



Latitude:36.05365, Longitude:-91.31031

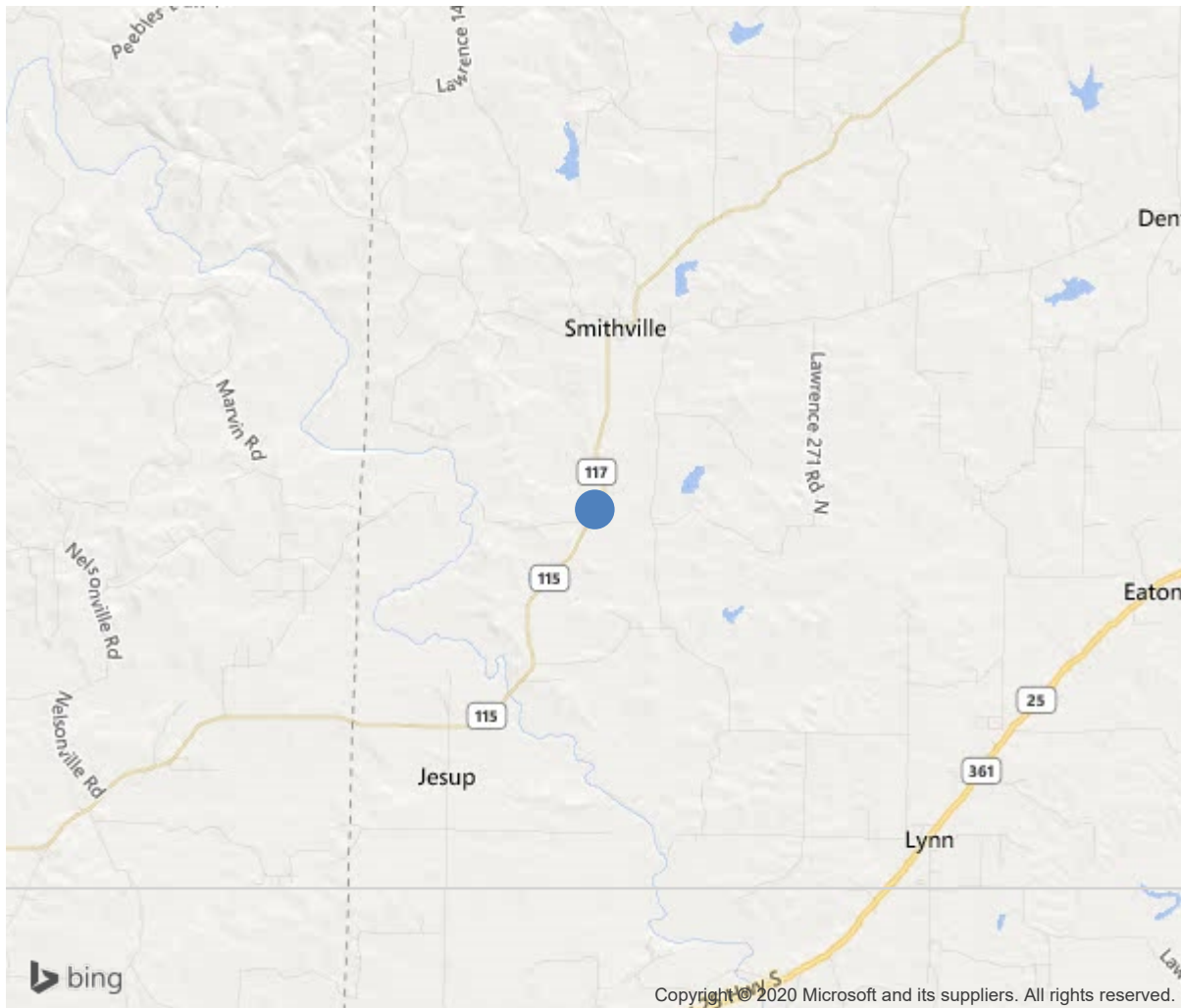
Route:115 Section:02 Log:14.13

Arnold Road ID:38x115x2xA, Arnold Log mile:14.035

District 10, Lawrence County

Owner: 1-State Highway Agency

2 MI S SMITHVILLE



36.05365, -91.31031



Bridge #05217(Routine)
SH 115-02-LM 14.13 over COOPER CREEK

Location: 2 MI S SMITHVILLE

Team Lead: Tim Myrick Inspection Date: July 25, 2016

IDENTIFICATION	
(1) State Names	Arkansas
(8) Structure Number	05217
(5) Inventory Route	115
(2) Highway Agency District	10
(3) County Code	75-Lawrence County, Arkansas
(4) Place Code	0
(6) Features Intersected	COOPER CREEK
(7) Facility Carried	SH 115-02-LM 14.13
(9) Location	2 MI S SMITHVILLE
(11) Mile Point	14.13 mi
(12) Base Highway Network	No
(13) LRS Inventory Rte & Subrte	0000000000
(16) Latitude	36.05365
(17) Longitude	-91.31031
(98) Border Bridge State Code	
(99) Border Bridge Structure No.	
STRUCTURE TYPE AND MATERIAL	
(43) Main Structure Type	32
Material	3-Steel
Type	2-Stringer/Multi-beam or girder
(44) Approach Structure Type	00
Material	0-Other
Type	0-Other
(45) No. of Spans in Main Unit	4
(46) No. of Approach Spans	0
(107) Deck Structure Type	1-Concrete Cast-in-Place
(108) Wearing Surface/Protective System	
