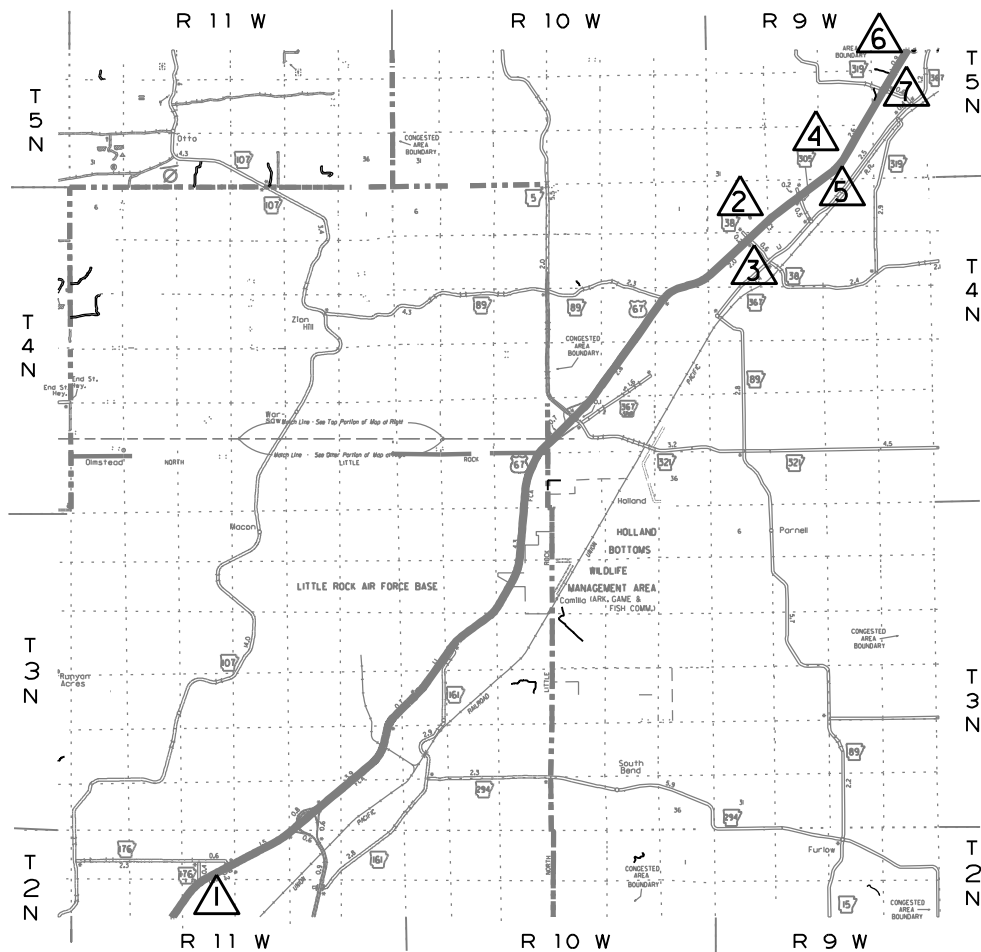


BRIDGE DATA

- ① LOG MILE 3.24 BR. END
218' BRIDGE NO. 06395
124.8' CLEAR ROADWAY
LOG MILE 3.28 BR. END
- ② LOG MILE 20.877 BR. END
122' BRIDGE NO. B5085
42' CLEAR ROADWAY
LOG MILE 20.901 BR. END
- ③ LOG MILE 101.596 BR. END
122' BRIDGE NO. A5085
42.3' CLEAR ROADWAY
LOG MILE 101.620 BR. END
- ④ LOG MILE 23.062 BR. END
122' BRIDGE NO. B5086
42.3' CLEAR ROADWAY
LOG MILE 23.090 BR. END
- ⑤ LOG MILE 99.410 BR. END
122' BRIDGE NO. A5086
42.3' CLEAR ROADWAY
LOG MILE 99.434 BR. END
- ⑥ LOG MILE 25.273 BR. END
120' BRIDGE NO. B5258
42.3' CLEAR ROADWAY
LOG MILE 25.295 BR. END
- ⑦ LOG MILE 97.198 BR. END
120' BRIDGE NO. A5258
42.3' CLEAR ROADWAY
LOG MILE 97.220 BR. END



MID POINT OF PROJECT
 LATITUDE = N 34° 56' 32"
 LONGITUDE = W 92° 04' 19"

ARKANSAS DEPARTMENT OF TRANSPORTATION
 CONSTRUCTION PLANS FOR STATE HIGHWAY

**I-57 GUARDRAIL IMPVTS.
 (LONOKE & PULASKI COS.) (S)**

LONOKE AND PULASKI COUNTIES

ROUTE I-57 SECTIONS 1 & 2

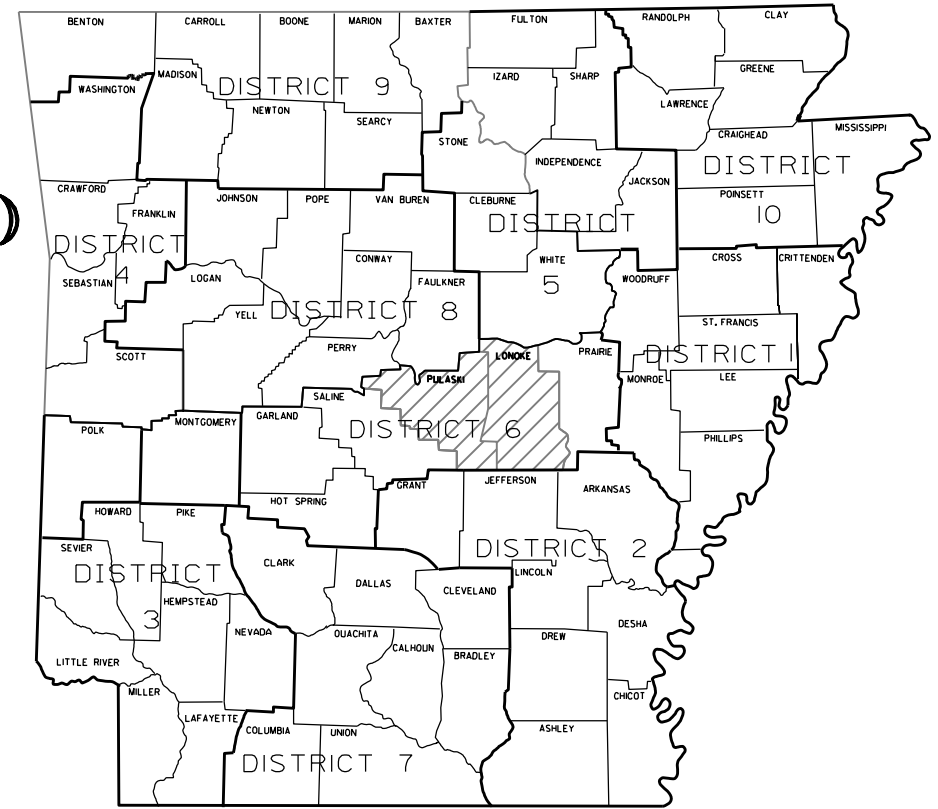
JOB 061915

FED. AID PROJ. HSIP-4360(8)

NET	"	"	ROADWAY	558.00	FEET	OR	0.106 MILES
NET	"	"	BRIDGES	0.00	"	"	0.000 "
NET	"	"	PROJECT	558.00	FEET	OR	0.106 MILES

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	061915		1	12

② I-57 GUARDRAIL IMPVTS. (LONOKE & PULASKI COS.) (S)

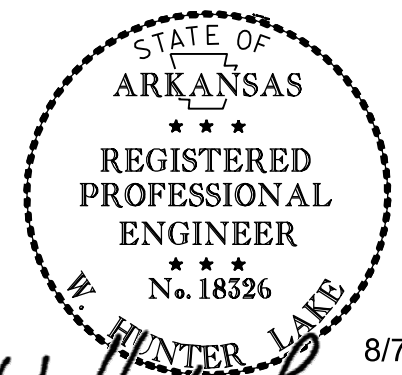


ARK. HWY. DIST. NO. 6

LOG MILE / ANTILOG MILE KEY

KEY	SECTION	BEGIN LM	END LM	DIRECTION
1	1	3.226	3.226	A
2	1	119.185	119.185	B
3	2	20.876	20.876	A
4	2	20.876	20.876	A
5	2	101.594	101.594	B
6	2	101.594	101.594	B
7	2	23.062	23.062	A
8	2	23.062	23.062	A
9	2	99.405	99.405	B
10	2	99.405	99.405	B
11	2	25.267	25.267	A
12	2	25.267	25.267	A
13	2	97.194	97.194	B
14	2	97.194	97.194	B

APPROVED



W. Hunter Lake
 8/7/25
 DISTRICT ENGINEER

INDEX OF SHEETS

SHEET NO.	TITLE
1	TITLE SHEET
2 - 3	INDEX OF SHEETS, STANDARD DRAWINGS, AND GOVERNING SPECIFICATIONS
4	GENERAL NOTES
5 - 7	SPECIAL DETAILS
8 - 11	QUANTITIES
12	SUMMARY OF QUANTITIES AND REVISIONS

BRIDGE STANDARD DRAWINGS

DRWG.NO.	TITLE	DATE
55036	STANDARD DETAILS FOR TYPE 'AT' APPROACH GUTTERS (BRIDGES WITH 6" CURBS & TYPE A, B, C, D OR E RAILING)	11-07-19

ROADWAY STANDARD DRAWINGS

DRWG.NO.	TITLE	DATE
GR-6	GUARDRAIL DETAILS	05-19-22
GR-7	GUARDRAIL DETAILS	11-07-19
GR-8	GUARDRAIL DETAILS	11-07-19
GR-9	GUARDRAIL DETAILS	11-07-19
GR-10	GUARDRAIL DETAILS	11-07-19
GR-11	GUARDRAIL DETAILS	11-07-19
GR-12	GUARDRAIL DETAILS	05-14-20
GRT-1	GUARD RAIL DETAILS	11-07-19
TC-1	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	08-14-25
TC-2	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	08-14-25
TC-3	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	08-14-25
TC-6	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	08-14-25
TEC-1	TEMPORARY EROSION CONTROL DEVICES	11-16-17

GOVERNING SPECIFICATIONS

ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY
CONSTRUCTION, EDITION OF 2014, AND THE FOLLOWING SPECIAL PROVISIONS
AND SUPPLEMENTAL SPECIFICATIONS:

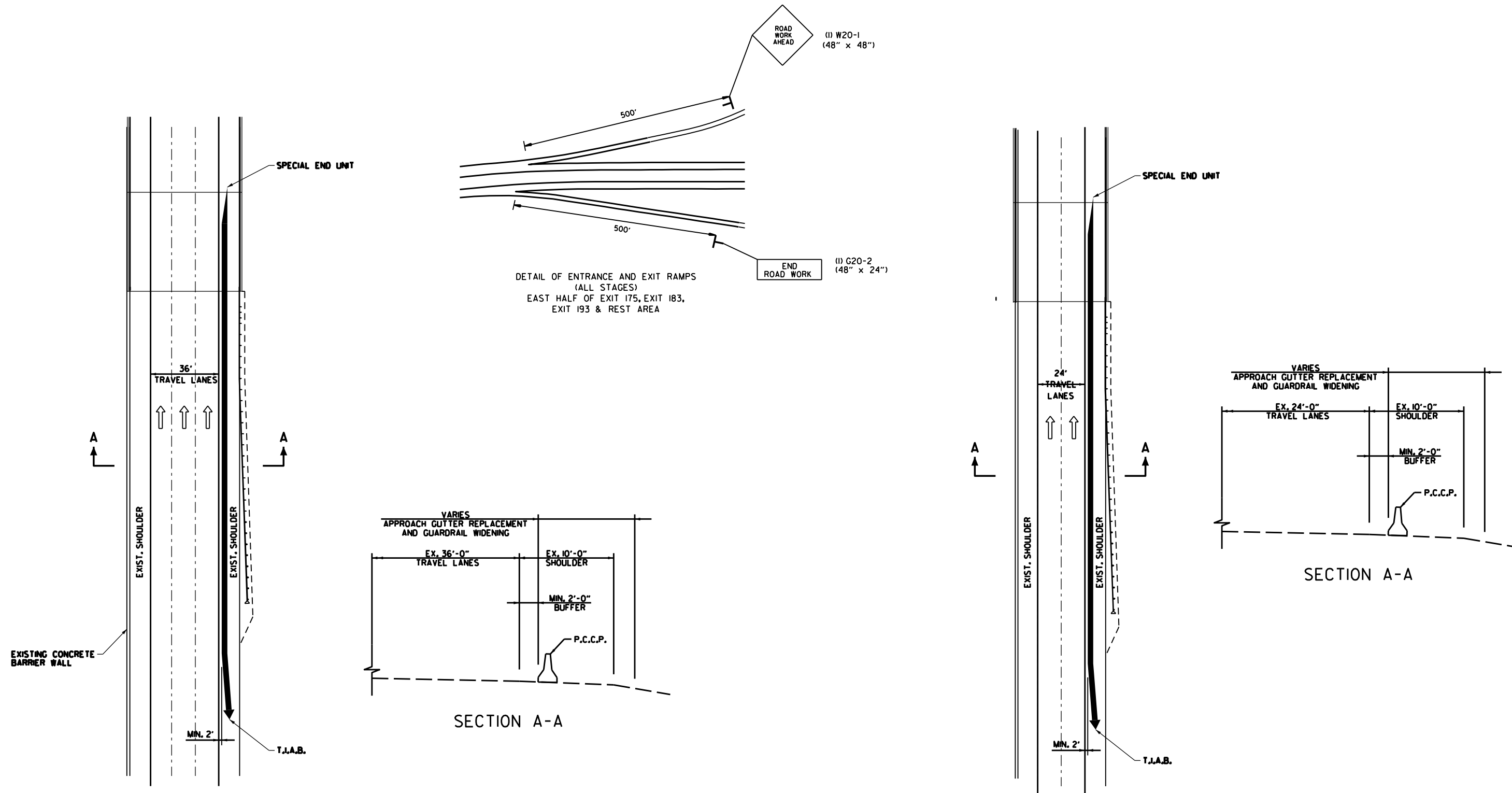
NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
FHWA-1273	REQUIRED CONTRACT PROVISIONS FEDERAL-AID CONSTRUCTION CONTRACTS
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - NOTICE TO CONTRACTORS
FHWA-1273	SUPPLEMENT - SPECIFIC EQUAL EMPLOYMENT OPPORTUNITY RESPONSIBILITIES (23 U.S.C. 140)
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - GOALS AND TIMETABLES
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - FEDERAL STANDARDS
FHWA-1273	SUPPLEMENT - POSTERS AND NOTICES REQUIRED FOR FEDERAL-AID PROJECTS
FHWA-1273	SUPPLEMENT - WAGE RATE DETERMINATION
100-3	CONTRACTOR'S LICENSE
100-4	DEPARTMENT NAME CHANGE
102-2	ISSUANCE OF PROPOSALS
102-3	PREQUALIFICATION OF BIDDERS
103-2	CONTACT INFORMATION FOR MOTORIST DAMAGE CLAIMS
105-4	MAINTENANCE DURING CONSTRUCTION
107-2	RESTRAINING CONDITIONS
108-1	LIQUIDATED DAMAGES
108-2	WORK ALLOWED PRIOR TO ISSUANCE OF WORK ORDER
303-1	AGGREGATE BASE COURSE
306-1	QUALITY CONTROL AND ACCEPTANCE
307-2	CEMENT TREATED BASE COURSE
308-2	CEMENT STABILIZED CRUSHED STONE BASE COURSE
400-4	DESIGN AND QUALITY CONTROL OF ASPHALT MIXTURES
400-5	PERCENT AIR VOIDS FOR ACHM MIX DESIGNS
400-6	LIQUID ANTI-STRIP ADDITIVE
404-3	DESIGN OF ASPHALT MIXTURES
409-2	ASPHALT LABORATORY FACILITY
410-1	CONSTRUCTION REQUIREMENTS AND ACCEPTANCE OF ASPHALT CONCRETE PLANT MIX COURSES
410-2	DEVICES FOR MEASURING DENSITY FOR ROLLING PATTERNS
410-4	EVALUATION OF ACHM SUBLOT REPLACEMENT MATERIAL
416-1	RECYCLED ASPHALT PAVEMENT
501-3	PORTLAND CEMENT CONCRETE PAVEMENT
600-2	INCIDENTAL CONSTRUCTION
603-1	LANE CLOSURE NOTIFICATION
604-1	RETROREFLECTIVE SHEETING FOR TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
604-3	TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES (MASH)
605-1	CONCRETE DITCH PAVING
617-1	GUARDRAIL TERMINAL (TYPE 2)
617-2	GUARDRAIL DELINEATORS
802-5	CONCRETE FOR STRUCTURES
804-2	REINFORCING STEEL FOR STRUCTURES
JOB 061915	ASSESSMENT OF WORKING DAYS - MAINTENANCE OF TRAFFIC
JOB 061915	BIDDING REQUIREMENTS AND CONDITIONS
JOB 061915	BUY AMERICA - CONSTRUCTION MATERIALS
JOB 061915	CARGO PREFERENCE ACT REQUIREMENTS
JOB 061915	CLASS C FLY ASH IN PORTLAND CEMENT CONCRETE PAVEMENT AND CLASS S(AE) CONCRETE
JOB 061915	DESIGN AND QUALITY CONTROL ASPHALT MIXTURES
JOB 061915	DISADVANTAGED BUSINESS ENTERPRISE SET-ASIDE PROJECT
JOB 061915	GOALS FOR DISADVANTAGED BUSINESS ENTERPRISE PARTICIPATION
JOB 061915	LIQUIDATED DAMAGES PROCEDURE FOR BID LETTINGS
JOB 061915	MAINTENANCE OF TRAFFIC
JOB 061915	MANDATORY ELECTRONIC CONTRACT
JOB 061915	MANDATORY ELECTRONIC DOCUMENT SUBMITTAL
JOB 061915	MOBILE SPEED NOTIFICATION SYSTEM (SPECIAL)
JOB 061915	OFF-SITE RESTRAINING CONDITIONS FOR INDIANA AND NORTHERN LONG-EARED BATS
JOB 061915	PRICE ADJUSTMENT FOR ASPHALT BINDER
JOB 061915	PRICE ADJUSTMENT FOR FUEL
JOB 061915	PROHIBITION OF CERTAIN TELECOMMUNICATIONS AND VIDEO SURVEILLANCE SERVICES OR EQUIPMENT
JOB 061915	SEQUENCE OF CONSTRUCTION
JOB 061915	SUBMISSION OF ASPHALT CONCRETE HOT MIX ACCEPTANCE TEST RESULTS
JOB 061915	TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
JOB 061915	UTILITY ADJUSTMENTS
JOB 061915	WARM MIX ASPHALT
JOB 061915	WORK ZONE LAW ENFORCEMENT

