

"A FULLY CONTROLLED ACCESS FACILITY"
 ARKANSAS DEPARTMENT OF TRANSPORTATION
 CONSTRUCTION PLANS FOR STATE HIGHWAY



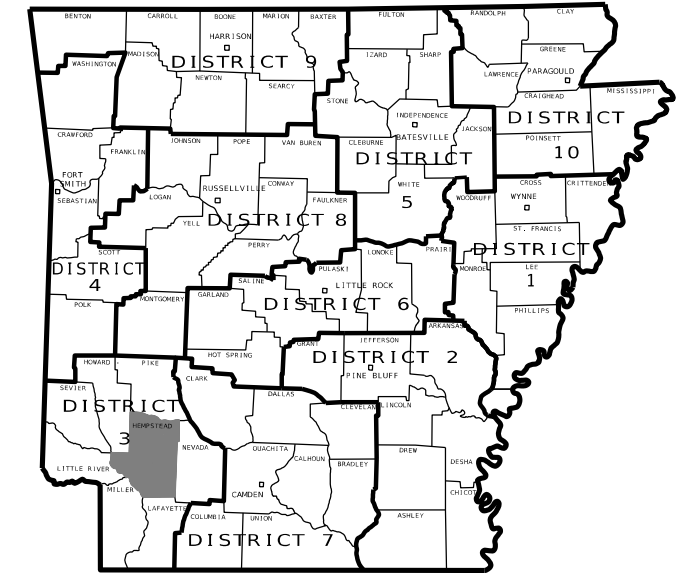
**DISTRICT 3 PAVEMENT
 FRICTION IMPVTS. (UTBWC) (S)**

HEMPSTEAD COUNTY
 ROUTE I-30 SECTION 12

JOB 030656

FED. AID PROJ. HSIP-2952(I)

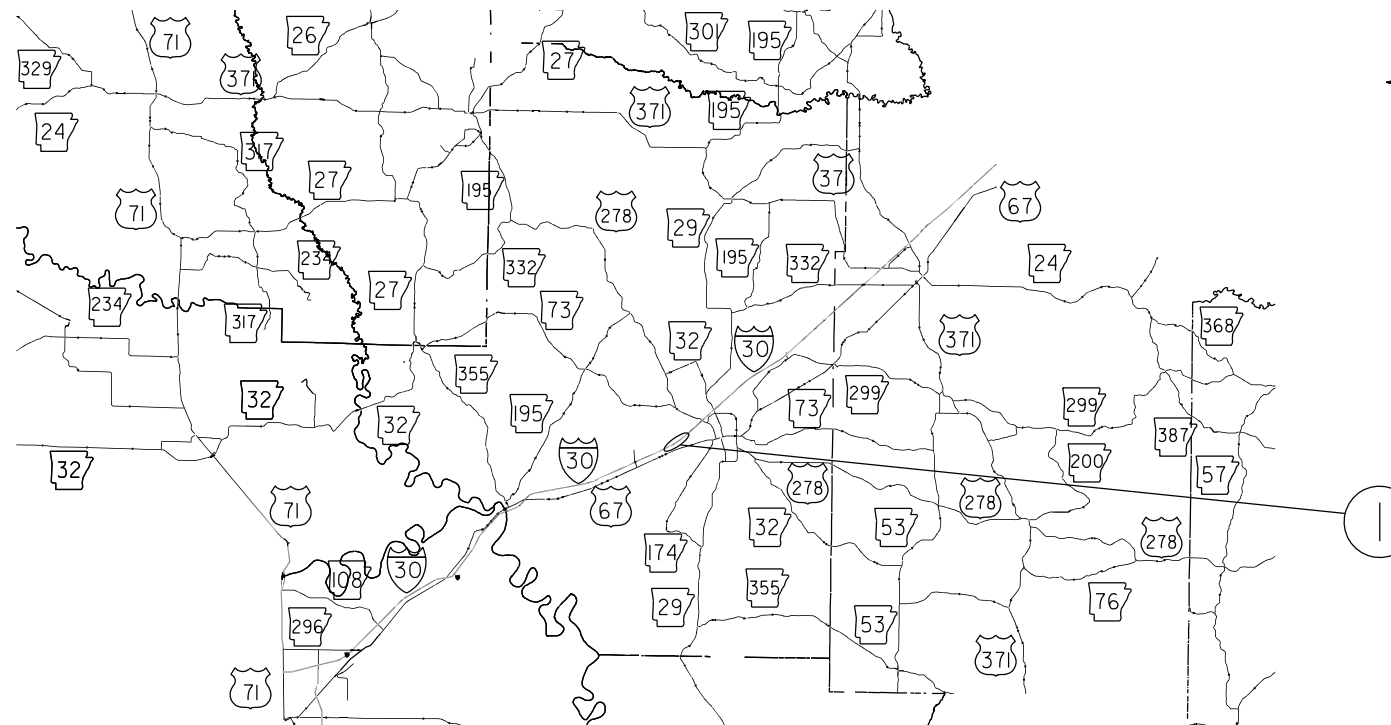
DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	030656	1	12
DISTRICT 3 PAVEMENT FRICTION IMPVTS. (UTBWC) (S)						



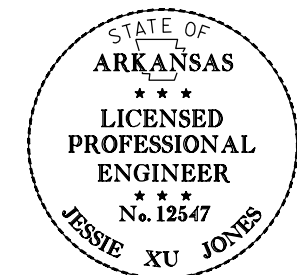
ARK. HWY. DIST. NO. 3

KEY	COUNTY	ROUTE	SECTION	BEGIN LM	END LM
1	HEMPSTEAD	I-30 EB	12	27.240	29.180

NOT TO SCALE



APPROVED



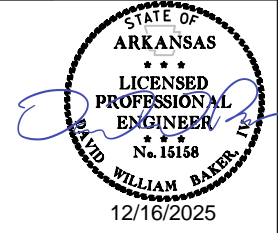
HEMPSTEAD COUNTY

GROSS LENGTH OF PROJECT	10243.20	FEET	OR	1.940	MILES
NET ROADWAY	10243.20			1.940	MILES
NET BRIDGES	00.00			0.000	MILES
NET PROJECT	10243.20			1.940	MILES

CHIEF ENGINEER - PRECONSTRUCTION

030656 TITLE SHEET.DGN CREATED: 2/25/2025 11/12/2025 OPENROADS DESIGNER V10.16.2.267

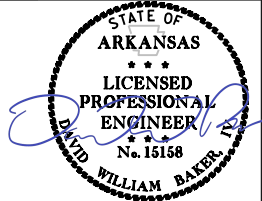
DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	030656	2	12
INDEX OF SHEETS AND STANDARD DRAWINGS						



SHEET NO.	INDEX OF SHEETS	TITLE
1	_____	TITLE SHEET
2	_____	INDEX OF SHEETS AND STANDARD DRAWINGS
3	_____	GOVERNING SPECIFICATIONS AND GENERAL NOTES
4	_____	TYPICAL SECTIONS OF IMPROVEMENT
5 - 6	_____	SPECIAL DETAILS
7 - 8	_____	MAINTENANCE OF TRAFFIC DETAILS
9	_____	PERMANENT PAVEMENT MARKING DETAILS
10 - 11	_____	QUANTITIES
12	_____	SUMMARY OF QUANTITIES AND REVISIONS

ROADWAY STANDARD DRAWINGS		
DRWG. NO.	TITLE	DATE
PM-1	PAVEMENT MARKING DETAILS	02-27-20
PM-2	PERMANENT PAVEMENT MARKING ON ACCESS CONTROLLED ROADWAYS	05-14-20
TC-1	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	08-14-25
TC-2	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	08-14-25
TC-3	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	08-14-25
TC-6	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	11-06-25

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	030656	3	12
GOVERNING SPECIFICATIONS AND GENERAL NOTES						



GOVERNING SPECIFICATIONS

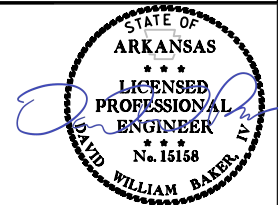
ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2014, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS:

NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
FHWA-1273	REQUIRED CONTRACT PROVISIONS FEDERAL-AID CONSTRUCTION CONTRACTS
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - NOTICE TO CONTRACTORS
FHWA-1273	SUPPLEMENT - SPECIFIC EQUAL EMPLOYMENT OPPORTUNITY RESPONSIBILITIES (23 U.S.C. 140)
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - GOALS AND TIMETABLES
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - FEDERAL STANDARDS
FHWA-1273	SUPPLEMENT - POSTERS AND NOTICES REQUIRED FOR FEDERAL-AID PROJECTS
FHWA-1273	SUPPLEMENT - WAGE RATE DETERMINATION
100-3	CONTRACTOR'S LICENSE
100-4	DEPARTMENT NAME CHANGE
102-2	ISSUANCE OF PROPOSALS
102-3	PREQUALIFICATION OF BIDDERS
103-2	CONTACT INFORMATION FOR MOTORIST DAMAGE CLAIMS
105-4	MAINTENANCE DURING CONSTRUCTION
107-2	RESTRAINING CONDITIONS
108-1	LIQUIDATED DAMAGES
108-2	WORK ALLOWED PRIOR TO ISSUANCE OF WORK ORDER
108-3	WORK ORDER FOR FIXED COMPLETION DATE CONTRACTS
306-1	QUALITY CONTROL AND ACCEPTANCE
400-4	DESIGN AND QUALITY CONTROL OF ASPHALT MIXTURES
400-5	PERCENT AIR VOIDS FOR ACHM MIX DESIGNS
400-6	LIQUID ANTI-STRIP ADDITIVE
404-3	DESIGN OF ASPHALT MIXTURES
409-2	ASPHALT LABORATORY FACILITY
410-4	EVALUATION OF ACHM SUBLLOT REPLACEMENT MATERIAL
603-1	LANE CLOSURE NOTIFICATION
604-1	RETROREFLECTIVE SHEETING FOR TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
604-3	TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES (MASH)
JOB 030656	ASSESSMENT OF WORKING DAYS - MAINTENANCE OF TRAFFIC
JOB 030656	BIDDING REQUIREMENTS AND CONDITIONS
JOB 030656	BUY AMERICA - CONSTRUCTION MATERIALS
JOB 030656	CARGO PREFERENCE ACT REQUIREMENTS
JOB 030656	COLD MILLING - COUNTY PROPERTY
JOB 030656	DOCUMENTATION OF PAYMENTS MADE TO DISADVANTAGED BUSINESS ENTERPRISES
JOB 030656	DOCUMENTATION OF PAYMENTS MADE TO SUBCONTRACTORS
JOB 030656	ELECTRONIC TICKETING SYSTEMS (E-TICKETING)
JOB 030656	ENHANCED THERMOPLASTIC PAVEMENT MARKING
JOB 030656	LIQUIDATED DAMAGES PROCEDURE FOR BID LETTINGS
JOB 030656	MAINTENANCE OF TRAFFIC
JOB 030656	MANDATORY ELECTRONIC CONTRACT
JOB 030656	MANDATORY ELECTRONIC DOCUMENT SUBMITTAL
JOB 030656	MOBILE SPEED NOTIFICATION SYSTEM (SPECIAL)
JOB 030656	PRICE ADJUSTMENT FOR ASPHALT BINDER
JOB 030656	PRICE ADJUSTMENT FOR FUEL
JOB 030656	PROHIBITION OF CERTAIN TELECOMMUNICATIONS AND VIDEO SURVEILLANCE SERVICES OR EQUIPMENT
JOB 030656	ROADWAY CONSTRUCTION CONTROL - PAVEMENT PRESERVATION
JOB 030656	SEQUENCE OF CONSTRUCTION
JOB 030656	SUBMISSION OF ASPHALT CONCRETE HOT MIX ACCEPTANCE TEST RESULTS
JOB 030656	TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
JOB 030656	ULTRATHIN BONDED WEARING COURSE
JOB 030656	WARM MIX ASPHALT
JOB 030656	WORK ZONE LAW ENFORCEMENT

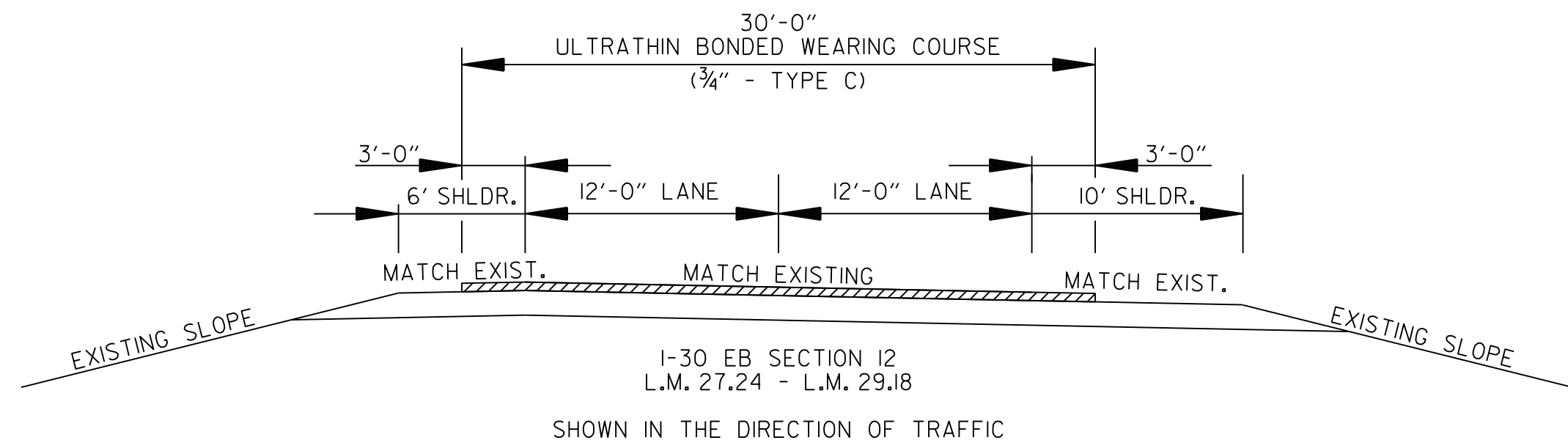
GENERAL NOTES

1. ALL PIPE LINES, POWER, TELEPHONE, AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY THE RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH OWNERS.
2. ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
3. ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
4. THE SEQUENCE AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS IS A GENERAL OUTLINE FOR THE CONSTRUCTION OF THIS PROJECT, AND IN NO WAY IS IT INTENDED TO COVER EVERY ITEM IN THE PROJECT. ITEMS NOT CRITICAL TO THE CONSTRUCTION SEQUENCE MAY BE CONSTRUCTED IN ANY STAGE AS APPROVED BY THE RESIDENT ENGINEER.

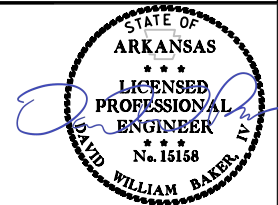
DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	030656	4	12
TYPICAL SECTIONS OF IMPROVEMENT						



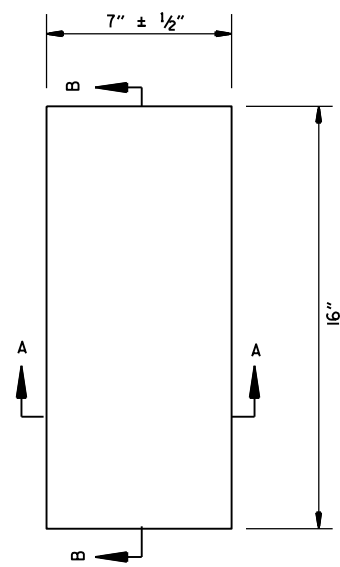
12/02/2025



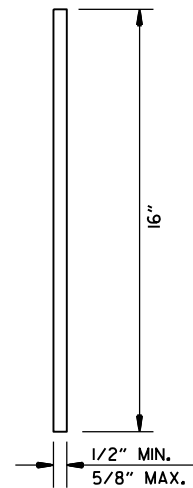
DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	030656	5	12
SPECIAL DETAILS						