Type of Wearing Surface	1-Monolithic Concrete (concurrently placed
Type of Membrane	0-None
Type of Deck Protection	0-None
AGE AND SERVICE	
(27) Year Built	1969
(106) Year Reconstructed	0
(42) Type of Service	15
On	1-Highway
Under	5-Waterway
(28) Lane	
On	2
Under	0
(29) Average Daily Traffic	1600
(30) Year of ADT	2018
(109) Truck ADT	1 %
(19) Bypass, Detour Length	9 mi
GEOMETRIC DATA	
(48) Length of Maximum Span	45 ft
(49) Structure Length	182 ft
(50) Curb or Sidewalk Width	
Left	0 ft
Right	0 ft
(51) Bridge Roadway Width Curb to Curb	27.9 ft
(52) Deck Width Out to Out	32 ft
(32) Approach Roadway Width (W/Shoulders)	27.9 ft
(33) Bridge Median	0-No median
(34) Skew	30 Deg
(35) Structure Flared	No flare
(10) Inventory Route Min Vert Clear	99.99 ft
(47) Inventory Route Total Horiz Clear	29.2 ft
(53) Min Vert Clear Over Bridge Rdwy	99.99 ft
(54) Min Vert Underclear	0 ft
Ref:	
(55) Min Lat Underclear RT	99.9 ft
Ref:	
(56) Min Lat Underclear LT	0 ft
NAVIGATION DATA	
(38) Navigation Control	0-No navigation control on water
(111) Pier Protection	5-None present but re-evaluation
(39) Navigation Vertical Clearance	0 ft
(116) Vert-Lift Bridge Nav Min Vert Clear	0 ft
(40) Navigation Horizontal Clearance	0 ft