JOB NO. 061915

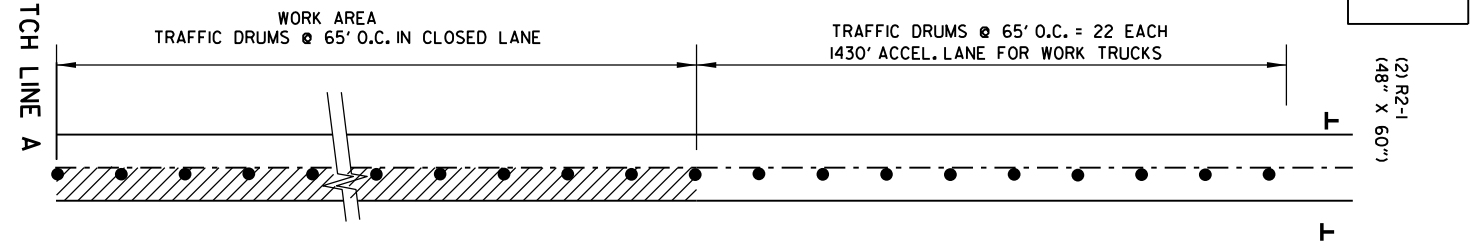
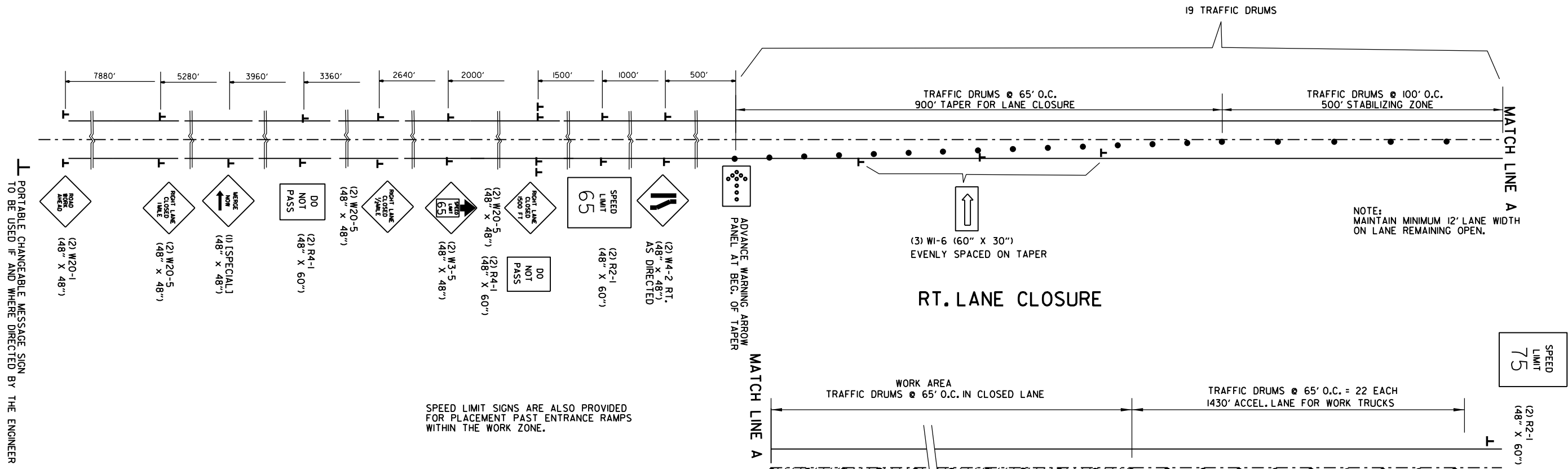
REVISED 08-29-2025
SHEET 3 OF 12
GOVERNING SPECIFICATIONS

GENERAL NOTES

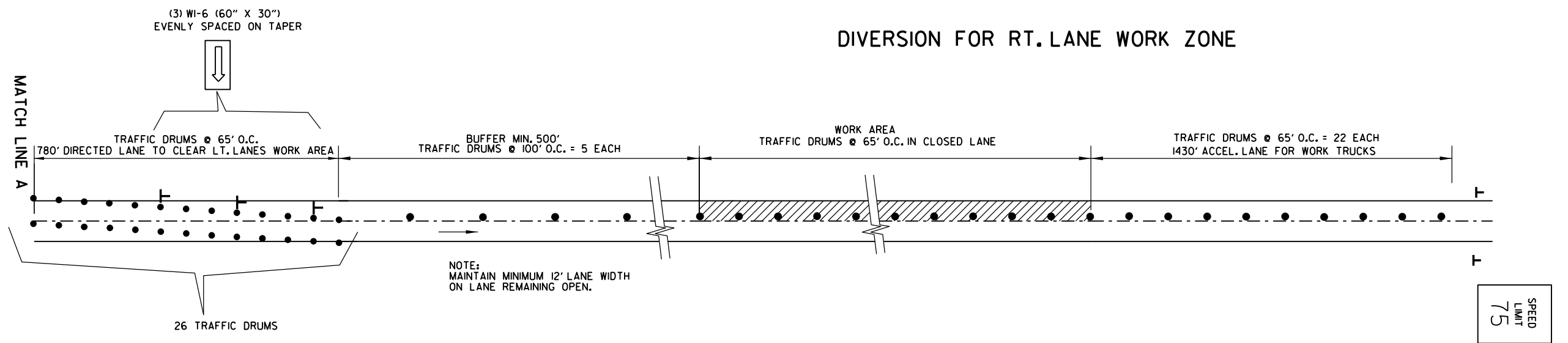
1. ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
2. ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
3. ALL TREES THAT DO NOT DIRECTLY INTERFERE WITH THE PROPOSED CONSTRUCTION SHALL BE SPARED AS DIRECTED BY THE ENGINEER. CARE AND DISCRETION SHALL BE USED TO INSURE THAT ALL TREES NOT TO BE REMOVED SHALL BE HARMED AS LITTLE AS POSSIBLE DURING THE CONSTRUCTION OPERATIONS.
4. THE EXISTING ASPHALT PAVEMENT TO BE REMOVED FROM THE REMAINING PAVEMENT SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT THAT IS TO REMAIN. ANY DAMAGE OF THE ASPHALT PAVEMENT THAT IS TO REMAIN IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
5. THIS PROJECT IS COVERED UNDER A NATIONWIDE ## SECTION 404 PERMIT. REFER TO SECTION 110 OF THE STANDARD SPECIFICATIONS, EDITION OF 2014, FOR PERMIT REQUIREMENTS.
6. STRINGLINE WILL BE USED TO MAINTAIN A UNIFORM HORIZONTAL ALIGNMENT.
7. AGGREGATE BASE COURSE OUTSIDE THE EXISTING SHOULDERS SHALL BE UNIFORMLY COMPACTED, STABLE, AND FREE OF SEGREGATION. THE DENSITY REQUIREMENTS OF SECTION 303 ARE HEREBY WAIVED.



DETAIL FOR APPROACH GUTTER REPLACEMENT AND GUARDRAIL WIDENING



DIVERSION FOR RT. LANE WORK ZONE



DIVERSION FOR LT. LANE WORK ZONE

PORTABLE CHANGEABLE MESSAGE SIGN TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER

ADVANCE WARNING SIGNS AND DEVICES

SIGN NUMBER	DESCRIPTION	SIGN SIZE	TOTAL SIGNS REQUIRED		*ADVANCED WARNING ARROW PANEL	TRAFFIC DRUMS	*PORTABLE CHANGEABLE MESSAGE SIGN	MOBILE SPEED NOTIFICATION SYSTEM (SPECIAL)	*FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER	*RELOCATING PRECAST CONCRETE BARRIER	*TEMPORARY IMPACT ATTENUATION BARRIER	*TEMPORARY IMPACT ATTENUATION BARRIER (REPAIR)	*TEMPORARY IMPACT ATTENUATION BARRIER (RELOCATION)
			NO.	SQ. FT.	DAYS	EACH	WEEK	EACH	LIN. FT.	LIN. FT.	EACH		
W20-1	ROAD WORK 1500 FT.	48"x48"	8	128.0									
W20-1	ROAD WORK AHEAD	48"x48"	10	160.0									
W20-5	RIGHT LANE CLOSED 1 MILE	48"x48"	8	128.0									
W20-5	RIGHT LANE CLOSED 1/2 MILE	48"x48"	8	128.0									
R2-1	RIGHT LANE CLOSED 1500 FT	48"x48"	8	128.0									
W20-5	ROAD WORK 1 MILE	48"x48"	8	128.0									
G20-2	END ROAD WORK	48"x24"	10	80.0									
G20-1	ROAD WORK NEXT 5 MILES	60"x24"	4	40.0									
W1-6	ARROW	60"x30"	12	150.0									
R4-1	DO NOT PASS	48"x60"	8	160.0									
R2-6aP	FINES DOUBLE IN WORK ZONES	48"x36"	8	96.0									
SPECIAL	MERGE NOW	48"x48"	4	64.0									
W3-5	SPEED LIMIT 55	48"x48"	8	128.0									
R2-1	SPEED LIMIT 55	48"x48"	8	128.0									
R2-1	SPEED LIMIT 65	48"x48"	8	128.0									
W4-2	MERGE	48"x48"	8	128.0									
WZ-1	PHOTO ENFORCED	84"x60"	8	280.0									
R2-11	END HIGHER FINES ZONE	36"x48"	8	96.00									
W21-5a	SHOULDER CLOSED	48"x48"	4	64.00									
	FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER								3704				
	RELOCATING PRECAST CONCRETE BARRIER									2804			
	TEMPORARY IMPACT ATTENUATION BARRIER										6		
	TEMPORARY IMPACT ATTENUATION BARRIER (REPAIR)											4	
	TEMPORARY IMPACT ATTENUATION BARRIER (RELOCATION)												8
	ADVANCED WARNING ARROW PANEL		4		120								
	TRAFFIC DRUMS					2213							
	PORTABLE CHANGEABLE MESSAGE SIGN						24						
	MOBILE SPEED NOTIFICATION SYSTEM (SPECIAL)							4					
TOTALS:			2342.0		120	2213	24	4	3704	2804	6	4	8

NOTE: THIS IS A HIGH TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

*QUANTITY ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.
TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.

BASE AND SURFACING

LOG MILE	LOG MILE	LOCATION	LENGTH	AGGREGATE BASE COURSE (CLASS 7)		ACHM SURFACE COURSE (1/2")			
				TON / STATION	TON	AVG. WID.	SQ.YD.	POUND / SQ.YD.	PG 76-22
			FEET		FEET		TON		
MAIN LANES									
3.226 (A)	3.226 (A)	RT. SIDE OF RT. MAIN LANES	243	45.00	109.35	5.50	148.50	220.00	16.34
119.185 (B)	119.185 (B)	LT. SIDE OF LT. MAIN LANES	243	45.00	109.35	5.50	148.50	220.00	16.34
20.876 (A)	20.876 (A)	LT. SIDE OF RT. MAIN LANES	243	45.00	109.35	5.50	148.50	220.00	16.34
20.876 (A)	20.876 (A)	RT. SIDE OF RT. MAIN LANES	243	45.00	109.35	5.50	148.50	220.00	16.34
101.594 (B)	101.594 (B)	LT. SIDE OF LT. MAIN LANES	243	45.00	109.35	5.50	148.50	220.00	16.34
101.594 (B)	101.594 (B)	RT. SIDE OF LT. MAIN LANES	243	45.00	109.35	5.50	148.50	220.00	16.34
23.062 (A)	23.062 (A)	LT. SIDE OF RT. MAIN LANES	243	45.00	109.35	5.50	148.50	220.00	16.34
23.062 (A)	23.062 (A)	RT. SIDE OF RT. MAIN LANES	243	45.00	109.35	5.50	148.50	220.00	16.34
99.405 (B)	99.405 (B)	LT. SIDE OF LT. MAIN LANES	243	45.00	109.35	5.50	148.50	220.00	16.34
99.405 (B)	99.405 (B)	RT. SIDE OF LT. MAIN LANES	243	45.00	109.35	5.50	148.50	220.00	16.34
25.267 (A)	25.267 (A)	LT. SIDE OF RT. MAIN LANES	243	45.00	109.35	5.50	148.50	220.00	16.34
25.267 (A)	25.267 (A)	RT. SIDE OF RT. MAIN LANES	243	45.00	109.35	5.50	148.50	220.00	16.34
97.194 (B)	97.194 (B)	LT. SIDE OF LT. MAIN LANES	243	45.00	109.35	5.50	148.50	220.00	16.34
97.194 (B)	97.194 (B)	RT. SIDE OF LT. MAIN LANES	243	45.00	109.35	5.50	148.50	220.00	16.34
TOTALS:					1530.90		2079.00		228.76

BASIS OF ESTIMATE:
 ACHM SURFACE COURSE (1/2").....94.3% MIN. AGGR.....5.7% ASPHALT BINDER

ACHM PATCHING OF EXISTING ROADWAY

DESCRIPTION	TON
ENTIRE PROJECT - TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER	50
TOTAL:	50

NOTE: QUANTITY ESTIMATED.
 SEE SECTION 104.03 OF THE STD. SPECS.

PORTLAND CEMENT CONCRETE PAVEMENT PATCHING

DESCRIPTION	SY
ENTIRE PROJECT - TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER	500
TOTAL	500

NOTE: QUANTITY ESTIMATED.
 SEE SECTION 104.03 OF THE STD. SPECS.

EROSION CONTROL

LOG MILE	LOG MILE	LOCATION	TEMPORARY EROSION CONTROL	
			*WATTLE (20")	
			LIN. FT.	
3.226 (A)	3.226 (A)	I-57 SEC. 1	300.00	
119.185 (B)	119.185 (B)	I-57 SEC. 1	300.00	
20.876 (A)	20.876 (A)	I-57 SEC. 2	300.00	
20.876 (A)	20.876 (A)	I-57 SEC. 2	300.00	
101.594 (B)	101.594 (B)	I-57 SEC. 2	300.00	
101.594 (B)	101.594 (B)	I-57 SEC. 2	300.00	
23.062 (A)	23.062 (A)	I-57 SEC. 2	300.00	
23.062 (A)	23.062 (A)	I-57 SEC. 2	300.00	
99.405 (B)	99.405 (B)	I-57 SEC. 2	300.00	
99.405 (B)	99.405 (B)	I-57 SEC. 2	300.00	
25.267 (A)	25.267 (A)	I-57 SEC. 2	300.00	
25.267 (A)	25.267 (A)	I-57 SEC. 2	300.00	
97.194 (B)	97.194 (B)	I-57 SEC. 2	300.00	
97.194 (B)	97.194 (B)	I-57 SEC. 2	300.00	
TOTAL:			4200.00	

NOTE: THE TEMPOARY EROSION CONTROL DEVICES SHOWN ABOVE AND ON THE PLANS SHALL BE INSTALLED IN SUCH A SEQUENCE AS TO DETER EROSION AND SEDIMENTATION ON U.S. WATERWAYS AS EXPLAINED BY THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM PERMIT.

*QUANTITIES ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.
TO BE USED IF AND WHERE DIRECTED
BY THE ENGINEER

GUARDRAIL

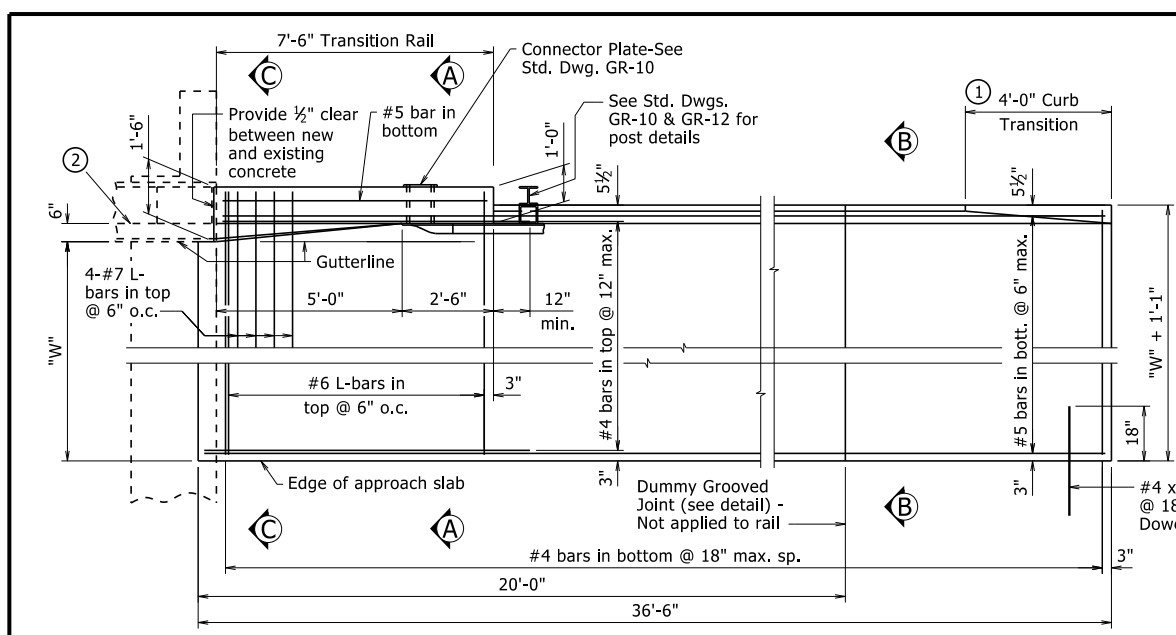
LOG MILE	LOG MILE	LOCATION	GUARDRAIL (TYPE A)	THRIE BEAM GUARDRAIL TERMINAL	GUARDRAIL TERMINAL (TYPE 2)
			LIN. FT.	EACH	EACH
3.226 (A)	3.226 (A)	RT. SIDE OF RT. MAIN LANES	150	1	1
119.185 (B)	119.185 (B)	LT. SIDE OF LT. MAIN LANES	150	1	1
20.876 (A)	20.876 (A)	LT. SIDE OF RT. MAIN LANES	150	1	1
20.876 (A)	20.876 (A)	RT. SIDE OF RT. MAIN LANES	150	1	1
101.594 (B)	101.594 (B)	LT. SIDE OF LT. MAIN LANES	150	1	1
101.594 (B)	101.594 (B)	RT. SIDE OF LT. MAIN LANES	150	1	1
23.062 (A)	23.062 (A)	LT. SIDE OF RT. MAIN LANES	150	1	1
23.062 (A)	23.062 (A)	RT. SIDE OF RT. MAIN LANES	150	1	1
99.405 (B)	99.405 (B)	LT. SIDE OF LT. MAIN LANES	150	1	1
99.405 (B)	99.405 (B)	RT. SIDE OF LT. MAIN LANES	150	1	1
25.267 (A)	25.267 (A)	LT. SIDE OF RT. MAIN LANES	150	1	1
25.267 (A)	25.267 (A)	RT. SIDE OF RT. MAIN LANES	150	1	1
97.194 (B)	97.194 (B)	LT. SIDE OF LT. MAIN LANES	150	1	1
97.194 (B)	97.194 (B)	RT. SIDE OF LT. MAIN LANES	150	1	1
TOTALS:			2100	14	14

