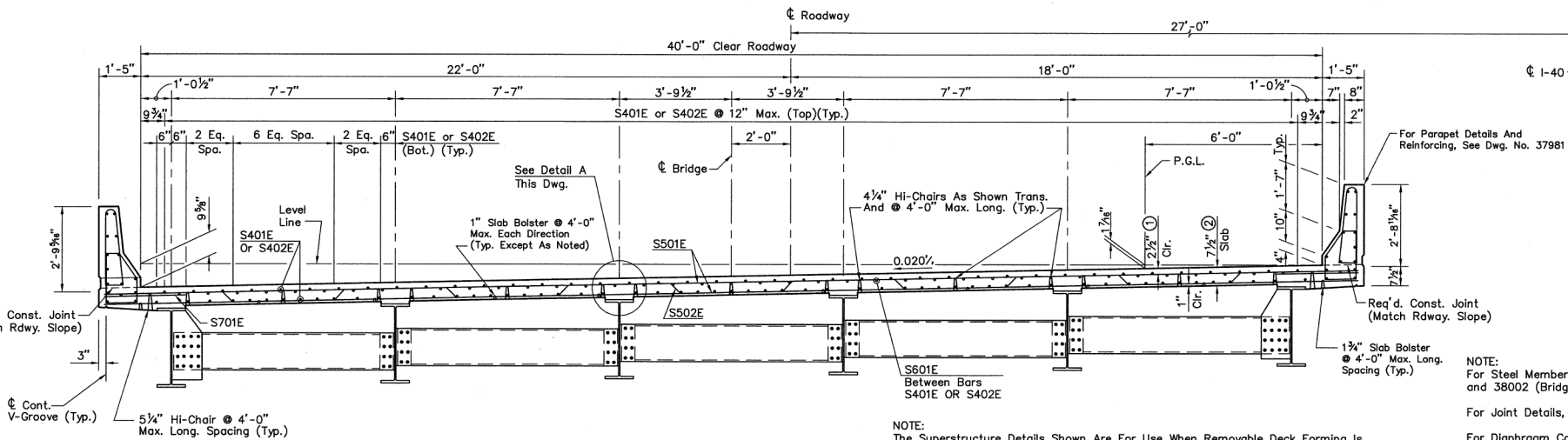


DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AID PROJ. NO.
				6	ARK	
				JOB NO.		R100

① A&B 3727 & A&B 3731



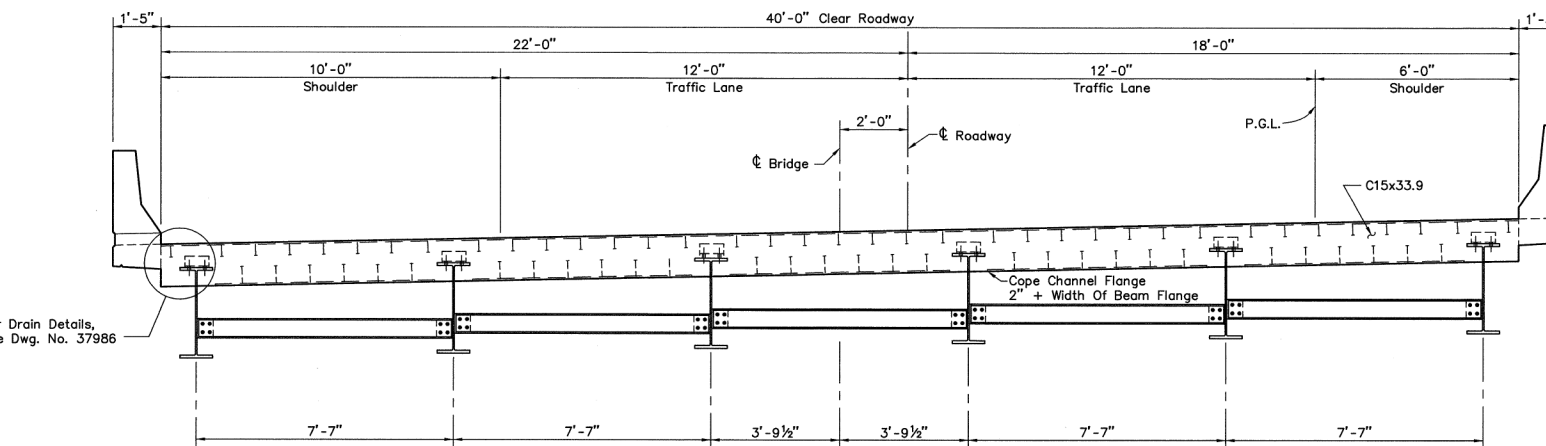
#### LAB REINFORCING:

