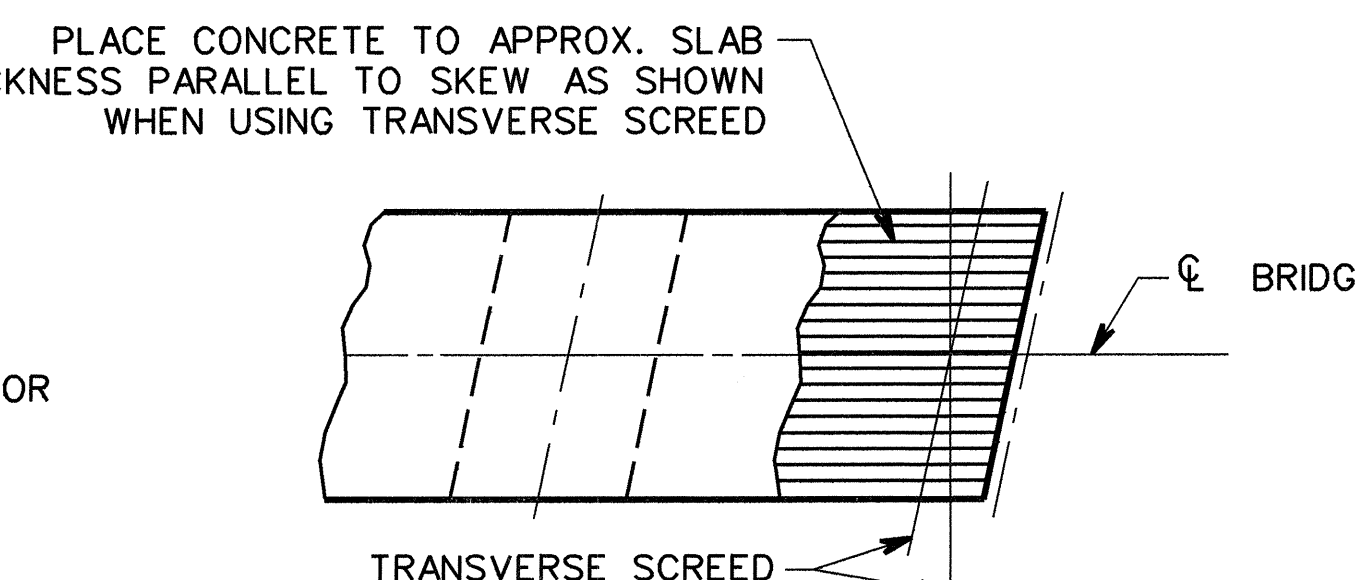
POURS WITH THE SAME NUMBER MAY BE PLACED SIMULTANEOUSLY OR SEPARATELY. ALL POURS (1) MUST BE PLACED BEFORE POURS (2) CAN BE PLACED. 48 HOURS SHALL ELAPSE BETWEEN POURS AND 72 HOURS SHALL ELAPSE BETWEEN ADJACENT POURS. ANY RAILING POURS MADE BEFORE ENTIRE SLAB UNIT HAS BEEN PLACED MUST BE APPROVED BY THE BRIDGE ENGINEER.

ALL CONCRETE IN BRIDGE SUPERSTRUCTURE SHALL BE PLACED, CONSOLIDATED AND SCREEDED OFF FOR THE ENTIRE POUR BEFORE ANY CONCRETE HAS TAKEN ITS INITIAL SET. THIS MAY REQUIRE THE USE OF A RETARDING AGENT. THE CONCRETE DECK SHALL BE FINISHED IN ACCORDANCE WITH SECTION 802.19 OF THE STANDARD SPECIFICATIONS. MOVEMENT OF THE FINISHING MACHINE ACROSS NEW CONCRETE SHALL BE ON PLANKS PLACED ON THE SURFACE AND SHALL BE PROHIBITED FOR 72 HOURS AFTER FINISHING THE POUR.

THE CONTRACTOR MUST OBTAIN APPROVAL FROM THE BRIDGE ENGINEER FOR ANY DEVIATIONS FROM THE POURING SEQUENCE SHOWN.