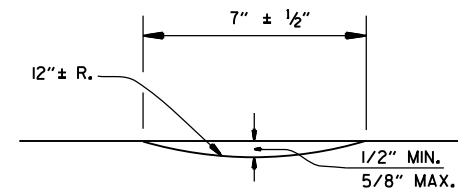
12/02/2025



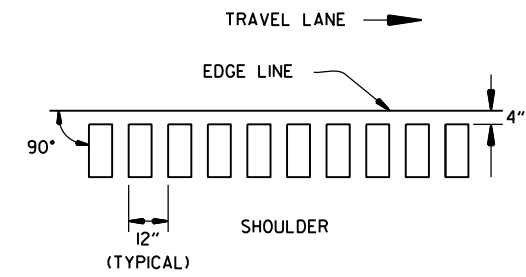
PLAN



SECTION B-B

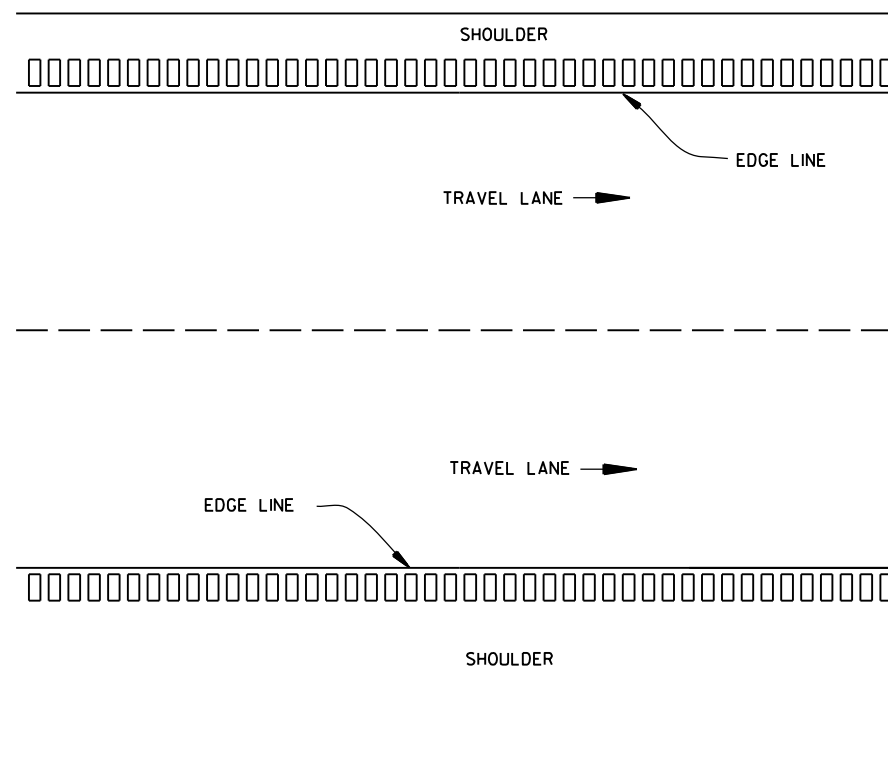


SECTION A-A



LOCATION PLAN OF RUMBLE STRIPS
LEFT OR RIGHT SHOULDER

DETAILS OF RUMBLE STRIPS

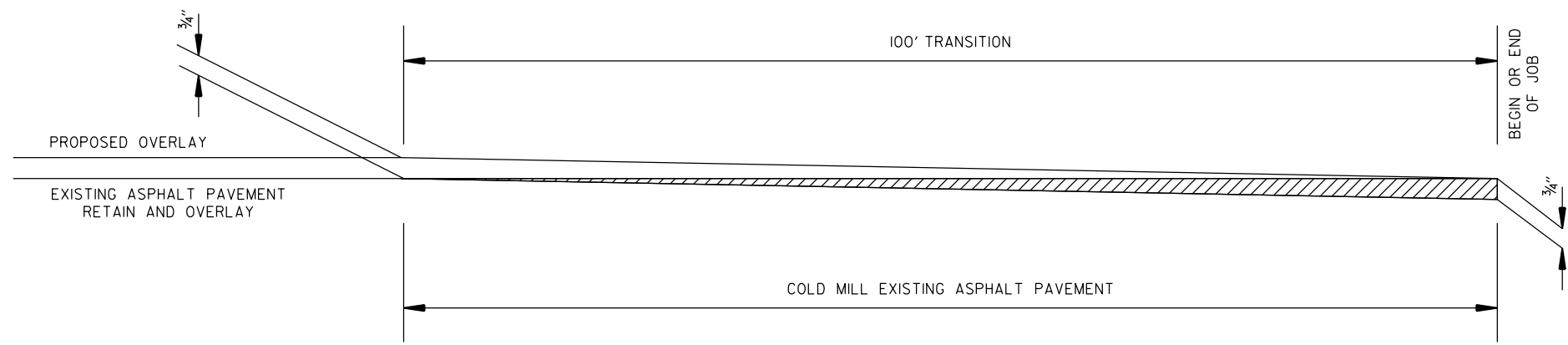


PLAN VIEW

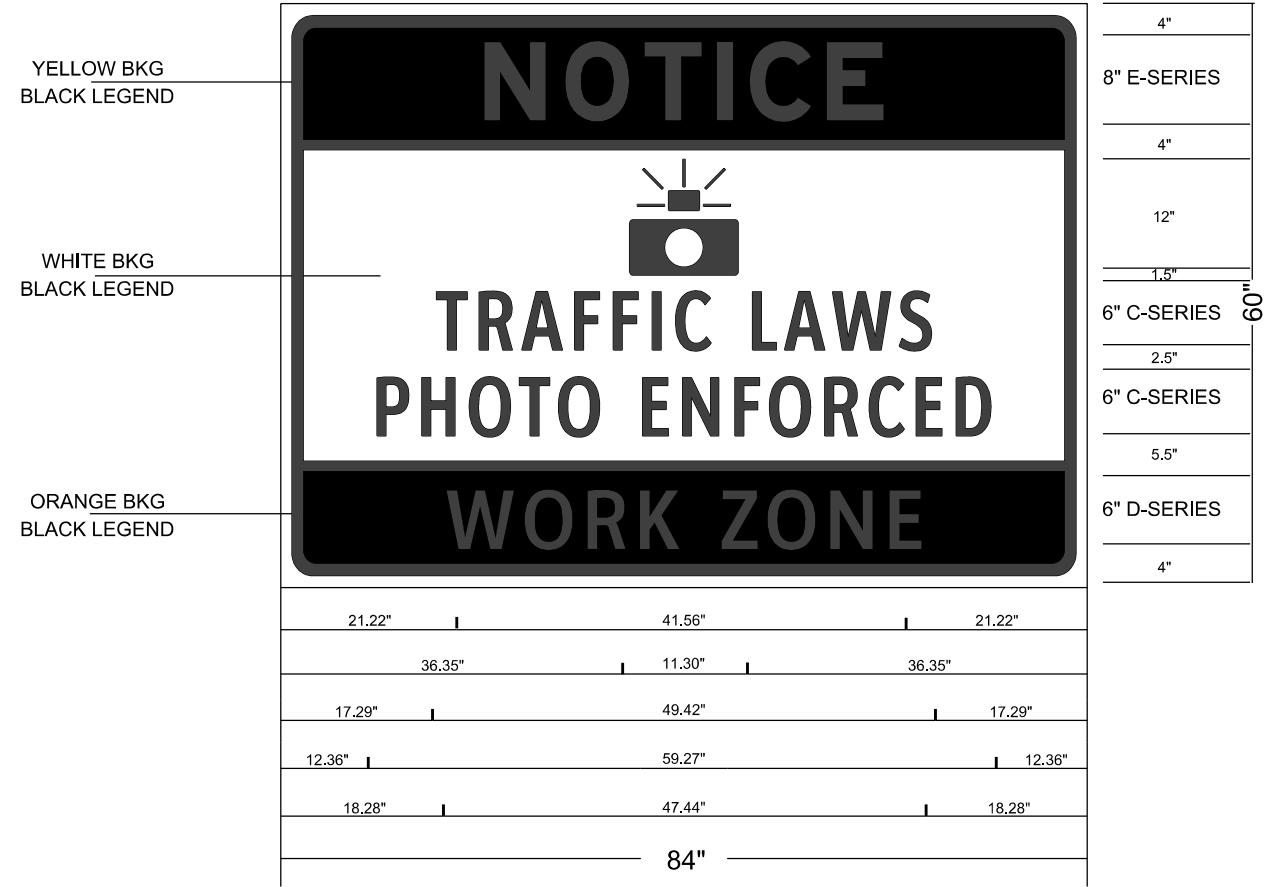
NOTES:

1. ALIGNMENT OF RUMBLE STRIPS SHALL GENERALLY BE STRAIGHT AND OFFSET APPROXIMATELY 4" FROM THE OUTER EDGE OF THE EDGE LINE. THIS OFFSET MAY BE ADJUSTED TO ACCOMMODATE VARIATIONS IN THE EDGE LINE.
2. THE 1/2" DEPTH SHALL GENERALLY APPLY FOR THE ENTIRE 16" LENGTH. SOME VARIATION TO SUIT SHOULDER SLOPE BREAKS MAY BE NECESSARY.
3. RUMBLE STRIPS SHALL NOT BE INSTALLED ON BRIDGE DECKS, APPROACH SLABS, OR ACROSS TRANSVERSE JOINTS OF CONCRETE SHOULDERS.