TRANSVERSE: S501E @ 12" In Top & Bottom  
S502E @ 12" Bent Up Over Beams  
S701E (See Plan For Placement)  
S401E & 402E In Top (Placed As Shown - 12" Max. Spa.)  
S401E & 402E In Bottom (Placed As Shown)  
S601E or S602E In Top  
(Placed As Shown - 12" Max. Spa.) (Over Int. Supports)

#### TYPICAL SECTION

Scale:  $\frac{1}{2}" = 1'-0"$   
Looking Ahead Bridge A  
Bridge B Sym. About C-L-40

- ① Tolerance Minus:  $\frac{1}{4}"$   
Plus: Equal To Amount Of Slab Thickening Used To Meet Slab Thickness Tolerance - See Typical Haunch Detail.
- ② Refer To Typical Haunch Detail

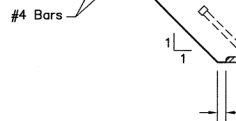


#### SECTION AT C-L-40

Scale:  $\frac{1}{2}" = 1'-0"$   
Looking Ahead Bridge A  
Bridge B Symmetrical  
About C-L-40

Note: Refer To Dwg. No. 37986  
For Joint Details.

Hold Ends Of Longitudinal  
Reinforcing Steel As Close  
To Channel As Possible.



#### SLAB END DETAIL

NOTE: For Anchor Stud  
See Dwg. No. 37986

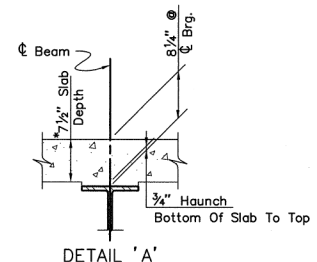
NOTE:  
For Steel Member Sizes, See Dwg. Nos. 37979 (Bridge 3727)  
and 38002 (Bridge 3731).

For Joint Details, See Dwg. No. 37986.

For Diaphragm Connection Details, See Dwg. No. 37985.

Class 1 Protective Surface Treatment Shall Be Applied To The  
Surface And To The Face And Top Of Parapet.

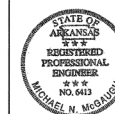
At The Contractors Option, In Lieu Of Providing Bars S502E, Two  
May Be Substituted With The Bars Epoxy Coated. Payment For  
Will Be Based On The Weight Of Bars S502E.



Haunch Is Required. Slab May Be Thickened And/Or  
The Haunch Thickened To Maintain Slab Tolerance.

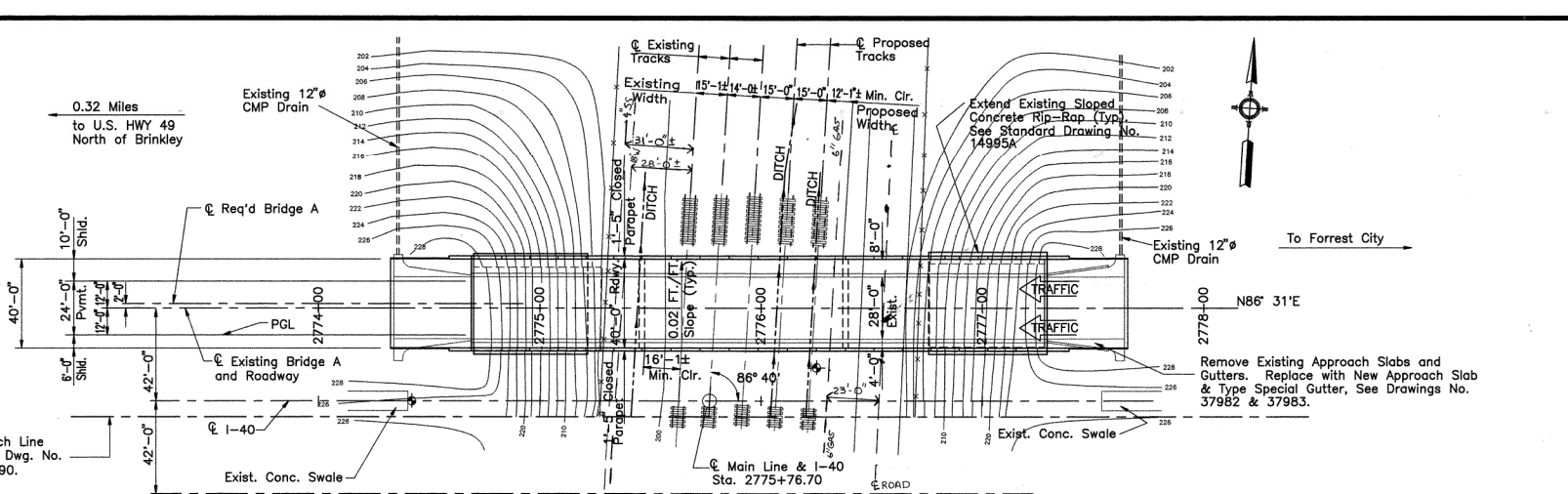
\*Thickness As Detailed On Roadway Section. Tolerance  
Is Minus  $\frac{1}{4}"$  And Plus  $\frac{1}{2}"$ .