CLASSIFICATION	
(112) NBIS Bridge Length	Y
(104) Highway System	0
(26) Functional Class	7-Rural Major Collector
(100) Defense Highway	0-The inventory route is not a S
(101) Parallel Structure	N-No parallel structure exists.
(102) Direction of Traffic	2 - way traffic
(103) Temporary Structure	
(105) Federal Lands Highways	0-N/A
(110) Designated National Network	0-The inventory route is not part of
(20) Toll	3-On free road. The structure is toll-
(21) Maintain	1-State Highway Agency
(22) Owner	1-State Highway Agency
(37) Historical Significance	5-Bridge is not eligible for the NRHP
CONDITION	
(58) Deck	6
(59) Superstructure	5
(60) Substructure	5
(61) Channel & Channel Protection	7
(62) Culverts	N
LOAD RATING AND POSTING	
(31) Design Load	4-M 18 / H 20
(63) Operating Rating Method	1
(64) Operating Rating	
Type	1-Load Factor(LF)
Rating	51
(65) Inventory Rating Method	1-Load Factor(LF)
(66) Inventory Rating	
Type	4
Rating	31
(70) Bridge Posting	5-Equal to or above legal loads
(41) Structure Open/Posted/Closed	A-Open, no restriction
APPRAISAL	
(67) Structural Evaluation	5
(68) Deck Geometry	5
(69) Clearances, Vertical/Horizontal	N
(71) Waterway Adequacy	9
(72) Approach Roadway Alignment	8
(36) Traffic Safety Features	0000
A) Bridge Railings	0-Inspected feature does not meet cur
B) Transitions	0-Inspected feature does not meet cur
C) Approach Guardrail	0-Inspected feature does not meet cur
D) Approach Guardrail Ends	0-Inspected feature does not meet cur
(113) Scour Critical Bridges	8-Bridge foundations determined to be
PROPOSED IMPROVEMENTS	
(75) Type of Work	
(76) Length of Structure Improvement	0 ft
(94) Bridge Improvement Cost	\$ 0
(95) Roadway Improvement Cost	\$ 0
(96) Total Project Cost	\$ 0
(97) Year of Improvement Cost Estimate	
(114) Future ADT	1250
(115) Year of Future ADT	2028
INSPECTIONS	
(90) Inspection Date	201808
(91) Frequency	24 Months
(92) Critical Feature Inspection	Done Freq. (Mon) Date
A: Fracture Critical Detail	No 24
B: Underwater Inspection	Yes 0
C: Other Special Inspection	No 0

SUFFICIENCY RATING	73.6
STATUS (SD/FO/None)	Not Deficient



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Team Lead: Tim Myrick, Inspection Date: July 25, 2016

ELEM	DESCRIPTION	UNITS	TOTAL	CS1	CS2	CS3	CS4
12	Reinforced Concrete Deck	SF	5836	5564	272	0	0
1080	Delamination/Spall/Patched Area	SF	48	0	48	0	0
1130	Cracking (RC and Other)	SF	224	0	224	0	0
107	Steel Open Girder/Beam	LF	900	860	40	0	0
1000	Corrosion	LF	40	0	40	0	0
515	Steel Protective Coating	SF	6738	6521	217	0	0
3440	Effectiveness (Steel Protective Coatings)	SF	217	0	217	0	0
205	Reinforced Concrete Column	EA	6	6	0	0	0
215	Reinforced Concrete Abutment	LF	75	75	0	0	0
234	Reinforced Concrete Pier Cap	LF	98	71	27	0	0
1080	Delamination/Spall/Patched Area	LF	6	0	6	0	0
1090	Exposed Rebar	LF	9	0	9	0	0
1130	Cracking (RC and Other)	LF	12	0	12	0	0
302	Compression Joint Seal	LF	140	140	0	0	0
311	Movable Bearing	EA	20	0	20	0	0
1000	Corrosion	EA	20	0	20	0	0
313	Fixed Bearing	EA	20	0	20	0	0
1000	Corrosion	EA	20	0	20	0	0
333	Other Bridge Railing	LF	364	364	0	0	0





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Location: 2 MI S SMITHVILLE

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Maintenance Needs



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Location: 2 MI S SMITHVILLE

Team Lead: Tim Myrick **Inspection Date:** July 25, 2016

Inspection Comments

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Deck Notes

Deck has insignificant size and density cracks. 4ft. x 12ft. Medium scale in left gutter line Span 3. Compression seal have drop down Bt.1 - 10ft. Bt.3 9ft. Bt. 4 -15ft.

Superstructure Notes

Girders have some minor rust on ends , 1ft. typical. Bearing are rust covered with some section loss.

Substructure Notes

Bent 3 cap Sp.3 side 6 ft. on left end and 9 ft. on right end of cap is delamination. ,spalls with rebar exposed and 12ft horizontal crack 6in. from top of cap.