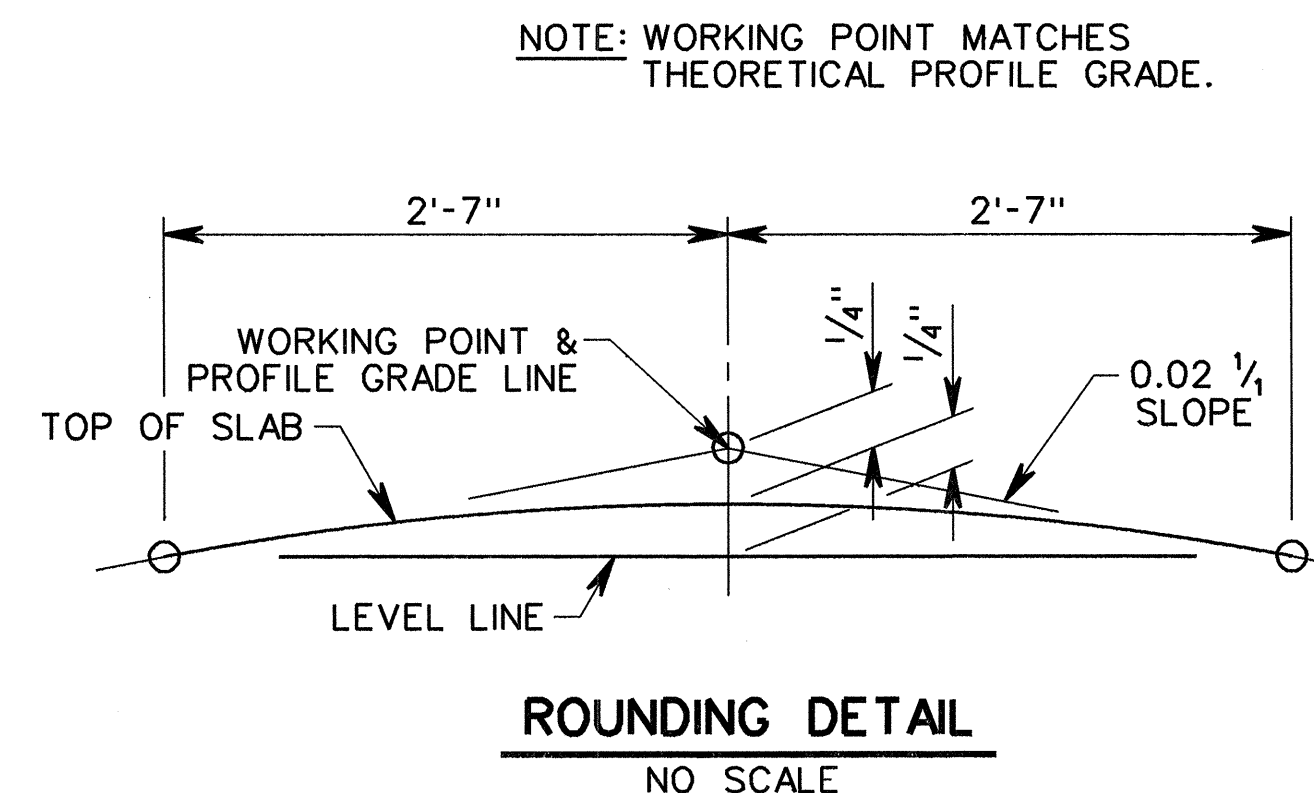


LONGITUDINAL CONST. JT. DETAIL
 NO SCALE

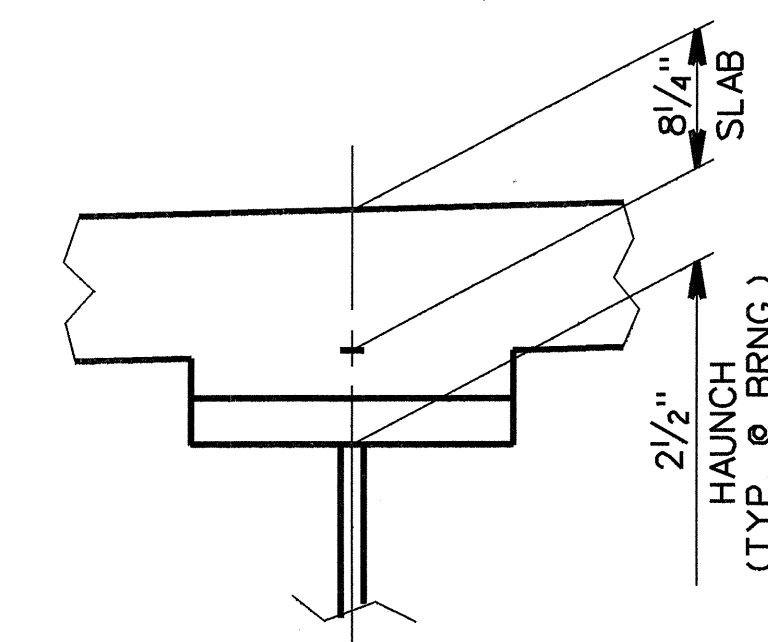


CONCRETE PLACEMENT PROCEDURE
 NO SCALE

NOTE: AT THE CONTRACTOR'S OPTION, THE TRANSVERSE SCREED MAY BE PLACED ON A RADIAL LINE OR PARALLEL TO THE SKEW. SCREED RAIL SUPPORT DETAIL SHOWN ON DWG. 35076 SHALL APPLY TO THIS BRIDGE



ROUNDING DETAIL
 NO SCALE



TYPICAL HAUNCH DETAIL
 NO SCALE

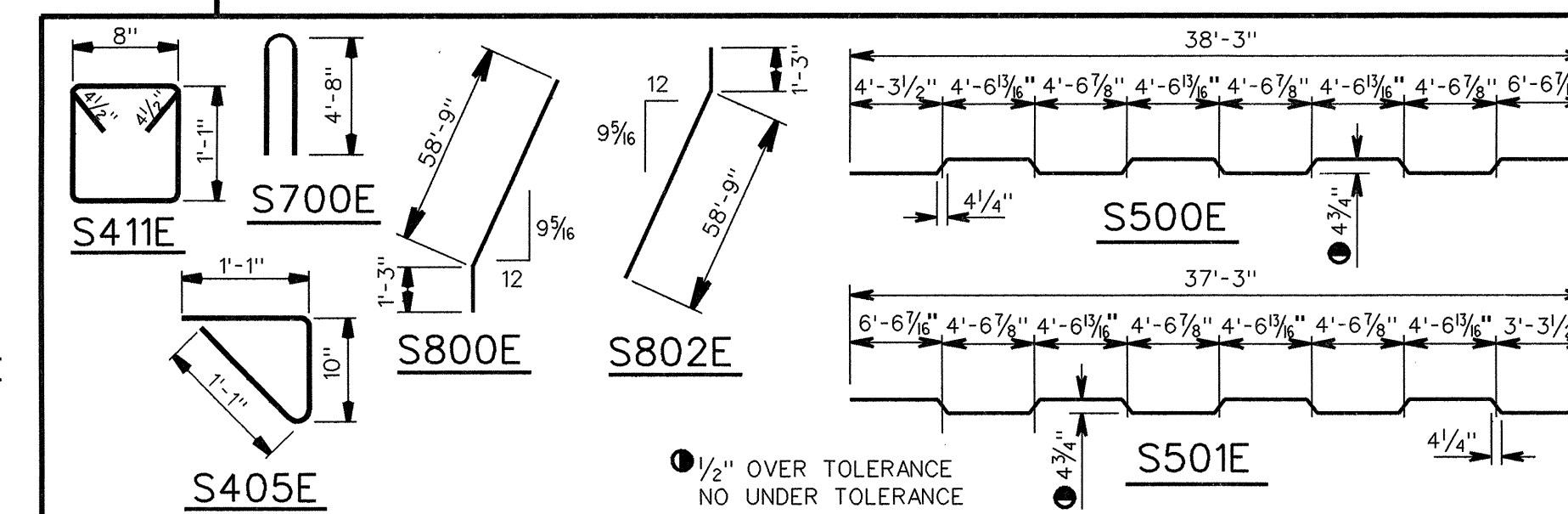
NOTE: TOLERANCE FOR SLAB THICKNESS IS MINUS 1/4" AND PLUS 1". PROVIDE HAUNCH AS REQ'D TO MEET SLAB TOLERANCES. NO ADJUSTMENT FOR INCREASE IN CONCRETE AND STRUCTURAL STEEL QUANTITIES WILL BE MADE FOR THICKENING SLAB OR DEEPENING HAUNCH TO MEET TOLERANCES.

THE SUPERSTRUCTURE DETAILS SHOWN ARE FOR USE WHEN REMOVABLE DECK FORMING IS USED AND ARE THE BASIS FOR MEASUREMENT OF CLASS S(AE) CONCRETE. SEE STANDARD DRAWING 14991 FOR ALLOWABLE MODIFICATIONS AND FOR TOLERANCES WHEN PERMANENT STEEL BRIDGE DECK FORMS ARE USED.