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	030656	6	12
SPECIAL DETAILS						



DETAIL FOR TRANSITION - MAIN LANES



WZ-1 (INTERSTATE) SIGN

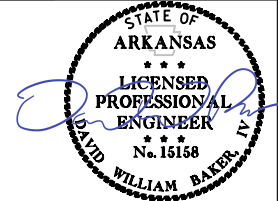
YELLOW BKG
BLACK LEGEND

WHITE BKG
BLACK LEGEND

ORANGE BKG
BLACK LEGEND

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	030656	7	12

MAINTENANCE OF TRAFFIC DETAILS



12/02/2025

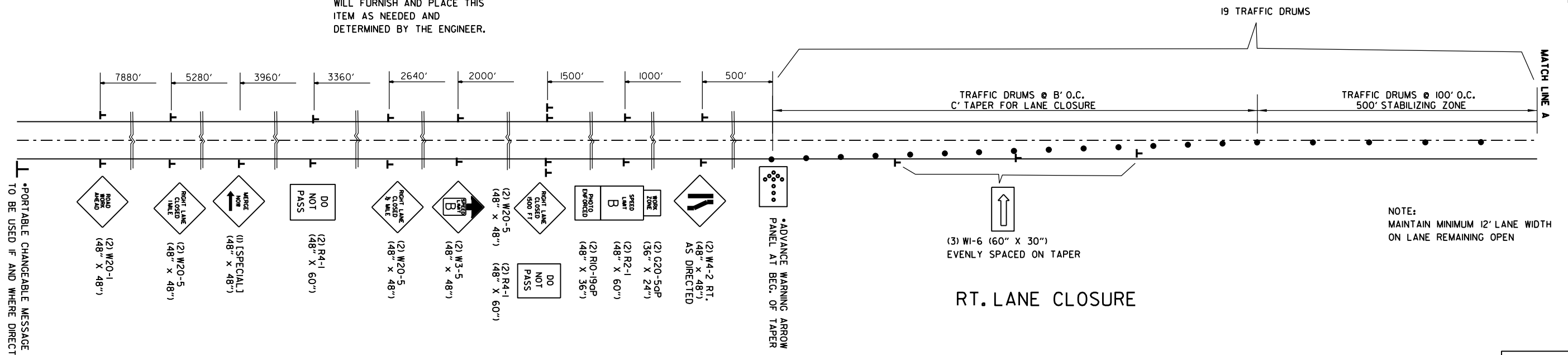
TABLE OF VARIABLES

DESIGN SPEED "A"	B	C	D	E
60	50	720	1100	600
65	55	780	1210	660
70	60	840	1320	720
75	65	900	1430	780

M.S. = MOBILE SPEED NOTIFICATION SYSTEM
N.S. =

P.R. = PHOTO RADAR TRAILER SYSTEM
T.S. =

•NOTE: IF NO PAY ITEM FOR A PHOTO RADAR TRAILER SYSTEM IS INCLUDED IN THE CONTRACT DOCUMENTS, ARDOT WILL FURNISH AND PLACE THIS ITEM AS NEEDED AND DETERMINED BY THE ENGINEER.



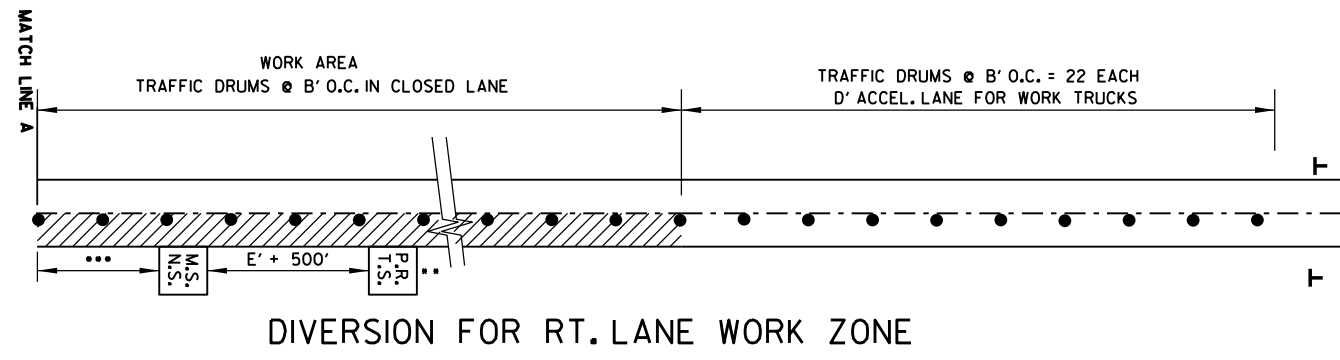
•PORTABLE CHANGEABLE MESSAGE SIGN TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER

•TO BE PLACED AT MINIMUM OFFSET OF 12' FROM EDGE OF THROUGH LANE OF TRAFFIC, OR FURTHER IF PRACTICAL.

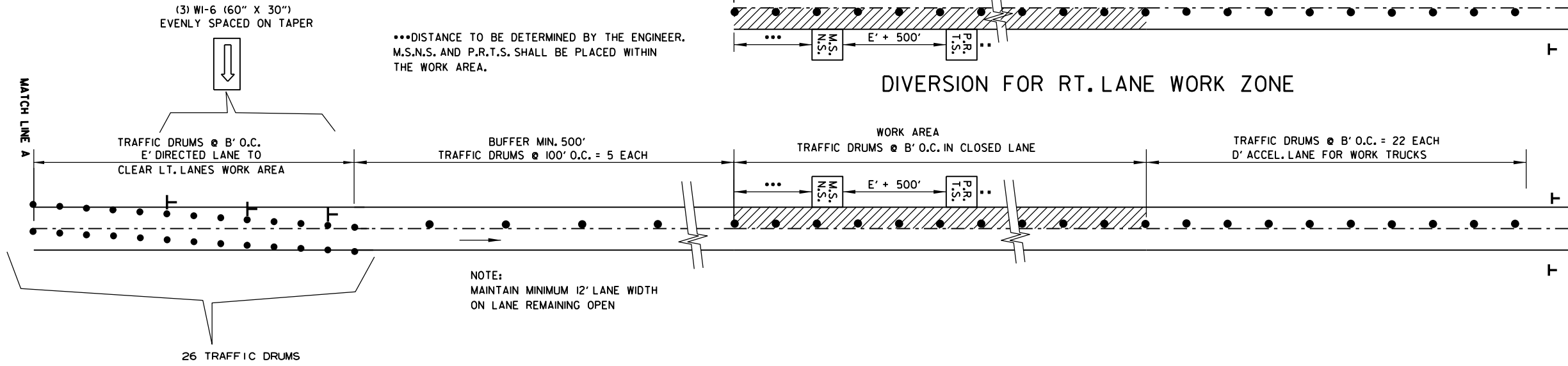
SPEED LIMIT SIGNS ARE ALSO PROVIDED FOR PLACEMENT PAST ENTRANCE RAMP WITHIN THE WORK ZONE.