SUMMARY OF QUANTITIES

ITEM NUMBER	ITEM	QUANTITY	UNIT
202	REMOVAL AND DISPOSAL OF APPROACH GUTTERS	8	EACH
202	REMOVAL AND DISPOSAL OF GUARDRAIL	2050	LIN. FT.
SP & 215	TRENCHING AND SHOULDER PREPARATION	36	STATION
SP, SS, & 303	AGGREGATE BASE COURSE (CLASS 7)	1531	TON
SP, SS, & 407	MINERAL AGGREGATE IN ACHM SURFACE COURSE (1/2")	216	TON
SP, SS, & 407	ASPHALT BINDER (PG 76-22) IN ACHM SURFACE COURSE (1/2")	13	TON
SP, SS, & 415	ACHM PATCHING OF EXISTING ROADWAY	50	TON
SP, SS, & 504	APPROACH GUTTERS	154.40	CU. YD.
SP, SS, & 507	PORTLAND CEMENT CONCRETE PAVEMENT PATCHING (12" UNIFORM THICKNESS)	500.00	SQ. YD.
601	MOBILIZATION	1.00	LUMP SUM
SP, SS, & 603	MAINTENANCE OF TRAFFIC	1.00	LUMP SUM
SS & 604	SIGNS	2342	SQ. FT.
SS & 604	TRAFFIC DRUMS	2213	EACH
SS & 604	FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER	3704	LIN. FT.
SS & 604	RELOCATING PRECAST CONCRETE BARRIER	2804	LIN. FT.
SP	MOBILE SPEED NOTIFICATION SYSTEM (SPECIAL)	4	EACH
SS & 604	ADVANCE WARNING ARROW PANEL	120	DAY
SP, SS, & 604	PORTABLE CHANGEABLE MESSAGE SIGN	24	WEEK
SS & 617	GUARDRAIL (TYPE A)	2100	LIN. FT.
SS & 617	GUARDRAIL TERMINAL (TYPE 2)	14	EACH
SS & 617	THRIE BEAM GUARDRAIL TERMINAL	14	EACH
621	WATTLE (20")	4200	LIN. FT.
731	TEMPORARY IMPACT ATTENUATION BARRIER	6	EACH
731	TEMPORARY IMPACT ATTENUATION BARRIER (REPAIR)	4	EACH
731	TEMPORARY IMPACT ATTENUATION BARRIER (RELOCATION)	8	EACH
SS & 804	REINFORCING STEEL-ROADWAY (GRADE 60)	12796	POUND

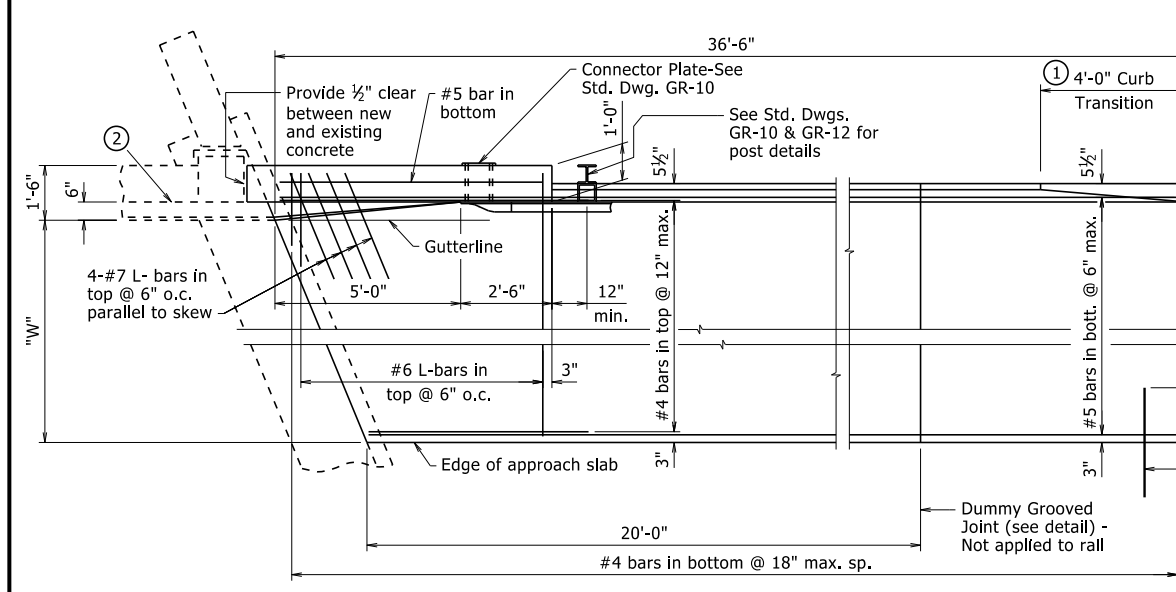
DATE	REVISION	SHEET NUMBER
8/29/2025	Added Work Zone Law Enforcement SP	3, 12

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
11/7/19				6	ARK.			
				JOB NO.		- TYPE AT GUTTERS - 55036		

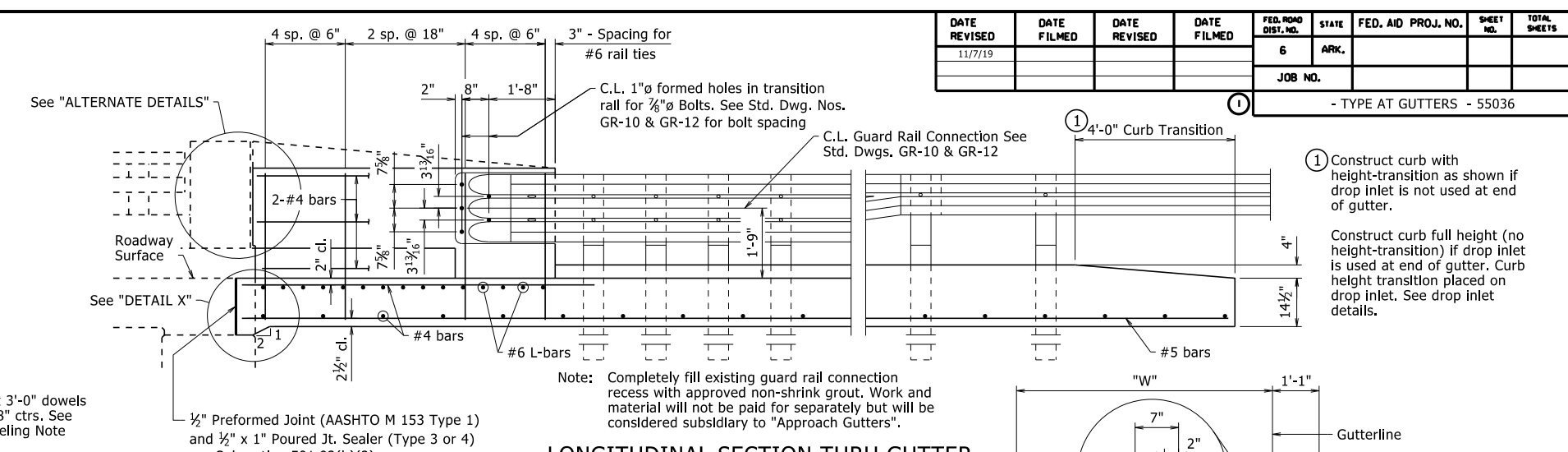


PLAN - SQUARE BRIDGES
3/8" = 1'-0"

② Front face of concrete wall (Type A Rail) or front face of metal pipe or tubing (Types B, C, D or E Rail).

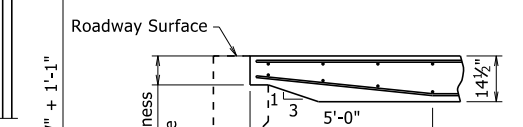


PLAN - SKEWED BRIDGES
3/8" = 1'-0"

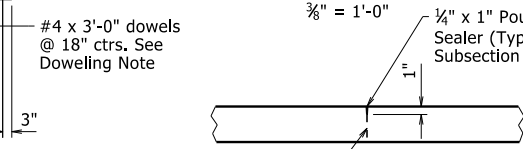


LONGITUDINAL SECTION THRU GUTTER
1/2" = 1'-0"

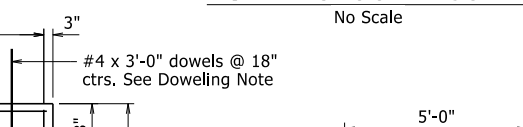
DOWELING NOTES
If new approach slab is used: Place dowels into approach slab using 18" embedment.
If existing approach slab is retained: Dowels shall be drilled and grouted 18" into existing slab. At the Contractor's option, existing dowels may be retained, cleaned and incorporated into new gutters. Work for drilling and grouting, or retaining and cleaning will not be paid for separately but will be considered subsidiary to "Approach Gutters".
Dowel bars, if required, will not be paid for separately, but will be considered subsidiary to other pay items.



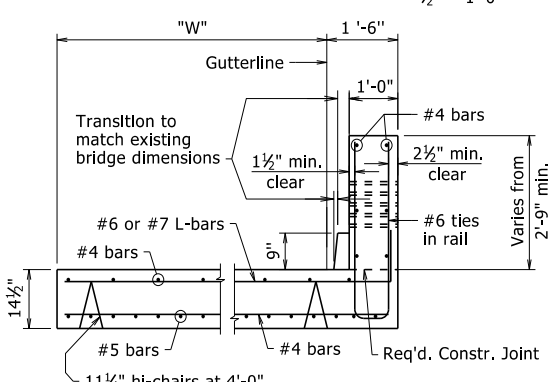
DETAIL X
3/8" = 1'-0"



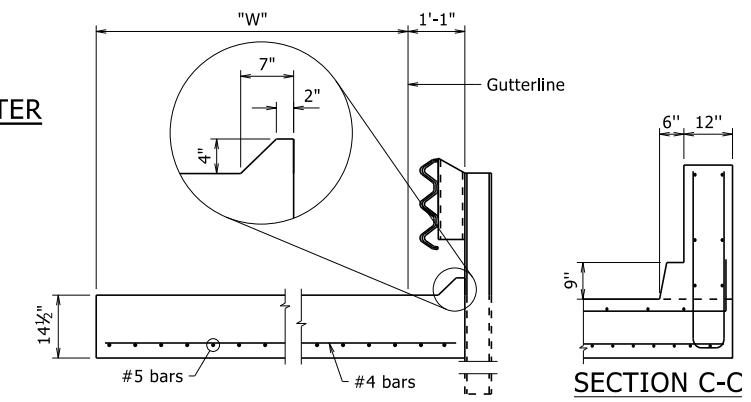
DUMMY GROOVED JOINT
No Scale



ALTERNATE DETAILS
NO SCALE

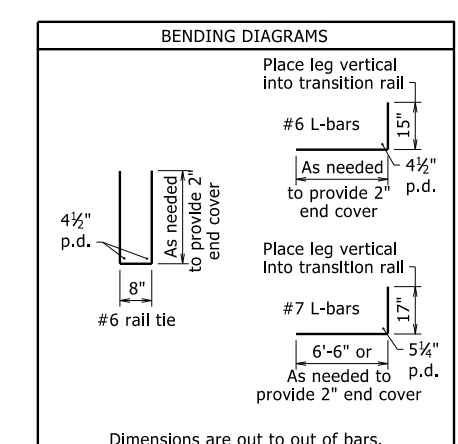


SECTION A-A
1/2" = 1'-0"

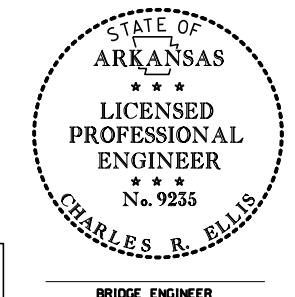


SECTION B-B
1/2" = 1'-0"

SECTION C-C
At End of Transition Rail
1/2" = 1'-0"



Revised and Redrawn. By: TMG
Checked By: CRE 11/7/2019



GENERAL NOTES
Concrete shall be Class S or S(AE) or mixture used for Portland Cement Concrete Pavement.
Reinforcing steel shall be Grade 60 (fy = 60,000 psi.) conforming to AASHTO M 31 or M 322, Type A, with mill test reports. Fabricate bar lengths to provide 2" minimum cover at each end.
Approach gutters will be measured and paid for in accordance with Section 504.
Preformed Joint and Poured Joint Sealer included in the item "Approach Gutters".
All longitudinal lines within the limits of horizontal curves shall be on curves concentric to C.L. Bridge. Adjustment to longitudinal bar lengths may be required. Transverse reinforcing shall be placed on radial lines to C.L. Bridge.
When this Standard Drawing is used as a retrofit for an existing bridge and an existing drop inlet is located within the Plan of the approach gutter, adjust the reinforcing as needed to facilitate construction of the approach gutter, unless otherwise noted.

APPROX. QUANTITIES FOR ONE SQUARE 36'-6" APPROACH GUTTER
(For Information Only)

Concrete (cu. yd.)	("W" x 1.65) + 2.80
Reinforcing Steel (lb.)	("W" x 128.1) + 318.5

Variables: Units of "W" are in feet.

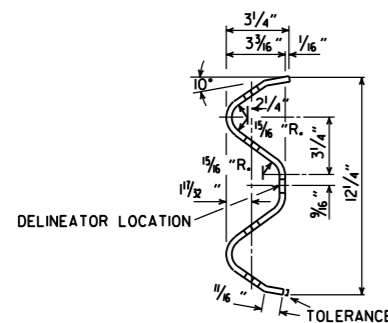
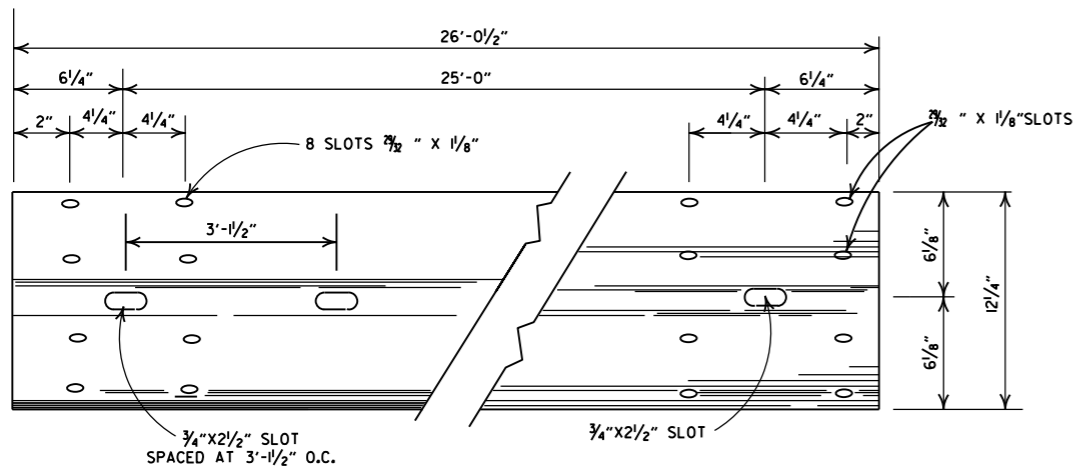
"W" = Distance from gutterline to edge of shoulder or edge of approach slab. "W" shall not be less than 3'-0" unless approach gutter is doweled into an approach slab or concrete pavement.

STANDARD DETAILS FOR TYPE 'A' APPROACH GUTTERS (BRIDGES WITH 6" CURBS & TYPE A, B, C, D OR E RAILING)
ROUTE SEC.
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

DRAWN BY: KDH DATE: 2/27/2014 FILENAME: b55036.dgn
CHECKED BY: KWY DATE: 2/27/2014 SCALE: AS NOTED
DESIGNED BY: STD. DATE: -
DRAWING NO. 55036

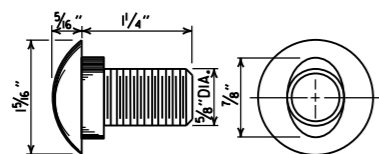
This document was originally issued and sealed by Charles R. Ellis, PE No. 9235, on November 7, 2019. This copy is not a signed and sealed document.

PRINT DATE: 11/20/2019

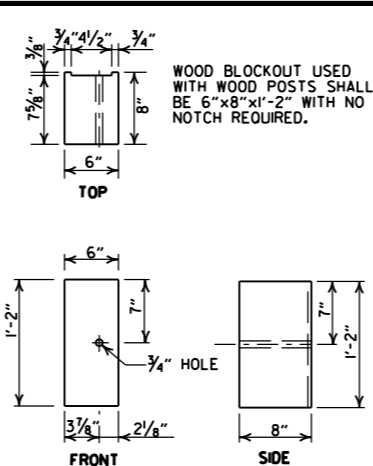
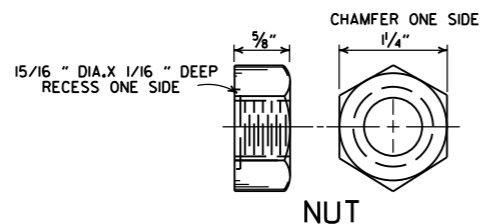
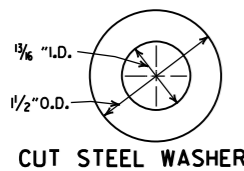


DETAILS OF W-BEAM GUARDRAIL

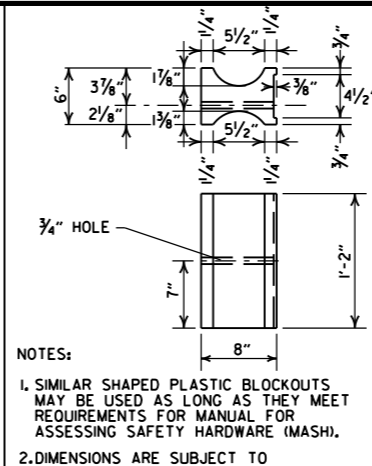
RAIL SECTION OF CLOSELY SIMILAR DIMENSIONS AND COMPARABLE STRENGTH MAY BE SUBSTITUTED IF APPROVED BY THE ENGINEER.



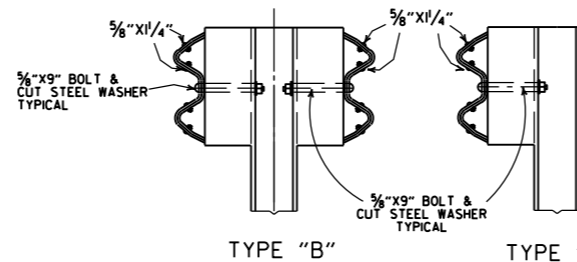
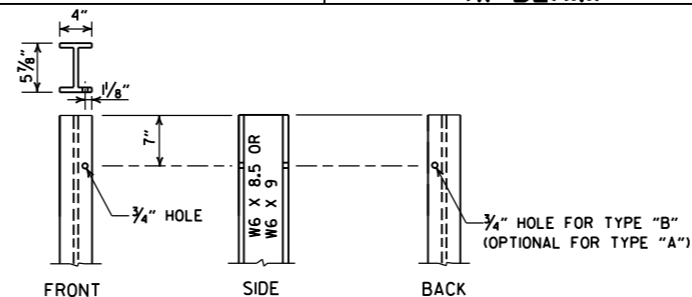
**SPLICE BOLT
POST BOLT - SAME EXCEPT LENGTH**



WOOD BLOCKOUT (W-BEAM)



NOTES:
1. SIMILAR SHAPED PLASTIC BLOCKOUTS MAY BE USED AS LONG AS THEY MEET REQUIREMENTS FOR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).
2. DIMENSIONS ARE SUBJECT TO MANUFACTURERS TOLERANCES.



DETAILS OF STEEL LINE POST CONNECTIONS (W-BEAM)

-GENERAL NOTES-

ALL BOLTS SHALL BE SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND NO MORE THAN 3/4" BEYOND IT.

WHERE W-BEAM GUARDRAIL CONTINUES, THE INTERMEDIATE SECTIONS SHALL HAVE A POST SPACING OF 6'-3" UNLESS OTHERWISE NOTED.

W-BEAM GUARDRAIL REPRESENTING INTERMEDIATE SECTIONS WILL BE MEASURED ALONG THE ROADWAY FACE FROM CENTERLINE OF POST TO CENTERLINE OF POST.