REINFORCING SCHEDULE - UNIT 5-8

MARK	NO.	LENGTH	PIN DIA.	MARK	NO.	LENGTH	PIN DIA.
S800E	4	60'-0"	6"	S6226E	2 EA.	VARIES 36'-11" TO 2'-6"	STR.
S801E	4	59'-10"	STR.	S6297E	224	31'-9"	STR.
S802E	4	60'-0"	6"	S6298E	224	33'-10"	STR.
S803E	4	59'-10"	STR.	S6299E	224	31'-9"	STR.
				S6300E	423	39'-2"	3"
S700E	350	9'-8"	6 1/2"	S500E	423	38'-2"	3"
				S501E	826	38'-4"	STR.
S600E	850	38'-2"	STR.	S400E	1778	38'-4"	STR.
S601E	850	37'-2"	STR.	S401E	92	2'-11"	3"
S605E	21	14'-1"	STR.	S411E	92	3'-9"	2"
S606E	6	4'-3"	STR.	S602E	140	12'-2"	STR.
S607E	2 EA.	VARIES 2'-8" TO 5'-7"	STR.	S603E	160	11'-9"	STR.
S610E	2 EA.	VARIES 2'-5" TO 3'-6"	STR.	S402E	112	12'-2"	STR.
S611E	982	5'-9"	STR.	S403E	128	11'-9"	STR.
S612E	2 EA.	VARIES 2'-5" TO 3'-6"	STR.	S406E	480	5'-6"	2"
S686E	2 EA.	VARIES 3'-6" TO 3'-6"	STR.	S408E	480	6'-6"	2"
S687E	65	38'-3"	STR.	S409E	420	3'-2"	2"
S688E	2 EA.	VARIES 3'-6" TO 3'-6"	STR.	S410E	420	6'-0"	2"
S6154E	2 EA.	VARIES 36'-6" TO 3'-2"	STR.	S402E	64	12'-2"	STR.
S6155E	2 EA.	VARIES 36'-6" TO 3'-2"	STR.	S403E	128	11'-9"	STR.
S6224E	70	37'-3"	STR.	S406E	288	5'-6"	2"
				S407E	288	5'-8"	2"

BENDING DIAGRAM



BENDING NOTES:

- DIMENSIONS OF BARS ARE OUT TO OUT.
- FOR PARAPET RAIL BAR BENDING DIAGRAMS, SEE DWG. NO. 35112.
- BARS TO BE EPOXY COATED, SEE SECTION 804 OF THE STANDARD SPECIFICATIONS FOR EPOXY COATED REINFORCING STEEL (GR. 60)

NOTES:

- FOR GENERAL NOTES, SEE DWG. NO. 35019.
- FOR PARAPET RAIL REINFORCING & DETAILS, SEE DWG. NO. 35112.
- FOR TYPE U2 JOINT DETAILS, SEE DWG. NO. 35110.
- THE ROADWAY SURFACE AND FRONT FACE AND TOP OF PARAPET RAILING TO BE TREATED WITH CLASS 1 PROTECTIVE SURFACE TREATMENT.
- AT THE CONTRACTORS OPTION, FOUR STRAIGHT #5 BARS MAY BE SUBSTITUTED IN LIEU OF BARS S500E & S501E. BARS SHALL BE EPOXY COATED. PAYMENT WILL BE BASED ON THE WEIGHT OF BARS S500E & S501E.

CTA/PB CRAFTON TULL & ASSOCIATES, INC.
PARSONS BRINCKERHOFF

A JOINT VENTURE ROGERS ARKANSAS

SHEET 3 OF 3

UNIT 5-8 DECK DETAILS
 HWY. 67/412 INTERCHANGE
 U.S. HWY. 412 GRADE SEPARATION
 LAWRENCE COUNTY
 ROUTE 67 SEC. 16

ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

DRAWN BY: C.G.H. DATE: NOV. '95
 CHECKED BY: D.K.M. DATE: OCT. '96
 DESIGNED BY: D.K.M. DATE: JUN. '95

SCALE: AS SHOWN

BRIDGE NO. 6544

DRAWING NO. 35067

WALNUT WALDR84F

MICROFILMED
 JAN 27 1999