•••DISTANCE TO BE DETERMINED BY THE ENGINEER. M.S.N.S. AND P.R.T.S. SHALL BE PLACED WITHIN THE WORK AREA.

NOTE: MAINTAIN MINIMUM 12' LANE WIDTH ON LANE REMAINING OPEN

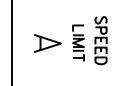


DIVERSION FOR RT. LANE WORK ZONE

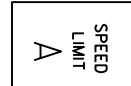


DIVERSION FOR LT. LANE WORK ZONE

LANE CLOSURES AND DIVERSIONS FOR 1-30, SECTION 12

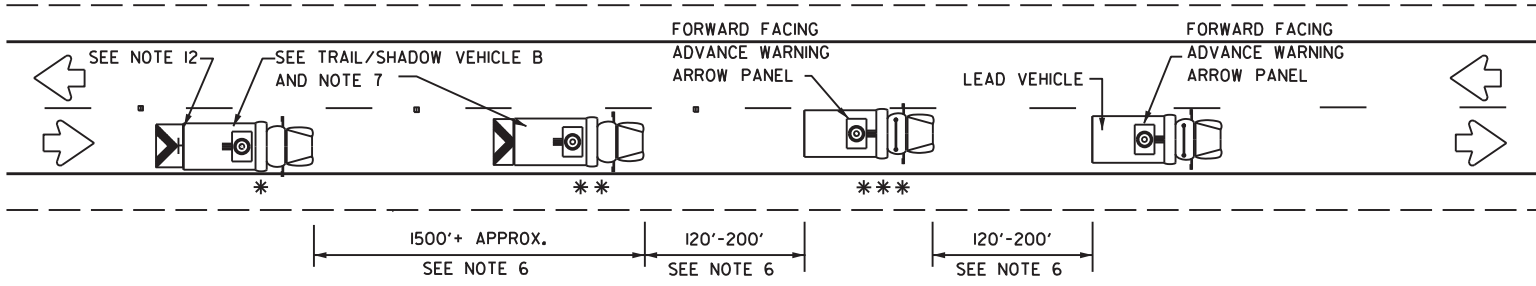
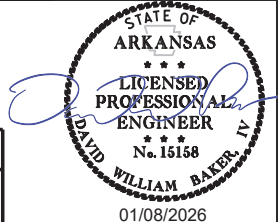


(2) R2-1 (48" X 60")

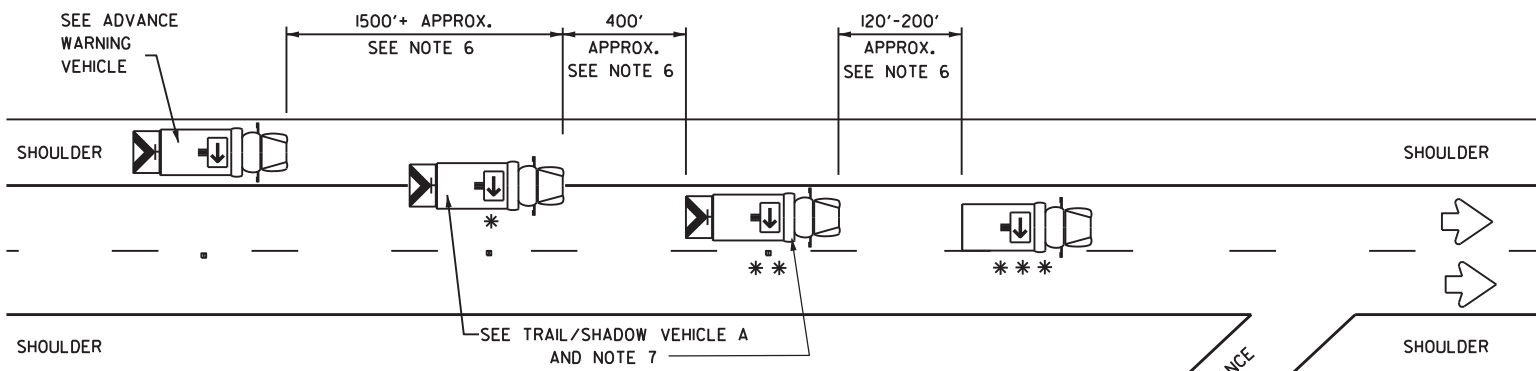


(2) R2-1 (48" X 60")

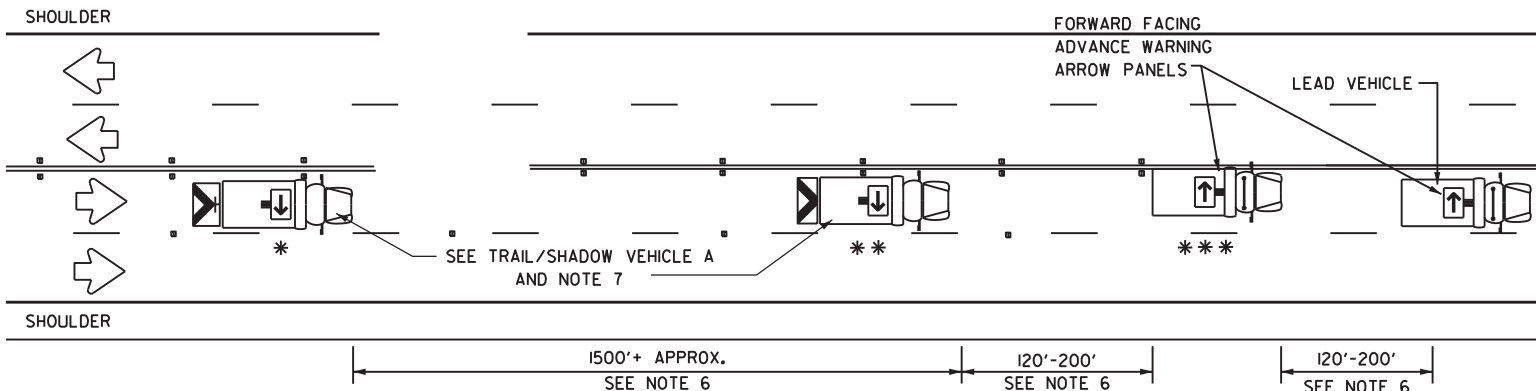
NOTE: MAINTAIN MINIMUM 12' LANE WIDTH ON LANE REMAINING OPEN



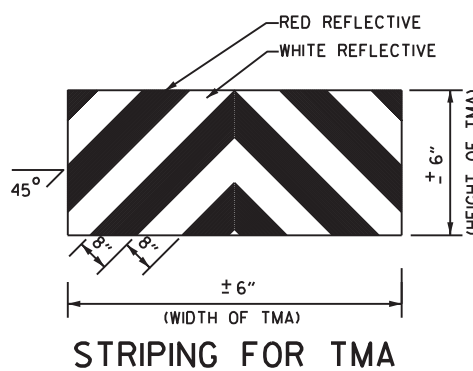
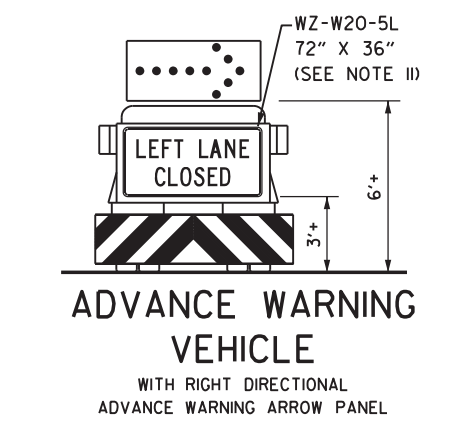
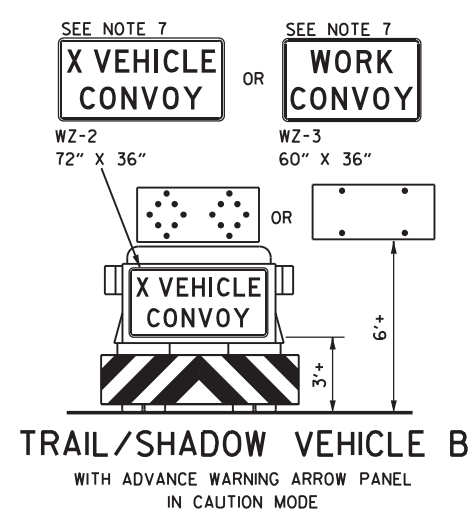
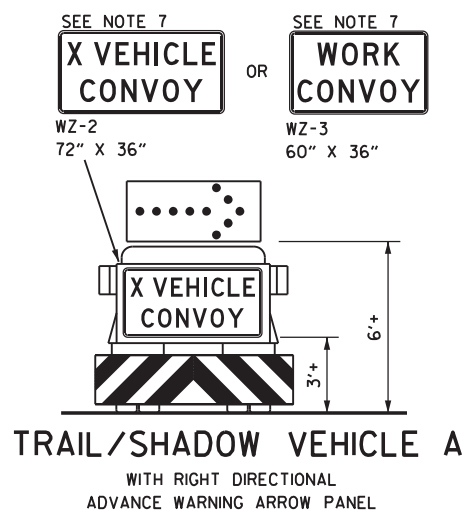
**TWO LANE HIGHWAY
(WORK ON TRAVEL LANE)**