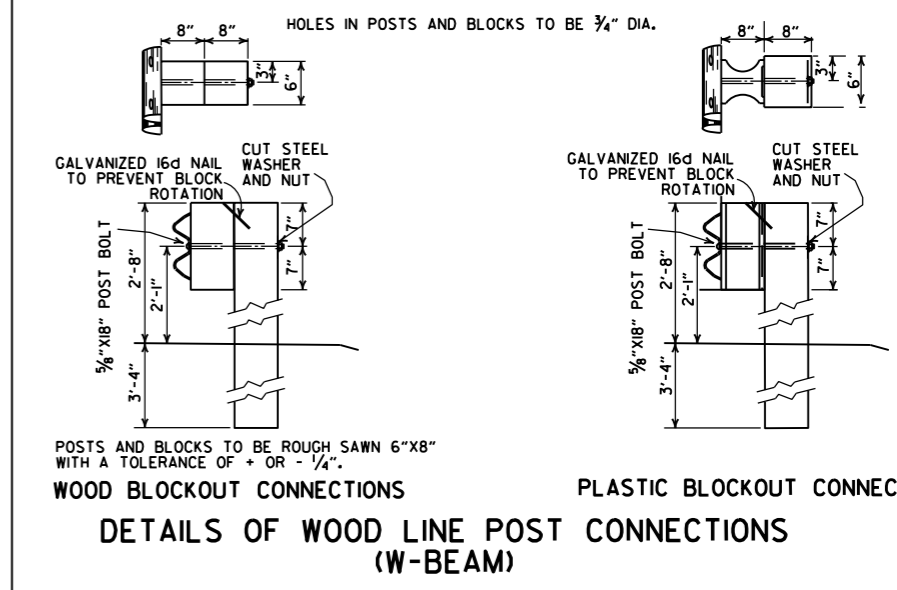
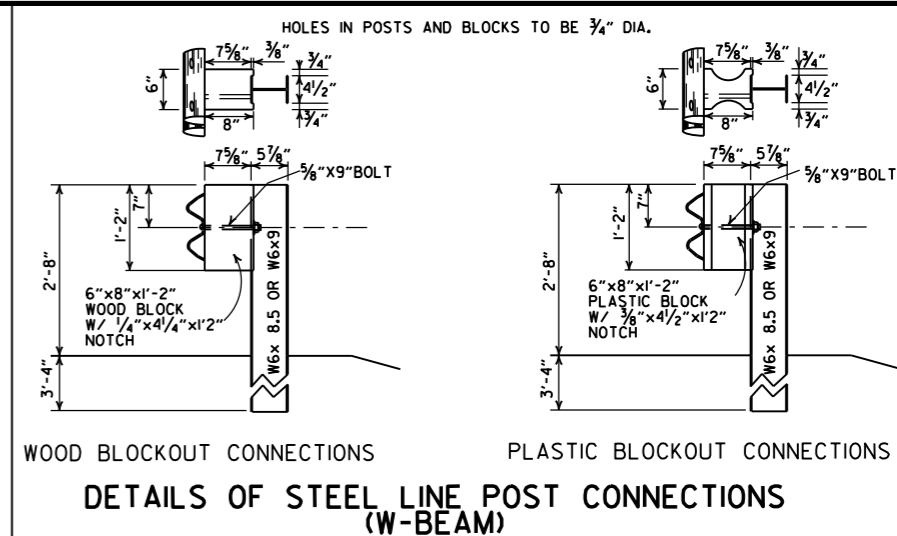
USE W-BEAM GUARDRAIL COMPONENTS OF SAME MATERIAL FOR ENTIRE JOB. FOR EXTENSIONS OR MODIFICATION OF EXISTING GUARDRAIL, W-BEAM GUARDRAIL COMPONENTS OF THE SAME TYPE AS THOSE EXISTING SHALL BE USED.

ANY BACKFILLING UNDER OR AROUND POST SHALL BE DAMP SAND THOROUGHLY TAMPED IN PLACE.

WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9.7f (1400 f) OR NO. 1 1350 f SOUTHERN PINE.

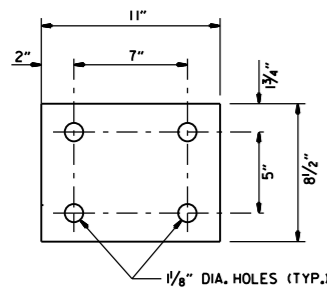
CONTRACTOR SHALL HAVE THE OPTION OF USING WOOD BLOCKOUTS FOR W-BEAM GUARDRAIL OR PLASTIC BLOCKOUTS, AS LONG AS BLOCKOUT USED MEETS REQUIREMENTS FOR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) FOR W-BEAM GUARDRAIL.

DELINEATORS SHALL BE MOUNTED AT 37.5' SPACING ON THE FRONT FACE OF THE GUARDRAIL. SPACING MAY BE REDUCED IN CURVES, AS DIRECTED BY THE ENGINEER. COLOR SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR DELINEATORS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID PER LIN. FT. FOR GUARDRAIL.

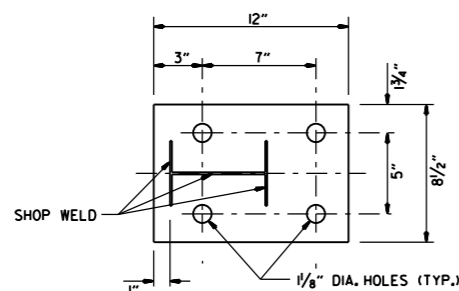


05-19-22	REVISED GENERAL NOTES, ADDED DELINEATOR LOCATION.	
11-07-19	RENUMBERED AND RENAMED	
11-16-17	REVISED GENERAL NOTES AND RAISED GUARDRAIL HEIGHT 3"	
07-14-10	RAISED HEIGHT OF GUARDRAIL 1"	
10-15-09	ADDED REFERENCE TO MASH	
04-10-03	REVISED GENERAL NOTES	
08-22-02	REVISED DIMENSION ON WOOD & PLASTIC BLOCKOUT CONNECTIONS & STEEL POST	
11-16-01	REVISED WOOD BLOCKOUT & DETAILS OF WOOD LINE POST CONNECTIONS	
03-30-00	REMOVED GUARDRAIL AT BRIDGE ENDS	
01-12-00	ADDED PLASTIC BLOCKOUT	
08-12-98	REV. BLOCKOUTS TO WOOD, DELETED CONC. POST & REV. GENERAL NOTE, DELETED DET. OF GUARDRAIL REPLACE. BEHIND CURB & DET. OF POST PLACE. IN SOLID ROCK, & ADDED DETAILS OF STEEL LINE POST CONC. REMOVED BACK-UP PLATE, REVISED HOLES IN STEEL POLES	
04-03-97	REMOVED "LAP IN DIRECTION OF TRAFFIC" NOTE & PLACED ARROWS ON WASHERS	
10-18-96	REVISED WOOD POST NOTE	
06-02-94	ADDED ALT. STEEL POST SIZE	
08-05-93	REVISED STEEL POST SIZE	8-5-93
10-01-92	REDRAWN & REVISED	10-1-92
08-15-91	REVISED WASHER NOTE	8-15-91
08-02-90	REV. GEN. NOTE & DEPTH OF ANC. POST IN ROCK	8-2-90
07-15-88	REVISED SECTION 3 & GENERAL NOTES	
03-04-88	REV. ANCHOR POST ELEV. NOTES & POST IN ROCK	780-3-4-88
10-30-87	REVISED WOOD LINE POST DETAIL	546-10-30-87
10-09-87	REDRAWN & REVISED	802-10-9-87
DATE	REVISION	FILMED

ARKANSAS STATE HIGHWAY COMMISSION
GUARDRAIL DETAILS
STANDARD DRAWING GR-6

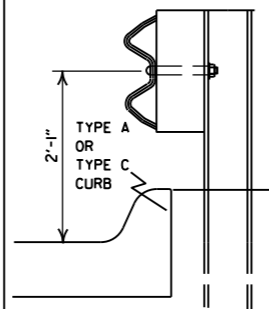


WASHER PLATE



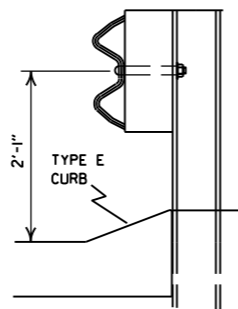
BASE PLATE

Note: Bolts, nuts, washers and plates shall be galvanized in accordance with Section 807 of the Standard Specifications.



FOR DESIGN SPEEDS OF 50 MPH OR LESS

ALIGN FACE OF GUARDRAIL WITH FACE OF CURB.

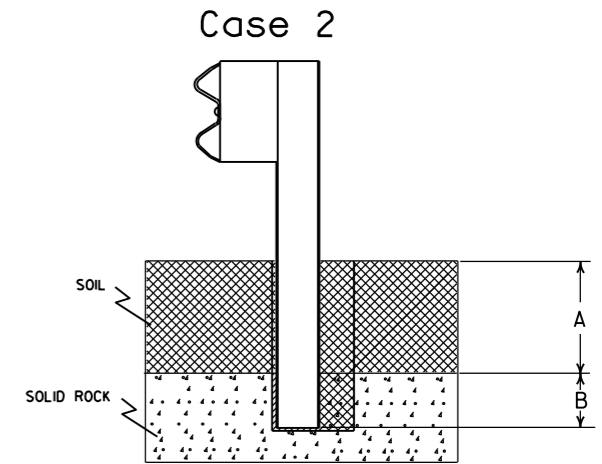
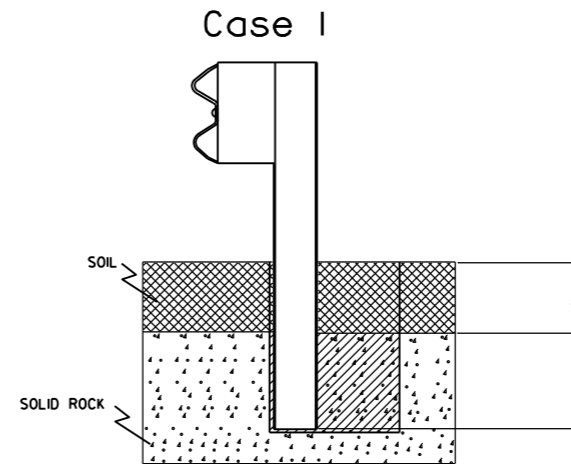


FOR DESIGN SPEEDS OF 55 MPH OR MORE

PLACE GUARDRAIL POSTS AGAINST BACK OF CURB.

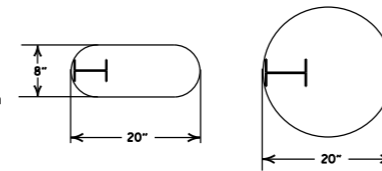
DETAIL OF GUARDRAIL PLACEMENT BEHIND CURB (W-BEAM)

FOR DESIGN SPEEDS OF 50 MPH OR LESS ALL CURB FACES, AS SHOWN ON STD. DRWG. CG-1, MAY BE USED. FOR DESIGN SPEEDS OF 55 MPH OR MORE TYPE "E" CURB FACE SHALL BE USED.



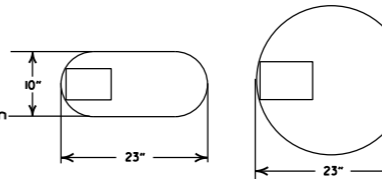
Plan View Steel Posts

Either hole configuration acceptable



Plan View Wood Posts

Either hole configuration acceptable



Notes: For overlying soil depths (A) ranging from 0 to 18", the depth of required drilling (B) is equal to 24".

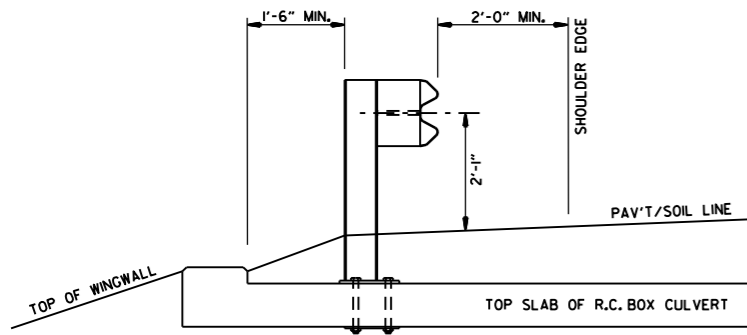
Zone A: Backfill according to Section 617.03(a).

Zone B: Backfill hole in 6" lifts with material meeting the requirements of Section 802.02(c) - Alternate gradation. Compact to 95% maximum dry density per ASTM D-698.

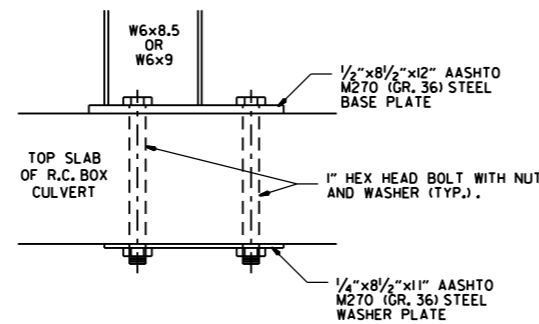
Notes: For overlying soil depths (A) ranging from 18" to 44", the depth of required drilling (B) is equal to either 12" or 44" minus the depth of soil whichever is less.

Zone A & B: Backfill according to Section 617.03(a).

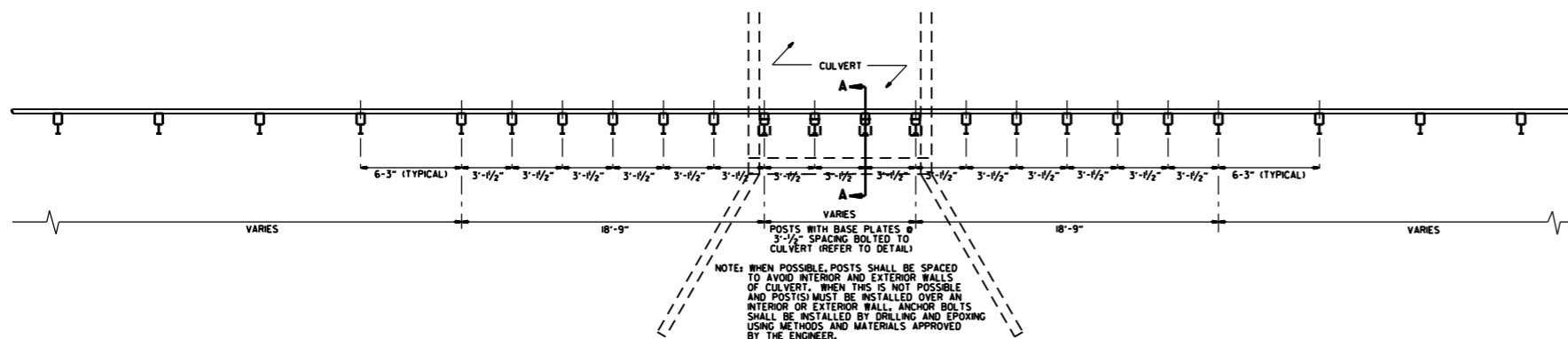
DETAIL OF POST PLACEMENT IN SOLID ROCK (W-BEAM)



SECTION A-A



DETAIL OF CONNECTION



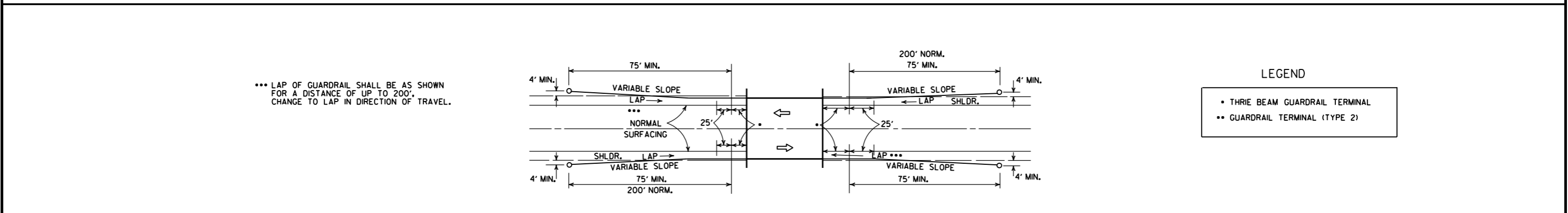
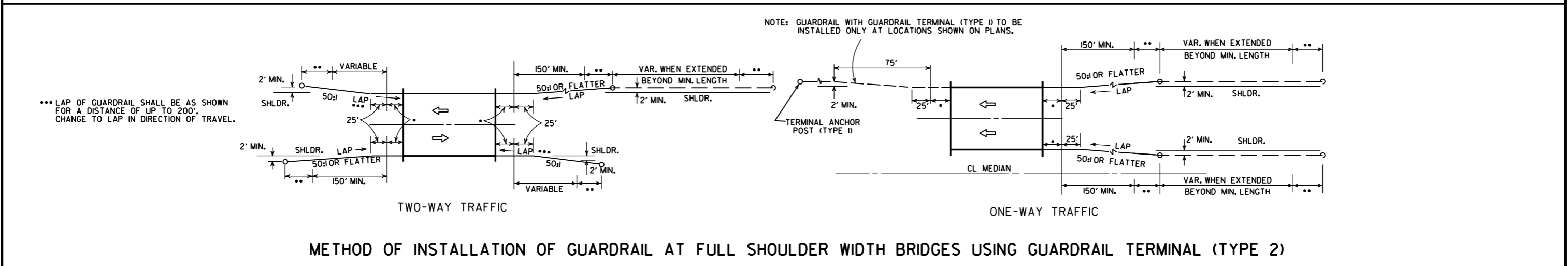
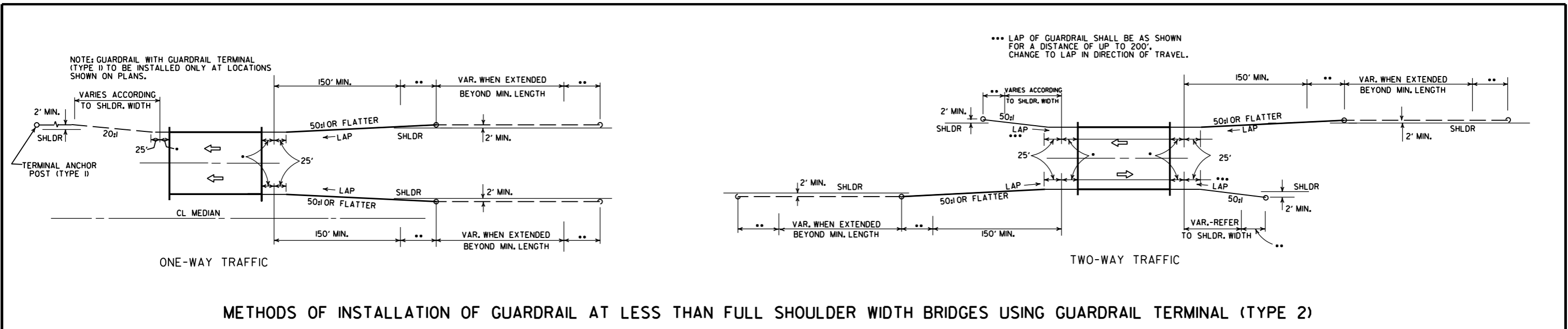
PLAN LAYOUT OF TYPE A GUARDRAIL AT LOW-FILL CULVERTS

NOTE: THIS DETAIL IS TO BE USED ONLY WHEN THE COVER OVER THE CULVERT DOES NOT PERMIT FULL EMBEDMENT OF GUARDRAIL POSTS AS SHOWN ON STD. DRWG. GR-6.

NOTE: WHEN POSSIBLE, POSTS SHALL BE SPACED TO AVOID INTERIOR AND EXTERIOR WALLS OF CULVERT. WHEN THIS IS NOT POSSIBLE AND POSTS MUST BE INSTALLED OVER AN INTERIOR OR EXTERIOR WALL, ANCHOR BOLTS SHALL BE INSTALLED BY DRILLING AND EPOXYING USING METHODS AND MATERIALS APPROVED BY THE ENGINEER.