Note: No Increase In Concrete And Structural Steel Quantities  
Will Be Made To Meet Slab Tolerances.



ENGSTROM/MODJESKI AND MASTER CONSULTING ENGINEERS	
TYPICAL SUPERSTRUCTURE	
BRIDGE A&B 3727 BRIDGE	
MONROE COUNTY INTERSTATE ROUTE 40 ARKANSAS STATE HIGHWAY C LITTLE ROCK, ARK.	
DRAWN BY: DHH	DATE: 11/98
CHECKED BY: MNM/GPS	DATE: 1/97
DESIGNED BY: GPS	DATE: 9/94
BRIDGE NO. A & B 3727	SCALE: $\frac{1}{2}" = 1'-0"$
A & B 3731	DRAWING

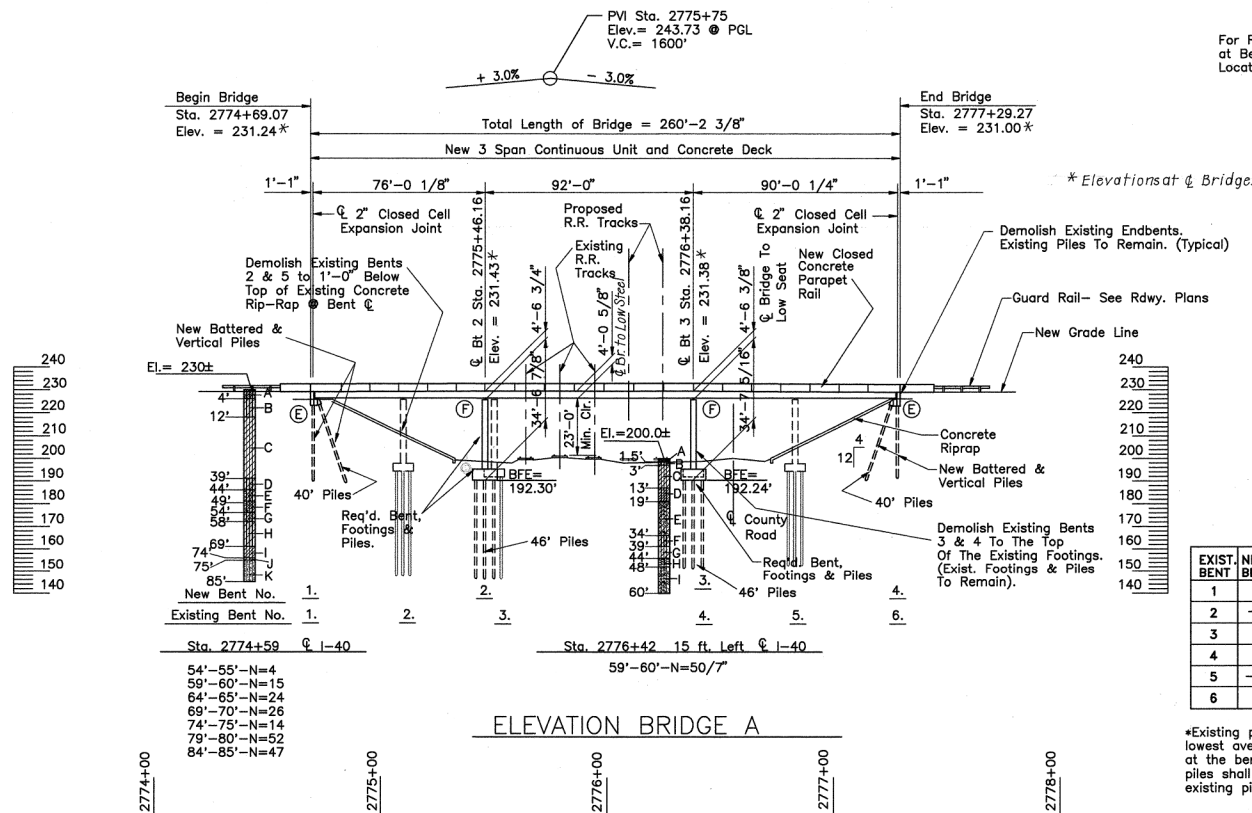
ABMB ENGINEERS, INC.