DIVIDED MULTILANE HIGHWAY



UNDIVIDED MULTILANE HIGHWAY



LEGEND			
*	TRAIL VEHICLE	ADVANCE WARNING ARROW PANEL DISPLAY	
**	SHADOW VEHICLE		
***	WORK VEHICLE		RIGHT DIRECTIONAL
	HEAVY WORK VEHICLE		LEFT DIRECTIONAL
	TRUCK MOUNTED ATTENUATOR (TMA)		DOUBLE ARROW
	TRAFFIC FLOW		CAUTION (ALTERNATING DIAMOND OR 4 CORNER FLASH)

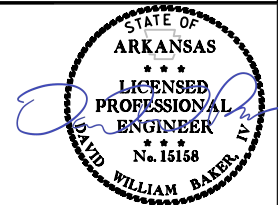
TYPICAL APPLICATION - MOBILE WORK ZONE

GENERAL NOTES:

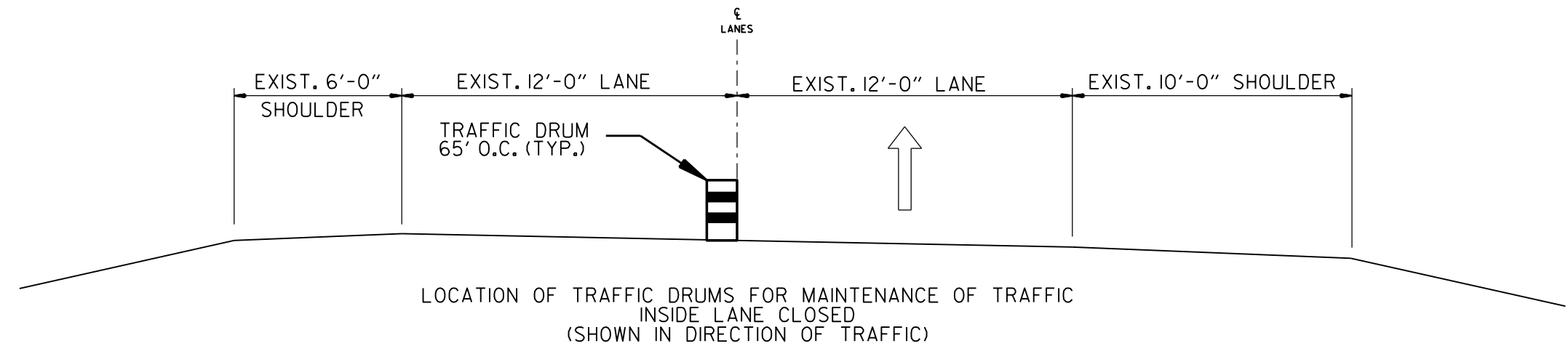
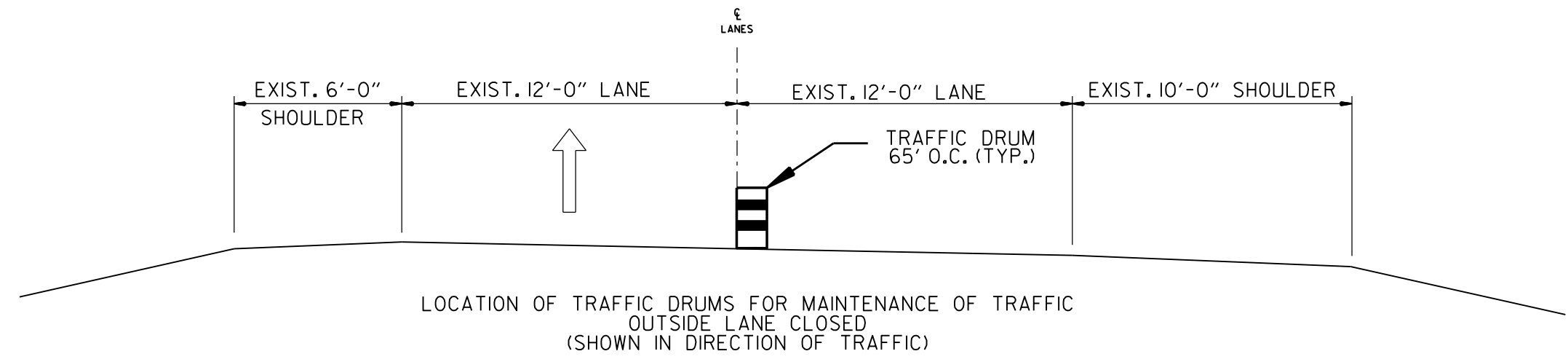
- TRAIL, SHADOW, AND LEAD VEHICLES SHALL BE EQUIPPED WITH ADVANCE WARNING ARROW PANELS AS ILLUSTRATED.
- THE USE OF AMBER OR GREEN HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS ON ALL VEHICLES ARE REQUIRED. VEHICLE HAZARD WARNING SIGNALS SHALL NOT BE USED INSTEAD OF THE VEHICLE'S HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.
- THE USE OF TRUCK MOUNTED ATTENUATORS (TMA) ON THE SHADOW VEHICLE, ADVANCE WARNING AND TRAIL VEHICLE ARE REQUIRED.
- EACH VEHICLE SHALL HAVE TWO-WAY RADIO COMMUNICATION CAPABILITY.
- WHEN WORK CONVOYS MUST CHANGE LANES, THE TRAIL VEHICLE SHOULD CHANGE LANES FIRST TO SHADOW THE OTHER CONVOY VEHICLES. WHEN WORK CONVOY EXITS THE ROADWAY, THE TRAIL VEHICLE SHOULD EXIT LAST.
- VEHICLE SPACING BETWEEN THE TRAIL VEHICLE AND THE SHADOW VEHICLE WILL VARY DEPENDING ON SIGHT DISTANCE RESTRICTIONS AND QUEUE LENGTHS. MOTORISTS APPROACHING THE CONVOY SHOULD BE ABLE TO SEE THE TRAIL VEHICLE IN TIME TO SLOW DOWN AND/OR CHANGE LANES AS THEY APPROACH THE TRAIL VEHICLE. VEHICLE SPACING BETWEEN THE WORK VEHICLE AND SHADOW VEHICLE AND VEHICLE SPACING BETWEEN THE WORK VEHICLE AND LEAD VEHICLE MAY VARY ACCORDING TO TERRAIN, WORK ACTIVITY AND OTHER FACTORS.
- X VEHICLE CONVOY (WZ-2) OR WORK CONVOY (WZ-3) SIGNS SHALL BE USED ON TRAIL VEHICLES AND SHADOW VEHICLES AS SHOWN. AS AN OPTION 48" X 48" DIAMOND SHAPED WORK CONVOY (WZ-2A) OR X VEHICLE CONVOY (WZ-3A) SIGNS MAY BE USED WHERE ADEQUATE MOUNTING SPACE EXISTS. WHEN USED, THE X VEHICLE CONVOY SIGN SHALL HAVE THE NUMBER OF THE CONVOY VEHICLES DISPLAYED ON THE SIGN IN THE NUMBER DESIGNATION "X" LOCATION. THE X VEHICLE CONVOY SIGN SHALL NOT BE USED ON THE SHADOW VEHICLE.
- FOR DIVIDED HIGHWAYS WITH TWO OR THREE LANES IN ONE DIRECTION, THE APPROPRIATE LEFT LANE CLOSED (WZ-W20-5L), RIGHT LANE CLOSED (WZ-W20-5R), OR CENTER LANE CLOSED (WZ-W20-5C) SIGN SHOULD BE USED ON THE ADVANCE WARNING VEHICLE. AS AN OPTION, A PORTABLE CHANGEABLE MESSAGE SIGN (PCMS) OR TRUCK MOUNTED CHANGEABLE MESSAGE SIGN (TMCMS) WITH A MINIMUM CHARACTER HEIGHT OF 12", AND DISPLAYING THE SAME LEGEND MAY BE SUBSTITUTED FOR THESE SIGNS. AN APPROPRIATE DIRECTIONAL ARROW DISPLAY, SIMULATING THE SIZE AND LEGIBILITY OF THE ADVANCE WARNING ARROW PANEL MAY BE USED IN THE SECOND PHASE OF THE PCMS/TMCMS MESSAGE. WHEN THIS IS DONE, THE ARROW BOARD WILL NOT BE REQUIRED ON THE ADVANCE WARNING VEHICLE.
- A DOUBLE ARROW SHALL NOT BE DISPLAYED ON THE ADVANCE WARNING ARROW PANEL ON THE ADVANCE WARNING VEHICLE.
- STANDARD DIAMOND SHAPE VERSIONS OF THE WZ SERIES SIGNS MAY BE USED AS AN OPTION IF THE RECTANGULAR SIGNS SHOWN ARE NOT AVAILABLE.
- THE ADVANCE WARNING VEHICLE MAY STRADDLE THE EDGELINE WHEN SHOULDER WIDTH MAKES IT NECESSARY.
- ON TWO-LANE TWO-WAY ROADWAYS, THE WORK AND PROTECTION VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW MOTOR VEHICLE TRAFFIC TO PASS. A DO NOT PASS (R4-1) SIGN SHALL BE PLACED ON THE BACK OF THE REARMOST PROTECTION VEHICLE.
- MOBILE WORK ZONE METHODS AS DEPICTED IN THIS STANDARD DRAWING SHALL BE UTILIZED ON ALL PROJECTS THAT INCLUDE INSTALLATION OR REMOVAL OF RAISED PAVEMENT MARKERS OR PAVEMENT MARKINGS UNLESS APPROVED BY THE ENGINEER ON PROJECTS LESS THAN 0.5 MILE IN LENGTH.
- NO DIRECT PAYMENT WILL BE MADE FOR COMPLIANCE WITH THE METHODS DEPICTED IN THIS STANDARD DRAWING (WHICH INCLUDES ALL SIGNS, DEVICES, MATERIALS, LABOR, TOOLS, EQUIPMENT, AND INCIDENTALS NECESSARY TO COMPLY) BUT SHALL BE CONSIDERED SUBSIDIARY TO THE MAINTENANCE OF TRAFFIC PAY ITEM.