DATE	REVISION	FILED
11-07-19	RENUMBERED, RENAMED, REVISED REFERENCE	
11-16-17	REVISED GUARDRAIL HEIGHT	
07-14-10	RAISED HEIGHT OF GUARDRAIL 1"	
04-12-07	REVISED DETAIL OF GUARDRAIL PLACEMENT BEHIND CURB	
11-10-05	ADDED GUARDRAIL PLACEMENT BEHIND CURB; REVISED DETAIL OF CONNECTION	
11-18-04	REVISED POST PLACEMENT IN ROCK & CULVERT CONNECTION DETAILS. ADDED DETAIL FOR GUARDRAIL PLACEMENT AT LOW-FILL CULVERTS	
03-30-00	REMOVED CONCRETE INSERT ANCHOR	
08-12-98	CHANGED STEEL SPACER BLOCK TO WOOD BLOCKOUT, ADDED DET. OF GUARDRAIL CONNECTION TO R.C. BOX CULVERT, DELETED DET. OF STEEL LINE POST CONN. & ADDED DET. OF GUARDRAIL PLACE. BEHIND CURB & DET. OF POSTPLACE. IN SOLID ROCK	
04-03-96	PLACED ARROWS AT CUT STEEL WASHERS	4-3-96
10-18-96	REV. ASTM REF. TO AASHTO	
11-22-95	ADDED OPTIONAL HOLES	
06-02-94	REVISED ALTERNATE POST SIZE	
08-05-93	REVISED STEEL POST SIZE	
10-01-92	REDRAWN & REVISED	10-1-92
08-02-90	DEL. WASHER ON ANCHOR ASSEMBLY	8-2-90
07-15-88	CONFORMED TO 1988 SPECS	
03-04-88	REVISED ANCHOR NOTE	
10-30-87	REVISED ANCHOR ASSEMBLY	712-10-30-87
10-30-87	REVISED PLACEMENT BEHIND CURB	547-10-30-87
10-09-87	REDRAWN & REVISED	803-10-9-87

ARKANSAS STATE HIGHWAY COMMISSION
GUARDRAIL DETAILS
 STANDARD DRAWING GR-7

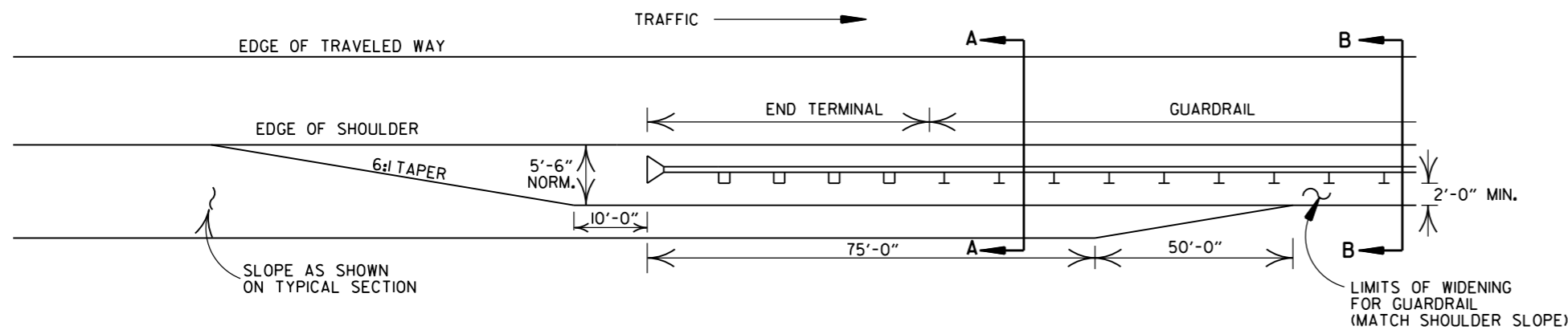


LEGEND

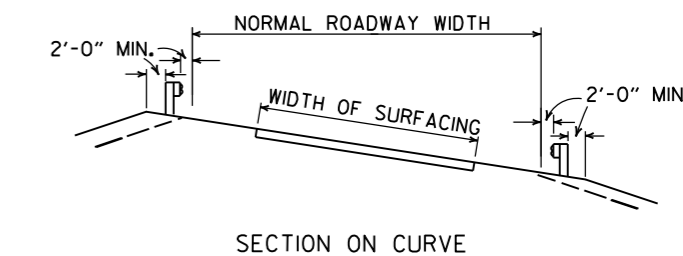
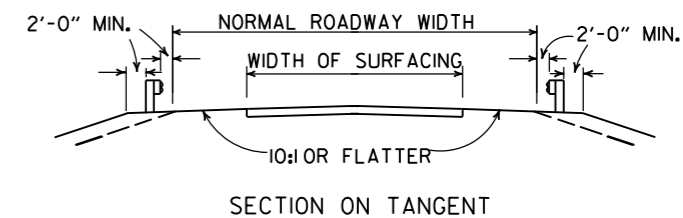
- THRE BEAM GUARDRAIL TERMINAL
- GUARDRAIL TERMINAL (TYPE 2)

DATE	REVISION	DATE FILM
11-07-19	RENUMBERED AND RENAMED	
4-17-08	REVISED LAYOUTS	
11-10-05	REMOVED GUARDRAIL NOTES AND DETAILS	
11-16-01	DELETED NOTE-METHOD OF INSTALLATION OF GUARDRAIL USING GUARDRAIL TERM. (TY. 1)	
1-12-00	ADDED CONSTRUCTION NOTE	1-12-00
6-26-97	REVISED LAYOUT	
10-1-92	REDRAWN & REVISED	10-1-92
10-9-87	ADDED NOTE	
10-9-87	REDRAWN & REVISED	

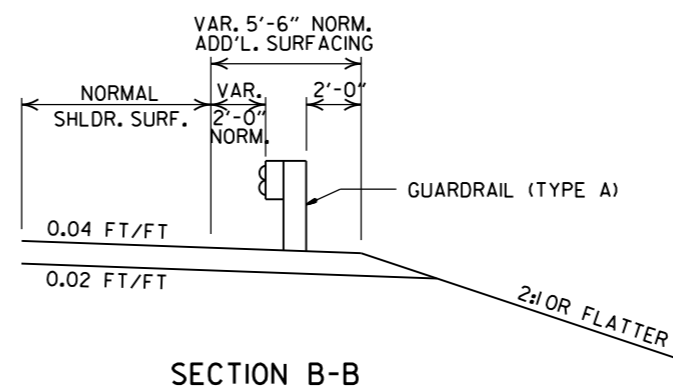
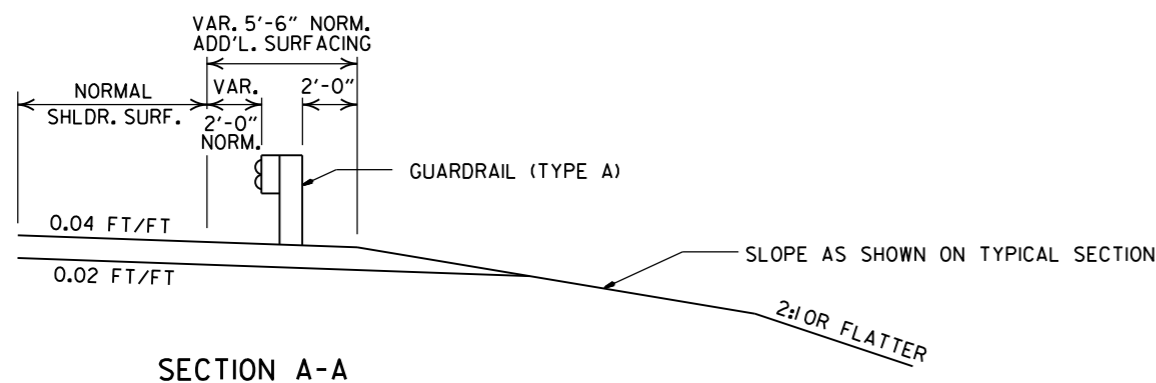
ARKANSAS STATE HIGHWAY COMMISSION
GUARDRAIL DETAILS
STANDARD DRAWING GR-8



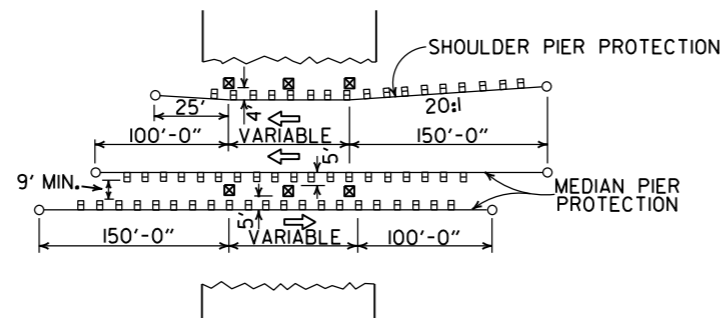
NOTE: NORMAL SECTION TO BE WIDENED APPROX. 5'-6" EACH SIDE TO SUPPORT GUARDRAIL.



DETAILS SHOWING POSITION OF GUARDRAIL ON HIGHWAY



DETAILS OF WIDENING FOR GUARDRAIL



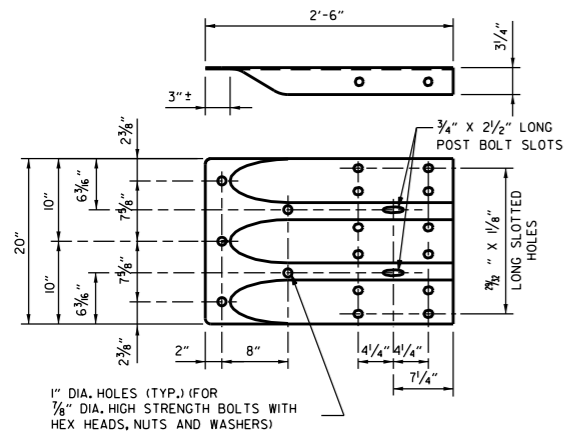
METHOD OF INSTALLATION OF GUARDRAIL AT FIXED OBSTACLE

DATE	REVISION	DATE FILM
11-07-19	RENUMBERED AND RENAMED	
4-17-08	MINOR REVISION	
11-10-05	DRAWN	

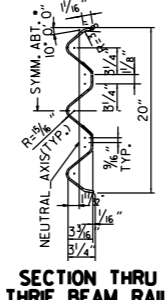
ARKANSAS STATE HIGHWAY COMMISSION

GUARDRAIL DETAILS

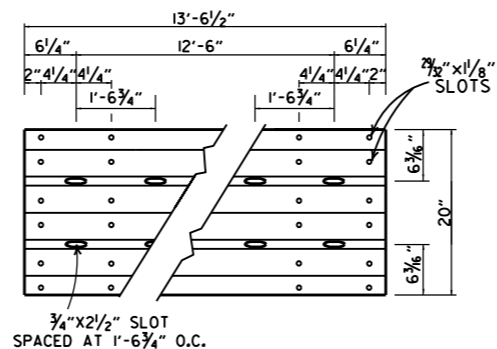
STANDARD DRAWING GR-9



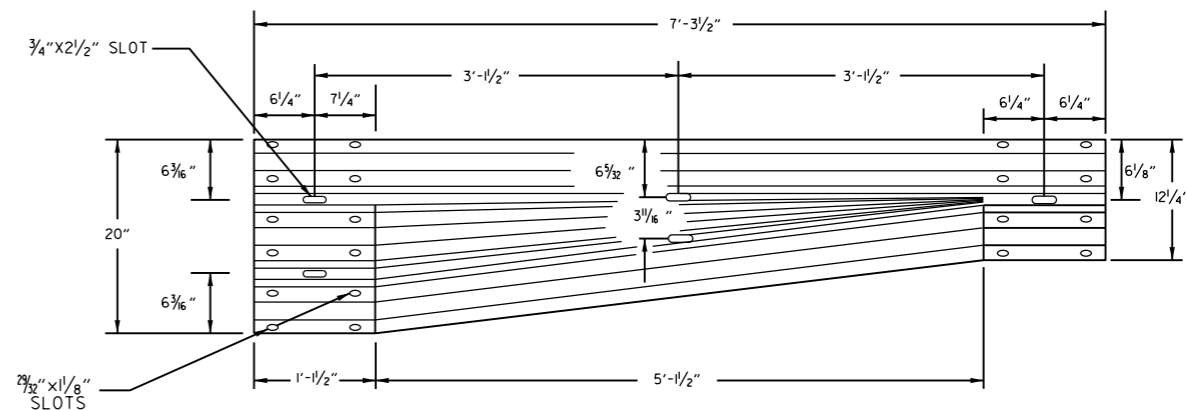
SPECIAL END SHOE



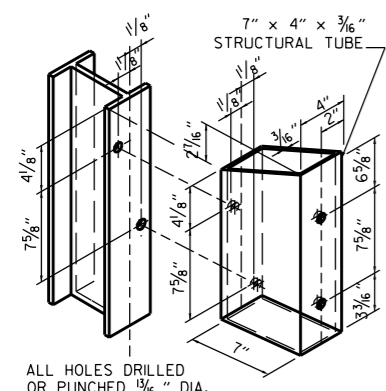
SECTION THRU THRIE BEAM RAIL



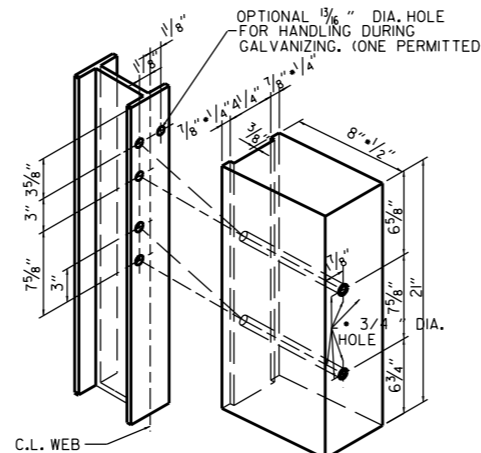
THRIE BEAM RAIL



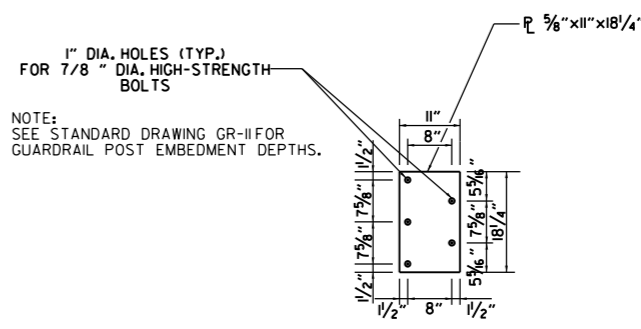
TRANSITION SECTION



STRUCTURAL STEEL TUBING BLOCKOUT DETAIL

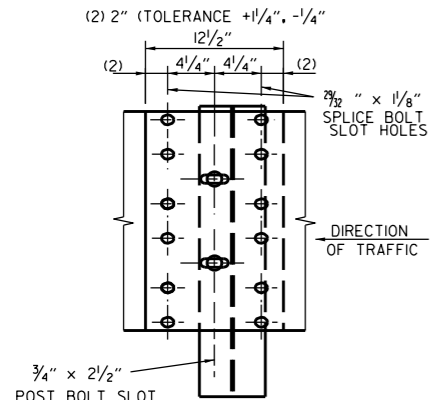


HOLE PUNCHING DETAIL FOR STEEL POST & WOOD OR PLASTIC BLOCKOUTS



CONNECTOR PLATE

CONNECTOR PLATE SHALL BE AASHTO M270, GR. 36 AND SHALL BE GALVANIZED AFTER FABRICATION. GALVANIZING SHALL CONFORM TO SUBSECTION 807.19 OF THE STANDARD SPECIFICATIONS. CONNECTOR PLATE TO BE BOLTED TO SPECIAL END SHOE USING 7/8" DIA. HIGH STRENGTH BOLTS, WITH THE HEADS PLACED ON THE TRAFFIC FACE. WASHERS SHALL BE USED UNDER THE HEAD AND NUT. BOLTS, NUTS AND WASHERS SHALL BE GALVANIZED AND SHALL CONFORM TO SUBSECTION 807.06.



THRIE BEAM RAIL SPLICE AT POST

GENERAL NOTES:

THE THRIE BEAM RAIL, SPECIAL END SHOE, AND THE TRANSITION SECTION SHALL BE MADE OF STEEL AND SHALL BE 12 GAGE. ZINC COATING SHALL BE TYPE I.

RAIL POSTS SHALL BE SET PERPENDICULAR TO THE ROADWAY PROFILE GRADE AND VERTICALLY IN CROSS SECTION.

ALL BOLTS SHALL BE SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND NO MORE THAN 3"4" BEYOND IT.

ALL LAP SPLICES, INCLUDING SPECIAL END SHOES, SHALL BE MADE IN THE DIRECTION SHOWN ON STANDARD DRAWINGS GR-8 & GR-13.

REFER TO STD. DRWG. GR-II FOR POST DETAILS.

USE THRIE BEAM GUARDRAIL COMPONENTS OF SAME MATERIAL FOR ENTIRE JOB.

THRIE BEAM POSTS SHALL BE SAME MATERIAL AS W-BEAM POSTS FOR ENTIRE JOB.

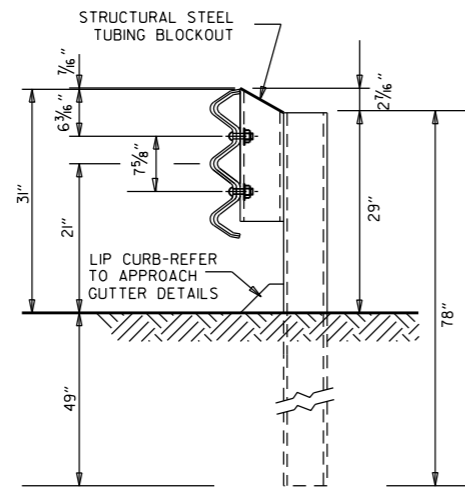
WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9.7F (1400 F) OR NO. 1 1350 F SOUTHERN PINE.