PLAN BRIDGE A

Denotes Boring Location

For Repair of Concrete Rip-Rap at Bents and Undermined Locations See Sheet 37993



ELEVATION BRIDGE A

\*Existing pile tip elevations are the lowest average pile tip elevations at the bents. At minimum, required piles shall be driven to the existing pile tip elevation.

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD NO.	STATE	FED. AID PR.
				6	ARK.	
				JOB NO.		R10055
				A3731		Layout

# BORING LOGS

Sta. 2774+59 @ I-40

- A-Medium-Dense Tan Sandy Silt With Clay Pockets
- B-Very Stiff Tan & Gray Clayey Silt With Ferrous Nodules & Clay Pockets
- C-Very Stiff Tan & Gray Silty Clay With Ferrous Stains
- D-Stiff Tan Clayey Silt
- E-firm, slightly sandy below 44 ft.
- F-very stiff below 49 ft.
- G-Soft Gray & Tan Clayey Silt
- H-stiff to very stiff below 58 ft.
- I-Very Stiff Gray Silty Clay
- J-stiff below 74 ft.
- K-Dense Gray Silty Fine Sand

Sta. 2776+42 15 ft. Left @ I-40

- A-Dense Reddish Tan Sand With Coarse Gravel (Fill)
- B-Very Stiff Tan Clayey Silt With Ferrous Stains
- C-Very Stiff Tan Silty Clay With Ferrous Stains, Occasional Sand Pockets & Trace Fine Gravel
- stiff, 5.5 to 6.5 ft.
- D-Firm To Stiff Reddish Tan Clayey Silt
- E-Medium-Dense Reddish Tan Silty Fine Sand, Slightly Clayey (Wet)
- F-Firm Brown Clayey Silt
- G-Firm Gray Silty Clay
- H-Medium-Dense Gray Silty Fine Sand
- I-Medium-Dense Gray Fine To Medium Sand, Slightly Silty With Occasional Clay Seams

## NOTES:

- Stations & Dimensions Based Upon Existing Plans
- (E) Indicates Expansion Bearing
- (F) Indicates Fixed Bearing
- Roadway Elevations Shown Are At @ New Bridges.
- Vertical Dimensions Are From @ Bridge To Top Of Low Riser.
- For General Notes See Drawing No. 37968.
- For Additional Notes And Boring Logs, See Drawing No. 37990.
- Temporary Shoring To Be Provided By The Contractor As Req'd. To Maintain The Existing Track And Protect Workers
- Shoring Shall Conform To The Requirements Of The South Pacific Transportation Co. (SPTCO) Supplemental Specification For Shoring. The Minimum Distance From @ Of Track To Nearest Face Of Shoring Shall Be 8'-6". Shoring Plan A Construction Sequence Shall Be Approved By The Engineer SPTCO Prior To Construction.

EXIST. BENT	NEW BENT	EXISTING PILE TIP ELEVATIONS*
1	1	EL. 190.00
2	—	—
3	2	EL. 147.00
4	3	EL. 152.00
5	—	—
6	4	EL. 192.00



ENGSTROM/MODJESKI AND MASTER CONSULTING ENGINEERS			
STRUCTURE PLAN & ELEVATION			
1 of 2 BRIDGE A3731			
ST. LOUIS - SOUTHWESTER			
MONROE COUNTY			
INTERSTATE ROUTE 40			
ARKANSAS STATE HIGHWAY C			
LITTLE ROCK, ARK.			
DRAWN BY: JHS	DATE: 8/94	CHECKED BY: CDE	DATE: 5-97
DESIGNED BY: CDE	DATE: 8-94	SCALE: 1"=	
BRIDGE NO. A3731		DRAWING	