030656 SPECIAL DETAILS.DGN CREATED: 6/18/2025 SAVED: 1/7/2026

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	030656	8	12
MAINTENANCE OF TRAFFIC DETAILS						



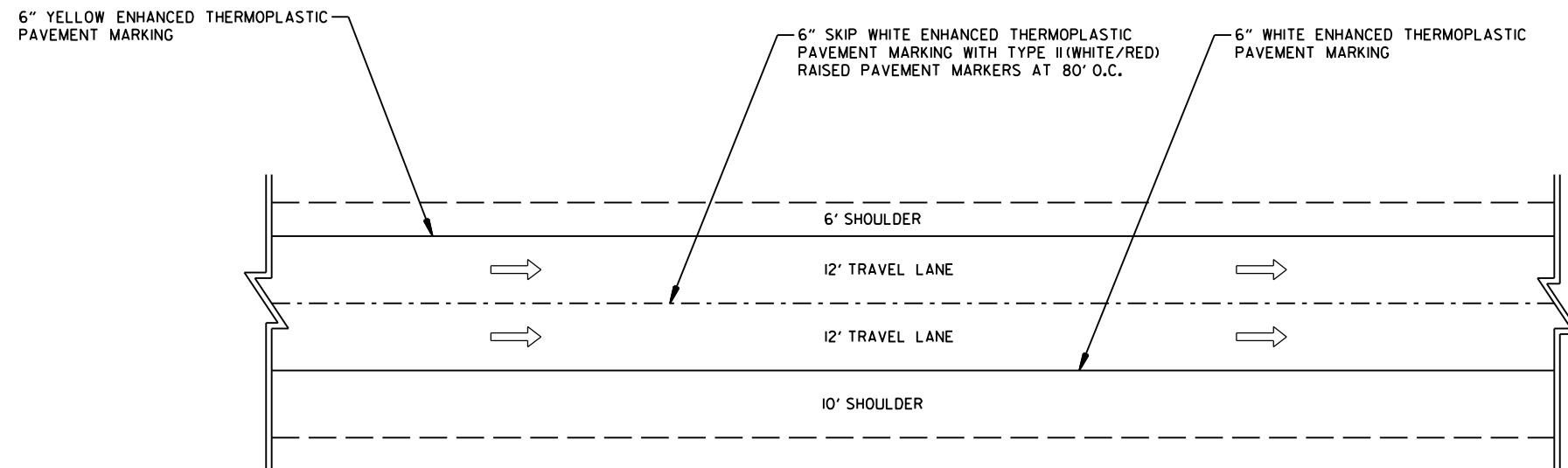
12/02/2025



DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	030656	9	12
PERMANENT PAVEMENT MARKING DETAILS						

PERMANENT PAVEMENT MARKINGS

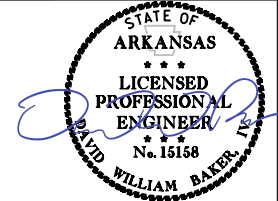
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) AT 80' O.C. = 129 EA.
 YELLOW ENHANCED THERMOPLASTIC PAVEMENT MARKING = 10243 LIN. FT.
 WHITE ENHANCED THERMOPLASTIC PAVEMENT MARKING = 12804 LIN. FT.



TYPICAL PERMANENT PAVEMENT MARKING LAYOUT

030656 PERMANENT PAVEMENT MARKING DETAILS.DGN CREATED: 9/9/2025 9:00:00 AM SAVED: 12/1/2025 10:00:00 AM

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	030656	10	12
QUANTITIES						



ADVANCE WARNING SIGNS AND DEVICES

SIGN NUMBER	DESCRIPTION	SIGN SIZE	HEMPSTEAD COUNTY, I-30, SEC. 12- L.M. 27.24-29.18	MAXIMUM NUMBER REQUIRED	TOTAL SIGNS REQUIRED		TRAFFIC DRUMS	* ADVANCE WARNING ARROW PANEL	* PORTABLE CHANGEABLE MESSAGE SIGN	MOBILE SPEED NOTIFICATION SYSTEM (SPECIAL)
			EACH		NO.	SQ. FT.				
W20-1	ROAD WORK 1 MILE	48"x48"	2	2	2	32.0				
W20-1	ROAD WORK 1/2 MILE	48"x48"	2	2	2	32.0				
W20-1	ROAD WORK 1500 FT.	48"x48"	2	2	2	32.0				
W20-1	ROAD WORK AHEAD	48"x48"	2	2	2	32.0				
G20-2	END ROAD WORK	48"x24"	2	2	2	16.0				
G20-1	ROAD WORK NEXT 2 MILES	60"x24"	2	2	2	20.0				
G20-5aP	WORK ZONE	36"x24"	2	2	2	12.0				
W21-5a	SHOULDER CLOSED	48"x48"	1	1	1	16.0				
W20-5	RIGHT LANE CLOSED 1 MILE	48"x48"	2	2	2	32.00				
SPECIAL	MERGE NOW WITH ARROW	48"x48"	1	1	1	16.00				
R4-1	DO NOT PASS	48"x60"	4	4	4	80.00				
W20-5	RIGHT LANE CLOSED 1/2 MILE	48"x48"	2	2	2	32.00				
W3-5	SPEED LIMIT 65 MPH AHEAD	48"x48"	2	2	2	32.00				
W20-5	RIGHT LANE CLOSED 1500 FT.	48"x48"	2	2	2	32.00				
R2-1	SPEED LIMIT 65 MPH	48"x60"	2	2	2	40.00				
W4-2R	RIGHT LANE CLOSED	48"x48"	2	2	2	32.00				
W1-6	DIRECTIONAL ARROW	60"x30"	6	6	6	75.00				
R2-1	SPEED LIMIT 75 MPH	48"x60"	2	2	2	40.00				
R2-6aP	FINES DOUBLE WHEN WORKERS ARE PRESENT	48"x36"	2	2	2	24.00				
R2-11	END HIGHER FINES ZONE	36"x48"	2	2	2	24.00				
WZ-1	TRAFFIC LAWS PHOTO ENFORCED WORK ZONE	84"x60"	2	2	2	70.00				
R10-19aP	PHOTO ENFORCED	48"x36"	2	2	2	24.00				
	TRAFFIC DRUMS		230	230			230			
	ADVANCE WARNING ARROW PANEL		25	25			25			
	PORTABLE CHANGEABLE MESSAGE SIGN		5	5				5		
	MOBILE SPEED NOTIFICATION SYSTEM (SPECIAL)		1	1					1	
TOTALS:						745.0	230	25	5	1