DATE	REVISION	FILMED
03-30-00	DRAWN & ISSUED	
05-18-00	ADDED NOTE	
06-29-00	MOVED DIMENSION LINES	
08-22-02	REVISED NOTE (2)	
04-10-03	REVISED GENERAL NOTES	
10-9-03	REVISED GENERAL NOTES	
11-18-04	REVISED GENERAL NOTES	
11-10-05	ADDED NOTE FOR ATTACHING STEEL BLOCKOUT	
11-29-07	ADDED PLASTIC BLOCKOUTS	
07-14-10	RAISED HEIGHT OF W-BEAM 1"	
11-16-17	REVISED TRANSITION SECTION, GUARD RAIL HEIGHT, AND GENERAL NOTES; MOVED THRIE BEAM GUARD RAIL CONNECTIONS AT BRIDGE ENDS TO STD. DRWG. GR-12	
11-07-19	RENAMED AND REVISED REFERENCES	

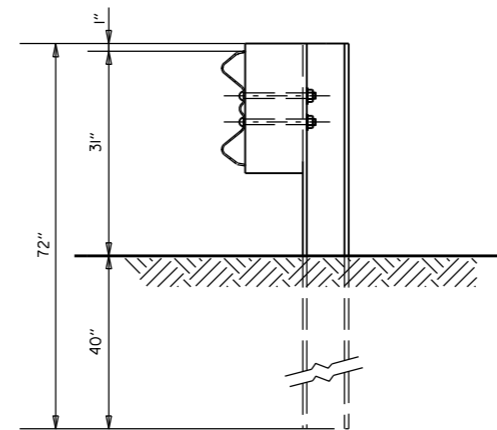
ARKANSAS STATE HIGHWAY COMMISSION

GUARDRAIL DETAILS

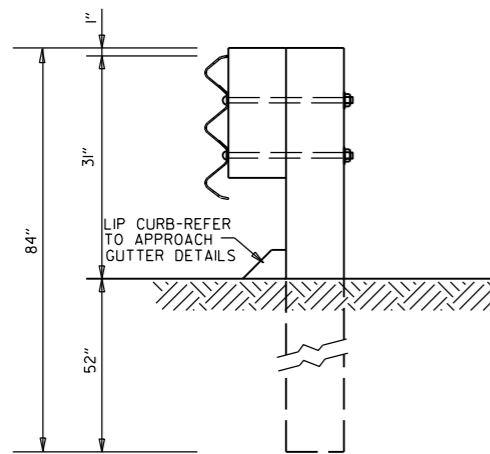
STANDARD DRAWING GR-10



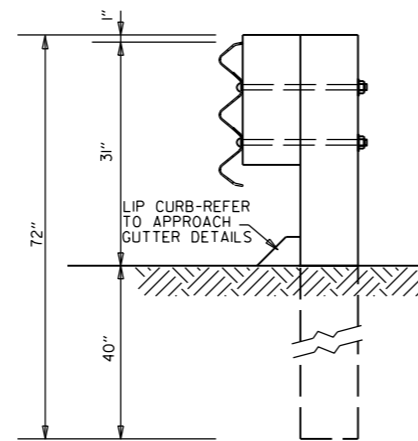
**THRIE BEAM RAIL WITH STEEL TUBING BLOCKOUT
AND STEEL POST
POSTS 1-7**



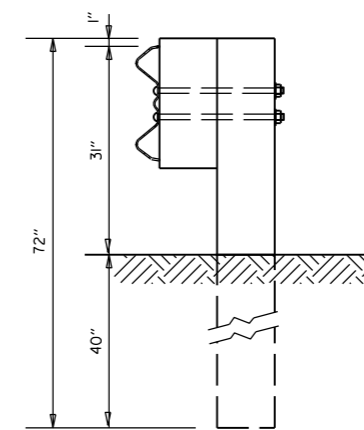
**W-BEAM TO THRIE BEAM TRANSITION RAIL
WITH WOOD OR PLASTIC BLOCKOUT AND STEEL POST
POST 8**



**THRIE BEAM RAIL
WITH WOOD OR PLASTIC
BLOCKOUTS & WOOD POSTS
POSTS 1-6**



**THRIE BEAM RAIL
WITH WOOD OR PLASTIC
BLOCKOUT & WOOD POST
POST 7**

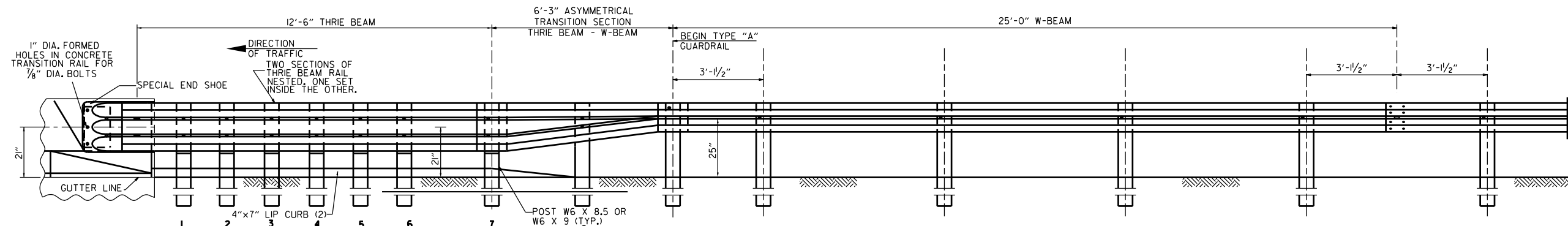


**W-BEAM TO THRIE BEAM
TRANSITION RAIL WITH WOOD OR
PLASTIC BLOCKOUT & WOOD POST
POST 8**

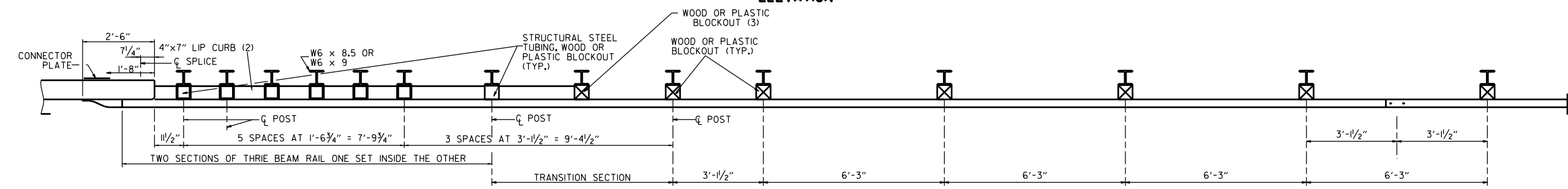
GENERAL NOTES:
RAIL POSTS SHALL BE SET PERPENDICULAR TO THE ROADWAY PROFILE GRADE AND VERTICALLY IN CROSS SECTION.

WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9.7f (1400 f) OR NO. 1 1350 f SOUTHERN PINE.

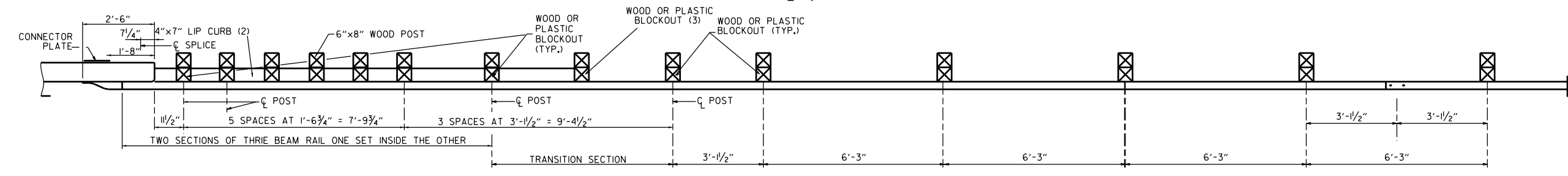
			ARKANSAS STATE HIGHWAY COMMISSION
11-07-19	RENAMED		GUARDRAIL DETAILS
11-16-17	REVISED GUARDRAIL HEIGHT, CHANGED STD. DWG. NUMBER FROM GR-10A TO GR-II		
07-14-10	REVISED POST 8 DIMENSIONS		STANDARD DRAWING GR-II
11-29-07	ADDED PLASTIC BLOCKOUTS		
08-22-02	REVISED LIP CURB NOTE		
03-30-00	DRAWN & ISSUED		
DATE	REVISION	FILMED	



ELEVATION



PLAN



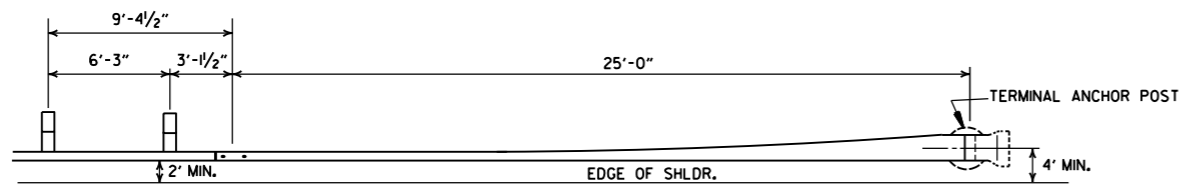
PLAN

- (1) VERIFY BOLT SPACING FROM RAIL TRANSITION PRODUCER.
- (2) REFER TO APPROACH GUTTER DETAILS.
- (3) LENGTH OF BLOCKOUT ON POST 8 TO BE MODIFIED TO FIT RAIL WIDTH.

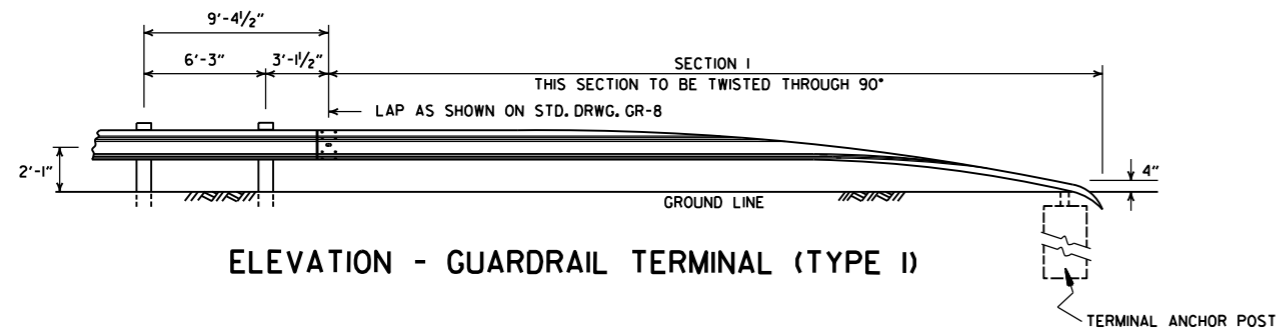
THRIE BEAM GUARDRAIL CONNECTION AT BRIDGE ENDS

GENERAL NOTES:
 THE THRIE BEAM RAIL, SPECIAL END SHOE, AND THE TRANSITION SECTION SHALL BE MADE OF STEEL AND SHALL BE 12 GAGE. ZINC COATING SHALL BE TYPE I.
 RAIL POSTS SHALL BE SET PERPENDICULAR TO THE ROADWAY PROFILE GRADE AND VERTICALLY IN CROSS SECTION.
 ALL BOLTS SHALL BE SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND NO MORE THAN 3/4" BEYOND IT.
 ALL LAP SPLICES, INCLUDING SPECIAL END SHOES, SHALL BE MADE IN THE DIRECTION SHOWN ON STANDARD DRAWINGS GR-8 & GR-13.
 REFER TO STD. DRWG. GR-II FOR POST DETAILS.
 USE THRIE BEAM GUARDRAIL COMPONENTS OF SAME MATERIAL FOR ENTIRE JOB.
 THRIE BEAM POSTS SHALL BE SAME MATERIAL AS W-BEAM POSTS FOR ENTIRE JOB.
 POSTS SHALL NOT BE PLACED AT SPLICE LOCATIONS ALONG W-BEAM RAILS.
 WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9.7F (1400 F) OR NO. 1 1350 F SOUTHERN PINE.

			ARKANSAS STATE HIGHWAY COMMISSION
			GUARDRAIL DETAILS
			STANDARD DRAWING GR-12
05-14-20	REVISED NOTES		
11-07-19	RENAMED & REVISED REFERENCES		
11-16-17	RE-DRAWN FROM STD. DWG. GR-10 & ISSUED		
DATE	REVISION		FILMED

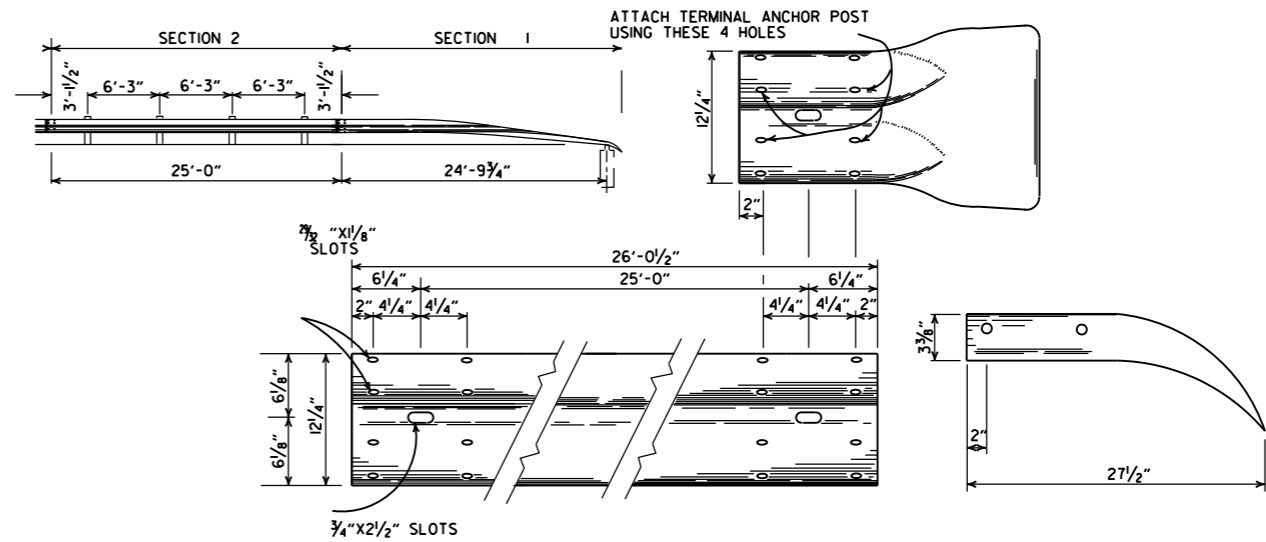


PLAN - GUARDRAIL TERMINAL (TYPE I)



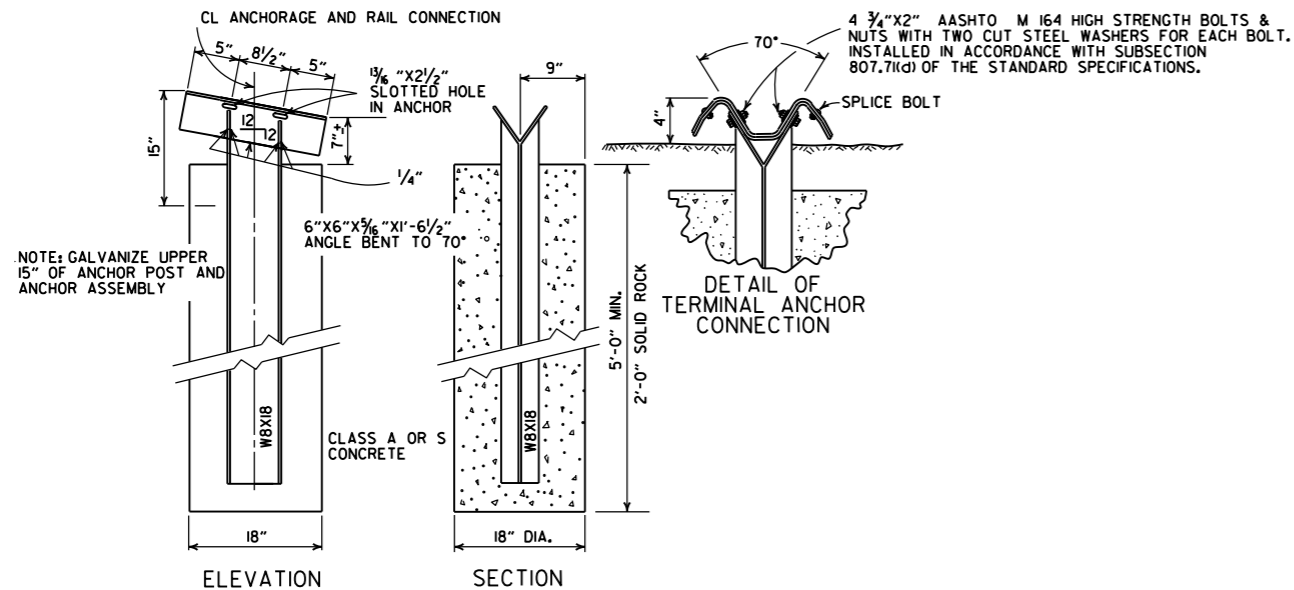
ELEVATION - GUARDRAIL TERMINAL (TYPE I)

NOTE:
SECTIONS 1 AND 2 OF GUARDRAIL TERMINAL
SHALL BE PAID FOR AT THE PRICE BID PER
LINEAR FOOT OF THE TYPE OF GUARDRAIL SPECIFIED.



SECTION 1


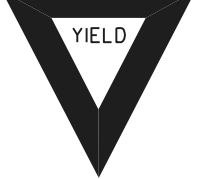





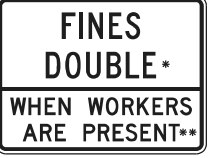


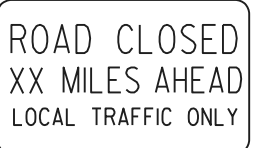


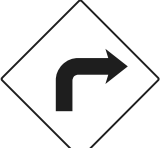






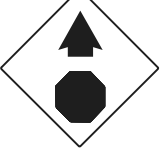

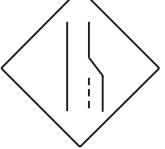




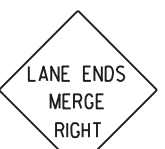















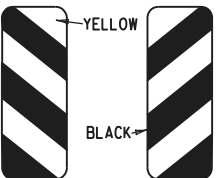


TERMINAL SECTION



DETAIL OF TERMINAL ANCHOR POST (TYPE I)

NOTE: RAIL MEMBERS MAY BE BOLTED TO ANGLE AT TERMINAL ANCHOR AND THE TWO ASSEMBLIES POSITIONED TO PROPER ALIGNMENT PRIOR TO PLACING CONCRETE AROUND 8 W/ 17 POST IF CONTRACTOR SO DESIRES.

11-07-19	RENAMED & REVISED REFERENCE.		ARKANSAS STATE HIGHWAY COMMISSION
11-16-17	REVISED GUARDRAIL HEIGHT AND LOCATION OF POSTS		GUARDRAIL DETAILS
07-14-10	RAISED HEIGHT OF GUARDRAIL 1"		
06-26-97	REVISED LAP NOTE		STANDARD DRAWING GRT-1
10-18-96	REVISED ASTM REF. TO AASHTO		
11-03-94	DIMENSION TERMINAL DETAIL		
11-11-92	ADDED NOTE FOR PAYMENT	11-11-92	
10-01-92	DRAWN & ISSUED	10-1-92	
DATE	REVISION	FILMED	

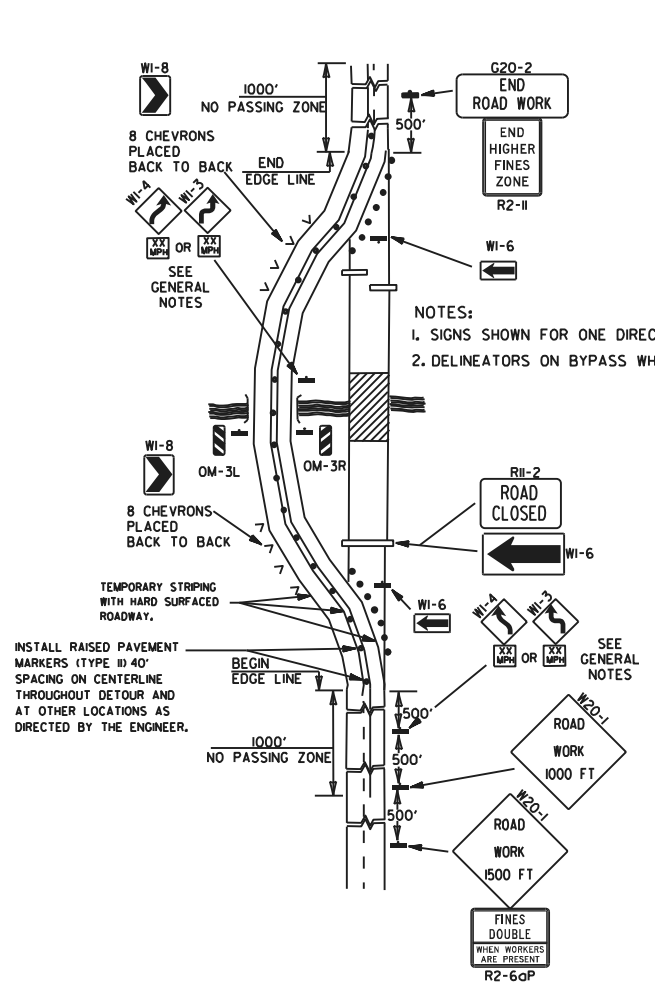
<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>W3-5</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>W3-5a</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R2-6aP</p>  <p>48"x36" *USE 6" C LETTERS **USE 4" D LETTERS</p>
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>W21-5a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>WI-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>WI-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>R2-II</p>  <p>36"x48"</p>
<p>WI-3</p>  <p>STD. 48"x48"</p>	<p>WI-4</p>  <p>STD. 48"x48"</p>	<p>WI-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>WI-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>R56-1</p>  <p>STD. 18"x18"</p>
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>	<p>W20-3</p>  <p>STD. 48"x48"</p>
<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>18" 500 FEET W6-2 24"</p> <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>	<p>WI-4b</p>  <p>STD. 48"x48"</p>	
<p>W8-II</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>	<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>	

GENERAL NOTES:

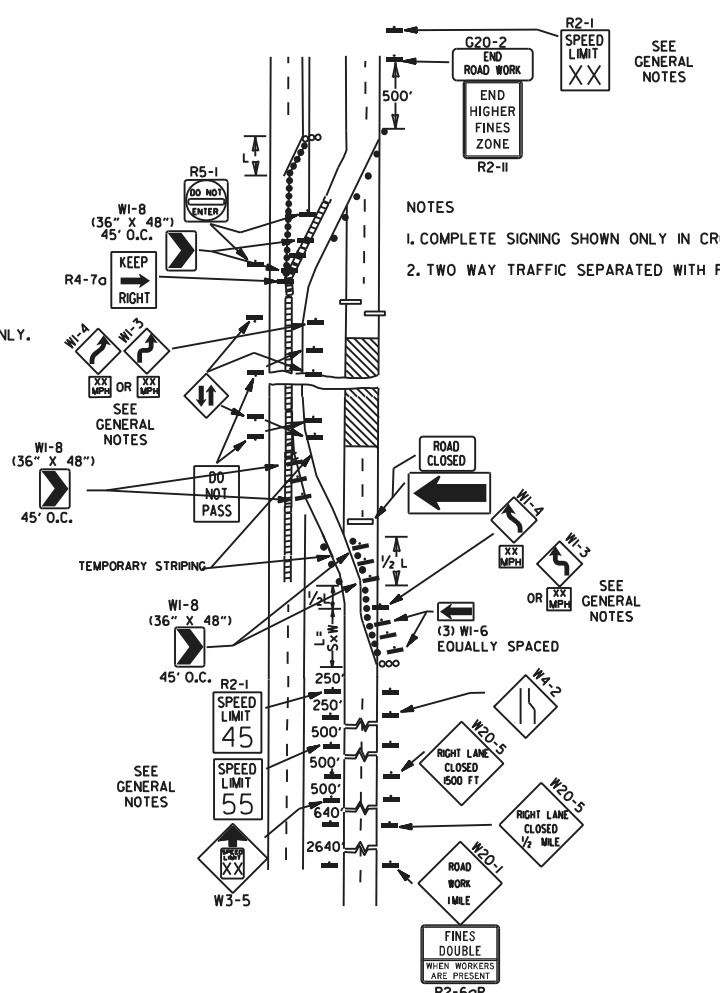
- ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.
- TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.
- EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.
- SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SO. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE.
- SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.
- POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.
- ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE, EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.
- FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
- MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.
- R2-6aP PLAQUES SHALL BE MOUNTED BELOW AN APPLICABLE ADVANCE WARNING SIGN THAT IS LOCATED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE PLAQUE SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE W3-5 "REDUCED SPEED AHEAD" SIGN. R2-II SIGNS SHALL BE INSTALLED AT OR NEAR THE DOWNSTREAM END OF THE WORK ZONE. SEE STANDARD DRAWINGS TC-2, TC-3, AND TC-6 FOR TYPICAL PLACEMENT LOCATIONS.

*NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.

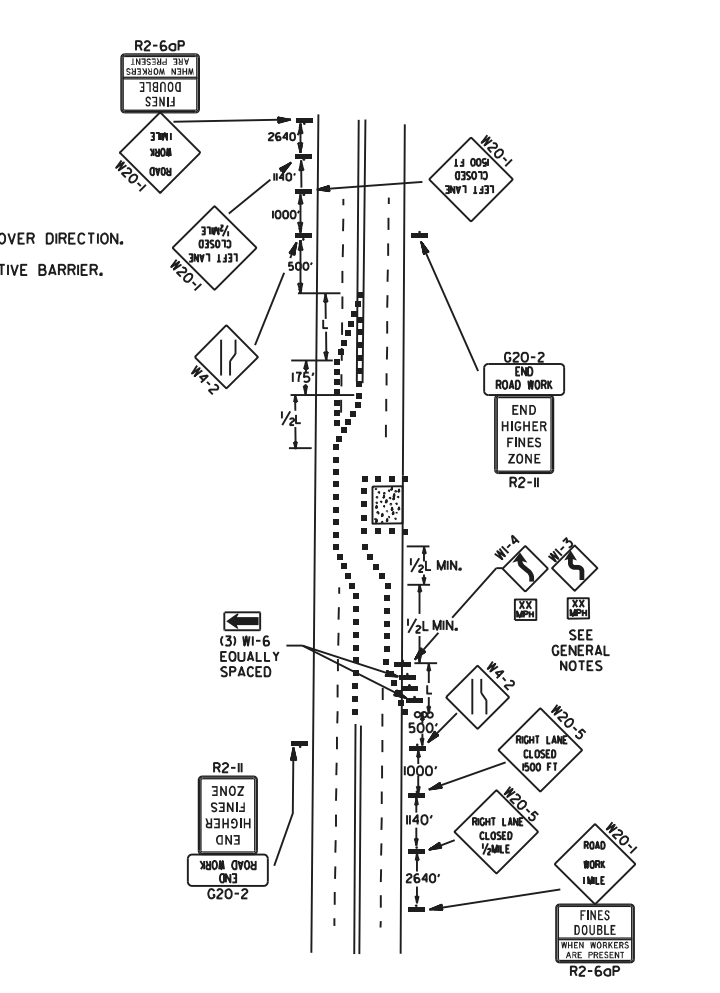
<p>REPLACED R55-1 WITH R2-6aP, ADDED R2-11 & REVISED NOTE 10</p> <p>11-07-19 REVISED FOR MASH & ADDED W21-5a</p> <p>04-13-17 DELETED RSP-1 REVISED REDUCED SPEED SIGN FOR ROADWORK NEXT SIX MILES</p> <p>09-02-15 REVISED W24-1</p> <p>12-15-11 REVISED W24-1</p>	<p>AR DOT ARKANSAS DEPARTMENT OF TRANSPORTATION</p> <p>ARKANSAS STATE HIGHWAY COMMISSION</p> <p>STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION</p>
<p>08-14-25</p> <p>11-07-19</p> <p>04-13-17</p> <p>09-02-15</p> <p>12-15-11</p> <p>DATE REV</p>	<p>DESCRIPTION</p> <p>DATE EFFECTIVE</p> <p>08-14-2025</p> <p>STANDARD DRAWING</p> <p>TC-1</p>



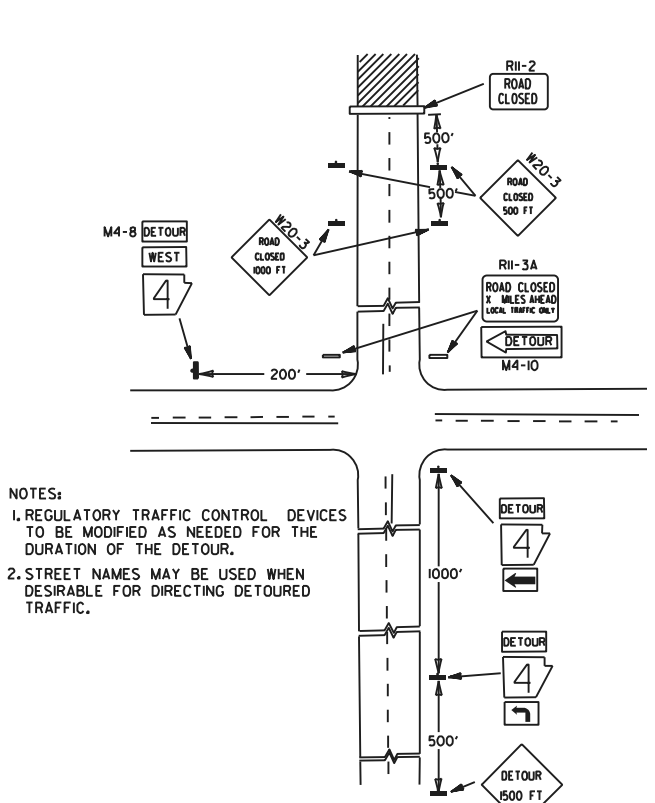
(A) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 2-LANE HIGHWAY WHERE THE ENTIRE ROADWAY IS CLOSED AND A BYPASS DETOUR IS PROVIDED.



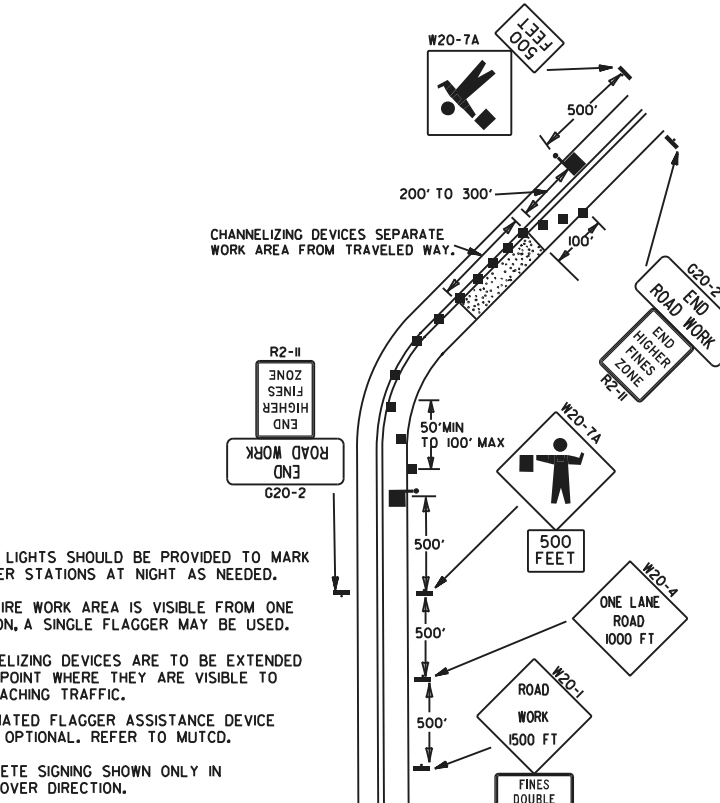
(B) TYPICAL APPLICATION - 4-LANE DIVIDED ROADWAY WHERE ONE ROADWAY IS CLOSED.



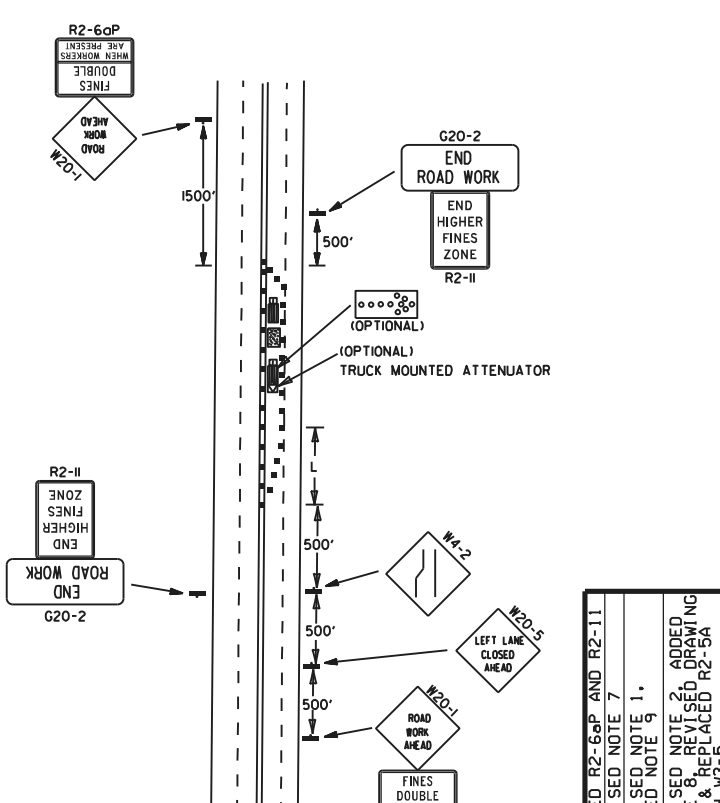
(C) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.



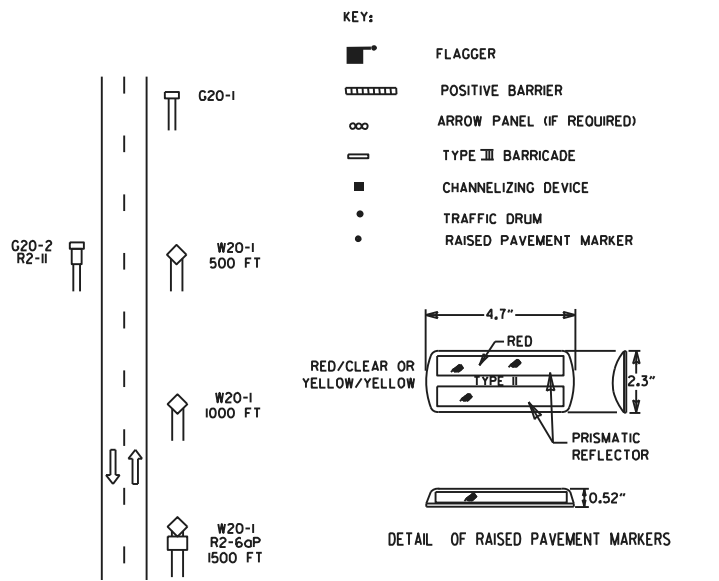
(D) TYPICAL APPLICATION - ROADWAY CLOSED BEYOND DETOUR POINT.



(E) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 2-LANE HIGHWAY WHERE ONE LANE IS CLOSED AND FLAGGING IS PROVIDED.



(F) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.

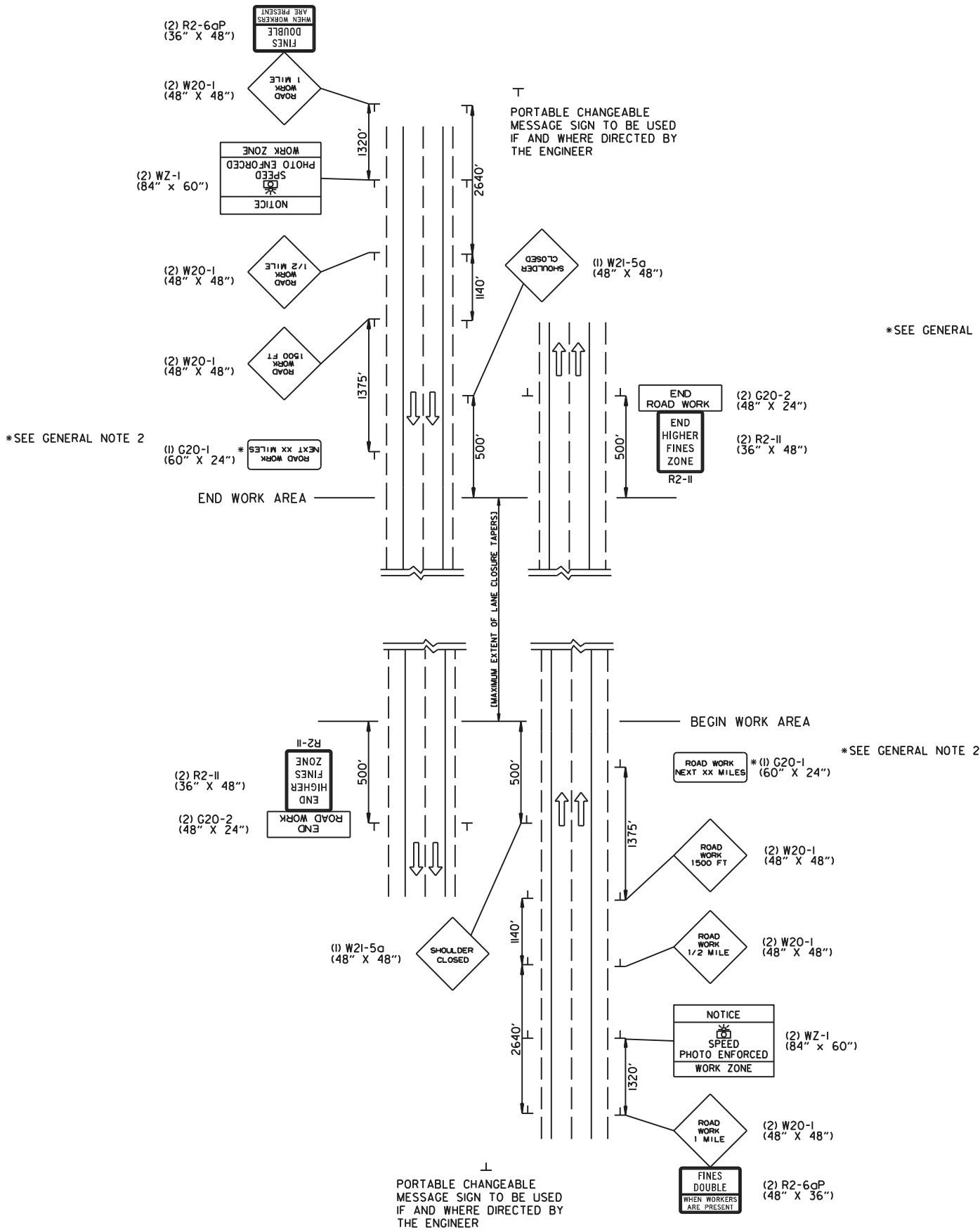


TYPICAL ADVANCE WARNING SIGN PLACEMENT
 TAPER FORMULAE:
 $L = SXW$ FOR SPEEDS OF 45MPH OR MORE.
 $L = \frac{WS^2}{60}$ FOR SPEEDS OF 40MPH OR LESS.
 WHERE:
 L = MINIMUM LENGTH OF TAPER.
 S = NUMERICAL VALUE OF POSTED SPEED LIMIT PRIOR TO WORK OR 85TH PERCENTILE SPEED.
 W = WIDTH OF OFFSET.

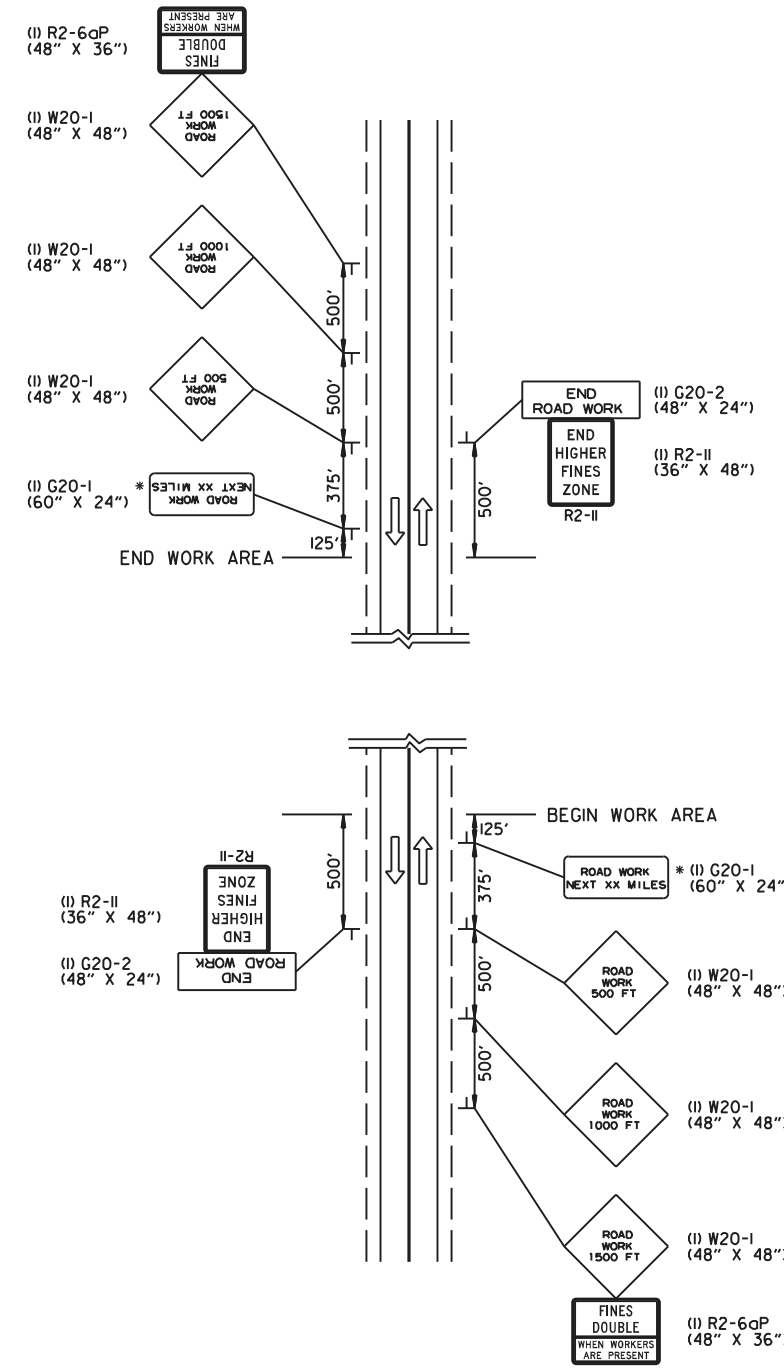
- GENERAL NOTES:
1. THE MAINTENANCE DIVISION SHALL CONDUCT A BALL BANK STUDY TO DETERMINE THE ADVISORY SPEED LIMIT PRIOR TO OPENING TO TRAFFIC. THE ADVISORY SPEED WILL BE POSTED ON W1-3 OR W1-4 CURVE WARNING SIGNS. USE W1-4 WHEN SPEED IS GREATER THAN 30MPH AND W1-3 WHEN 30MPH OR LESS.
 2. WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-155 SHALL BE OMITTED AND THE W3-5 SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-145MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/2 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-145 SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 3. WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-165 SHALL BE OMITTED. ADDITIONAL R2-155MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/2 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-155 SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 4. THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT, OR AS DIRECTED BY THE ENGINEER.
 5. WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
 6. PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
 7. TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE. PAYMENT FOR TRAFFIC DRUMS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR VARIOUS TRAILER MOUNTED DEVICES.
 8. DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE ARDOT QUALIFIED PRODUCTS LIST.
 9. ALL TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL MEET THE REQUIREMENTS OF THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).

		ARKANSAS STATE HIGHWAY COMMISSION	
STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION			
08-14-25 ADDED R2-6aP AND R2-11 05-20-21 REVISED NOTE 7 11-07-19 REVISED NOTE 1, ADDED NOTE 9 REVISED NOTE 2 ADDED NOTE 8, REVISED DRAWING (A) & REPLACED R2-15A WITH W3-5 REVISED DETAIL OF RAISED PAVEMENT MARKINGS	DESCRIPTION	DATE EFFECTIVE 08-14-2025	STANDARD DRAWING
DATE REV	DESCRIPTION	DATE EFFECTIVE	TC-2

NOTE : W20-1 (VARIOUS DISTANCE) ADVANCE SIGNS TO BE REPLACED AS NEEDED BY EQUIVALENT W20-5 SIGNS AS WORKING AREA SHIFTS.



(A) TYPICAL APPLICATION - ADVANCE WARNING SIGNS AT BEGINNING AND END OF JOB (FULLY CONTROLLED ACCESS FACILITIES) (ALL STAGES)



(B) TYPICAL APPLICATION - ADVANCE WARNING SIGNS AT BEGINNING AND END OF JOB (NON-FULLY CONTROLLED ACCESS FACILITIES) (ALL STAGES)

GENERAL NOTES:

1. WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
2. THE G20-1 SIGN SHOULD BE INSTALLED ON JOBS OF OVER TWO MILES IN LENGTH, WHEN THE LANE CLOSURE IS NOT AT THE BEGINNING OF THE PROJECT, THE G20-1 SIGN SHALL BE ERECTED 125' IN ADVANCE OF THE JOB LIMIT. ADDITIONAL W20-1 (1 MILE OR 1500 FT) SIGNS ARE NOT REQUIRED IN ADVANCE OF LANE CLOSURES THAT BEGIN INSIDE THE PROJECT LIMITS. THE DISTANCE DISPLAYED ON THE G20-1 SIGN SHALL BE STATED TO THE NEAREST WHOLE MILE.
3. PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER, WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THIS DEVICE SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE. PAYMENT FOR TRAFFIC DRUMS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR PORTABLE CHANGEABLE MESSAGE SIGNS.
4. PORTABLE CHANGEABLE MESSAGE SIGNS SHALL MEET THE REQUIREMENTS OF THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).

*SEE GENERAL NOTE 2

*SEE GENERAL NOTE 2



ARKANSAS STATE HIGHWAY COMMISSION

STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION

08-14-25 ISSUED
DATE REV

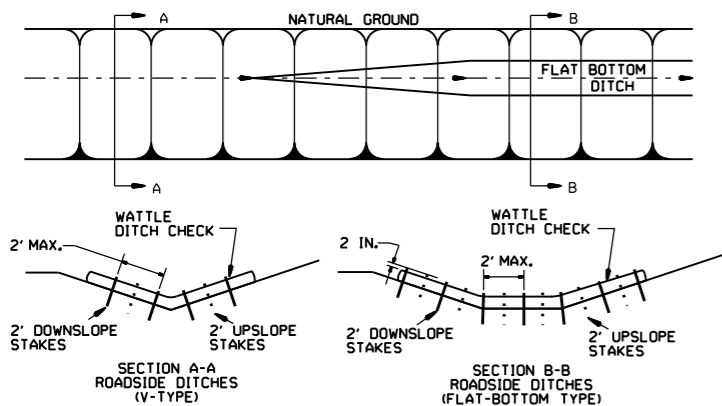
DATE EFFECTIVE
08-14-2025

STANDARD DRAWING

TC-6

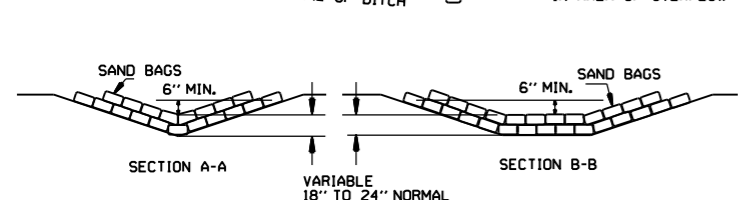
GENERAL NOTES

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

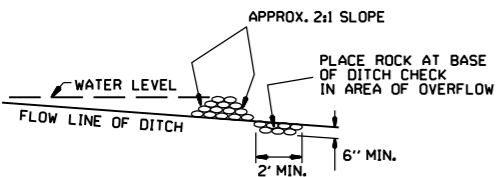


WATTLE DITCH CHECK (E-1)

NUMBER OF SAND BAGS AND ARRANGEMENT VARIABLE WITH ON-SITE CONDITIONS. PLACE SAND BAGS AT BASE OF DITCH CHECK IN AREA OF OVERFLOW.

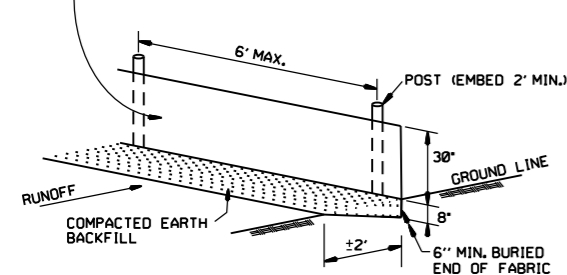


SAND BAG DITCH CHECK (E-5)

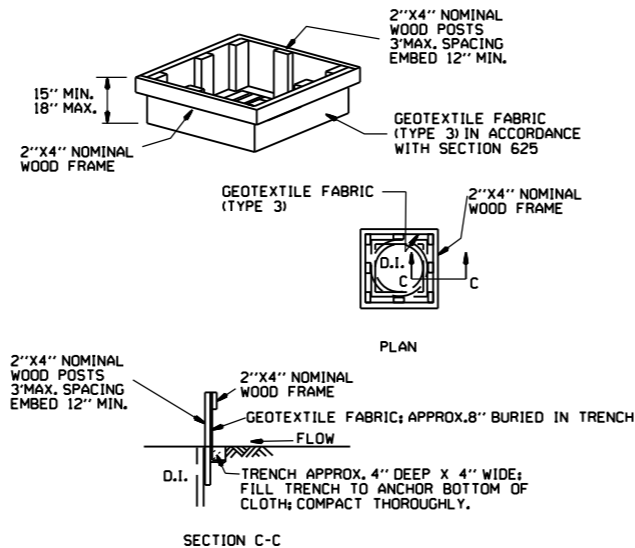


ROCK DITCH CHECK (E-6)

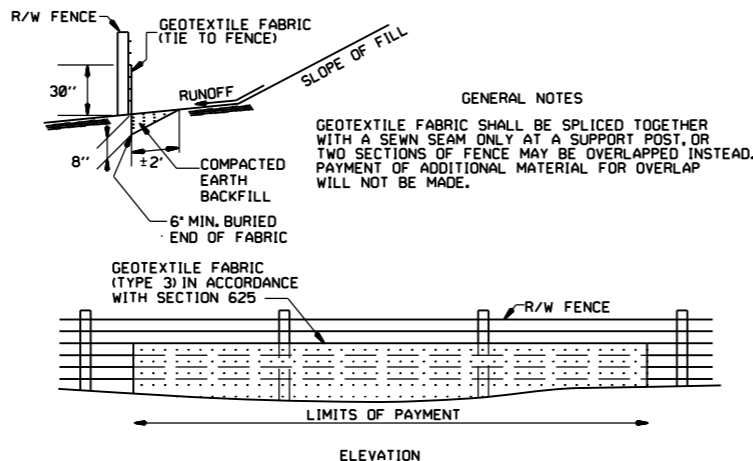
GENERAL NOTES
 GEOTEXTILE FABRIC (TYPE 4) IN ACCORDANCE WITH SECTION 625
 GEOTEXTILE FABRIC SHALL BE SPLICED TOGETHER WITH A SEWN SEAM ONLY AT A SUPPORT POST OR TWO SECTIONS OF FENCE MAY BE OVERLAPPED INSTEAD. PAYMENT OF ADDITIONAL MATERIAL FOR OVERLAP WILL NOT BE MADE.



SILTS FENCE (E-11)

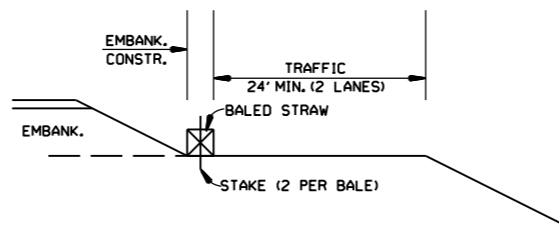


DROP INLET SILTS FENCE (E-7)

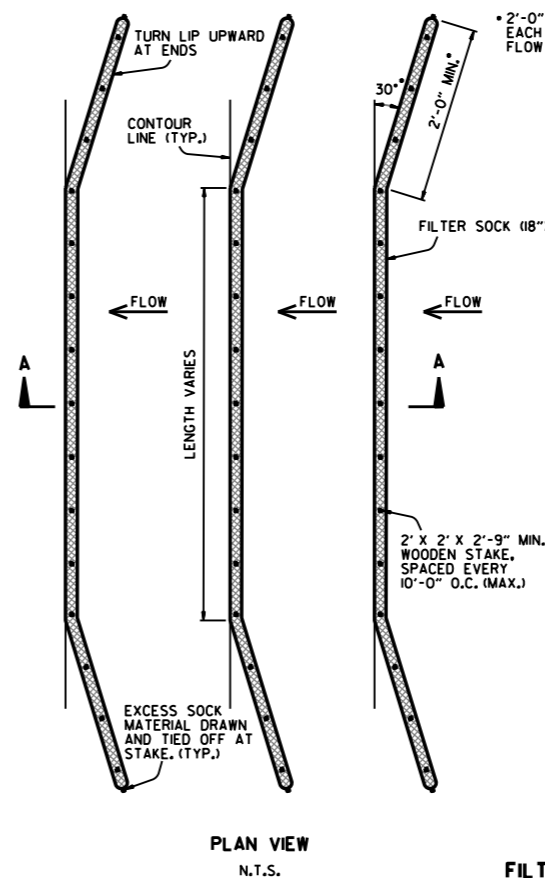


SILTS FENCE ON R/W FENCE (E-4)

GENERAL NOTES
 1. STRAW BALES SHALL BE INSTALLED SO THAT THE BINDINGS ARE ORIENTED AROUND THE SIDES RATHER THAN ALONG THE TOPS AND BOTTOMS OF THE BALES. THE BALES SHALL BE A MINIMUM OF 30 INCHES IN LENGTH.
 2. NO GAPS SHALL BE LEFT BETWEEN BALES.
 3. BALED STRAW FILTER BARRIERS COMPLETED AND ACCEPTED WILL BE MEASURED BY THE BALE IN PLACE AS AUTHORIZED BY THE ENGINEER AND WILL BE PAID FOR AT THE CONTRACT UNIT PRICE BID PER BALE FOR BALED STRAW DITCH CHECKS.

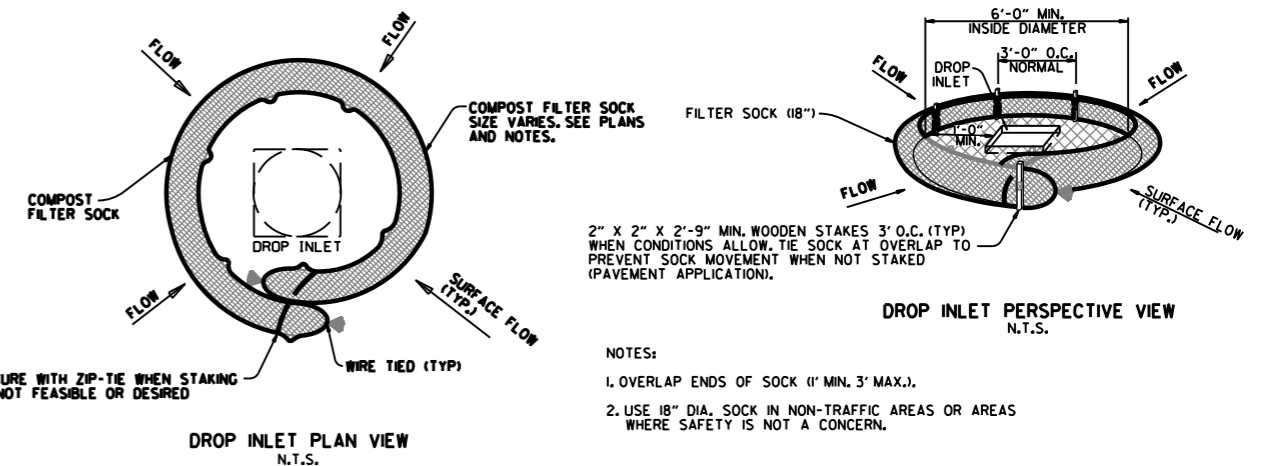


BALED STRAW FILTER BARRIER (E-2)



FILTER SOCK ALONG SLOPE (E-3)

NOTES:
 1. FILTER SOCKS CAN BE PLACED AT THE TOP, ON THE FACE, AND AT THE TOE OF SLOPES AS SEDIMENT-TRAPPING DEVICES FOR SHEET FLOW RUNOFF.
 2. FILTER SOCKS ARE TYPICALLY SUPPLIED AND INSTALLED WITH 18 INCH DIAMETERS. DIAMETER TOLERANCE IS 2 INCHES, AS FILTER SOCKS TEND TO FLATTEN OUT WHEN PLACED.
 3. STEEL POSTS MAY BE USED AND SHALL BE ROLLED FROM HIGH CARBON STEEL AND HAVE A MINIMUM OF 1.25 LB./FT. POSTS SHALL BE HOT-DIPPED GALVANIZED OR PAINTED WITH HIGH-GRADE WEATHER RESISTANT BROWN OR BLACK STEEL PAINT. STEEL POSTS SHALL BE EQUIPPED WITH ANCHOR PLATE HAVING A MINIMUM AREA OF 14 SQUARE INCHES. POSTS SHALL BE STUDDED, EMBOSSED, OR PUNCHED. POSTS AND ANCHOR PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A702. NO ADDITIONAL PAYMENT WILL BE PROVIDED FOR STEEL POSTS, BUT PRICE WILL BE CONSIDERED SUBSIDIARY TO "FILTER SOCK (18\"/>



COMPOST FILTER SOCK DROP INLET PROTECTION (E-13)

NOTES:
 1. OVERLAP ENDS OF SOCK (1' MIN. 3' MAX.).
 2. USE 18" DIA. SOCK IN NON-TRAFFIC AREAS OR AREAS WHERE SAFETY IS NOT A CONCERN.

DATE	REVISION
11-16-17	ADDED FILTER SOCK E-3 AND E-13
12-15-11	DELETED BALED STRAW DITCH CHECK & ADDED WATTLE DITCH CHECK
11-18-98	ADDED NOTES
07-02-98	ADDED BALED STRAW FILTER BARRIER (E-2)
07-20-95	REVISED SILTS FENCE E-4 AND E-11
07-15-94	REV. E-4 & E-11 MIN. 13" BURIED END OF FABRIC
06-02-94	REVISED E-1, 4, 7 & 11; DELETED E-2 & 3
04-01-93	REDRAWN
10-01-92	REDRAWN
08-02-76	ISSUED R.D.M.

ARKANSAS STATE HIGHWAY COMMISSION
 TEMPORARY EROSION CONTROL DEVICES
 STANDARD DRAWING TEC-1