NOTE: SEE ULTRATHIN BONDED WEARING COURSE QUANTITY BOX FOR TRAFFIC VOLUME CLASSIFICATION AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

NOTE: THE QUANTITY OF TRAFFIC DRUMS PROVIDED IS FOR ONE SIDE OF THE ROADWAY FOR THE FULL LENGTH OF JOB. HOWEVER, THE INSTALLATION OF TRAFFIC DRUMS SHALL NEVER EXCEED THE ACTUAL WORK AREA BY MORE THAN 1/4 MILE, UNLESS APPROVED BY THE ENGINEER.

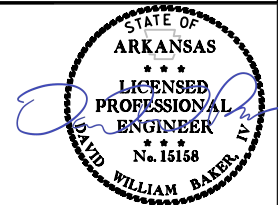
* QUANTITY ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.
TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.

RUMBLE STRIPS IN ASPHALT SHOULDERS

COUNTY	ROUTE	SECTION	LOG MILE	LOG MILE	* RUMBLE STRIPS IN ASPHALT SHOULDERS
					LIN.FT.
HEMPSTEAD	I-30 EB	12	27.24	29.18	20486
TOTAL:					20486

* QUANTITY ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.
TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	030656	11	12
QUANTITIES						



12/02/2025

ULTRATHIN BONDED WEARING COURSE

COUNTY	ROUTE	SECTION	LOG MILE	LOG MILE	TOTAL LENGTH	AVG. WIDTH	ULTRATHIN BONDED WEARING COURSE (3/4" - TYPE C)	TRAFFIC VOLUME TYPE	PAVEMENT MARKING TYPE
					FEET	FEET			
HEMPSTEAD	I-30 EB	12	27.24	29.18	10243.20	30.00	1349	HIGH	ENHD.
TOTAL:							1349		

NOTE: ENHD. = ENHANCED THERMOPLASTIC PAVEMENT MARKING

CONSTRUCTION PAVEMENT MARKINGS AND PERMANENT PAVEMENT MARKINGS

DESCRIPTION	HEMPSTEAD COUNTY, I-30, SEC. 12-L.M. 27.24-29.18	CONSTRUCTION PAVEMENT MARKINGS	RAISED PAVEMENT MARKERS	ENHANCED THERMOPLASTIC PAVEMENT MARKING	
			TYPE II (WHITE/RED) EACH	6"	
				WHITE	YELLOW
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED)	129		129		
CONSTRUCTION PAVEMENT MARKINGS	23047	23047			
ENHANCED THERMOPLASTIC PAVEMENT MARKING WHITE (6")	12804			12804	
ENHANCED THERMOPLASTIC PAVEMENT MARKING YELLOW (6")	10243				10243
TOTALS:		23047	129	12804	10243

NOTE: SEE ULTRATHIN BONDED WEARING COURSE QUANTITY BOX FOR TRAFFIC VOLUME CLASSIFICATION AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

COLD MILLING ASPHALT PAVEMENT

LOG MILE	LOG MILE	LOCATION	TOTAL LENGTH	AVG. WIDTH	COLD MILLING ASPHALT PAVEMENT
			FEET	FEET	SQ. YD.
27.24	27.26	I-30 EB LANES TRANSITION	100.00	30.00	333.33
29.16	29.18	I-30 EB LANES TRANSITION	100.00	30.00	333.33
TOTAL:					666.66

NOTE: COORDINATE COLD MILLING STOCKPILE LOCATIONS WITH DISTRICT ENGINEER. STOCKPILE LOCATIONS SHALL BE NO FURTHER THAN FIVE MILES FROM EACH SITE.

ACHM PATCHING OF EXISTING ROADWAY

DESCRIPTION	TON
ENTIRE PROJECT - TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER	50
TOTAL:	50

NOTE: QUANTITY ESTIMATED. SEE SECTION 104.03 OF THE STD. SPECS.

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
1-7-26		6	ARK.	030656	12	12
SUMMARY OF QUNATITES AND REVISIONS						



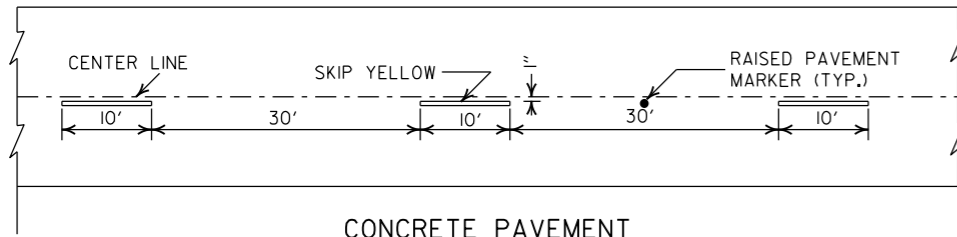
Digitally signed by Baker, David W
Date: 2026.01.08 07:22:04-06'00'

SUMMARY OF QUANTITIES

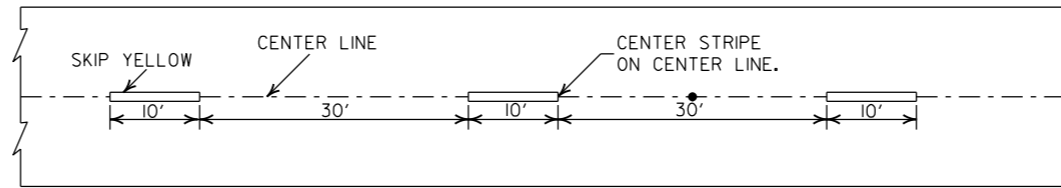
ITEM NUMBER	ITEM	QUANTITY	UNIT
SP	ULTRATHIN BONDED WEARING COURSE (3/4"-TYPE C)	1349	TON
SP & 412	COLD MILLING ASPHALT PAVEMENT	667	SQ. YD.
SP, SS, & 415	ACHM PATCHING OF EXISTING ROADWAY	50	TON
601	MOBILIZATION	1.00	LUMP SUM
SP, SS, & 603	MAINTENANCE OF TRAFFIC	1.00	LUMP SUM
SS & 604	SIGNS	745	SQ. FT.
SS & 604	TRAFFIC DRUMS	230	EACH
604	CONSTRUCTION PAVEMENT MARKINGS	23047	LIN. FT.
SS & 604	ADVANCE WARNING ARROW PANEL	25	DAY
SP, SS, & 604	PORTABLE CHANGEABLE MESSAGE SIGN	5	WEEK
SP	MOBILE SPEED NOTIFICATION SYSTEM (SPECIAL)	1	EACH
SP & 635	ROADWAY CONSTRUCTION CONTROL	1.00	LUMP SUM
642	RUMBLE STRIPS IN ASPHALT SHOULDERS	20486	LIN. FT.
SP	ENHANCED THERMOPLASTIC PAVEMENT MARKING WHITE (6")	12804	LIN. FT.
SP	ENHANCED THERMOPLASTIC PAVEMENT MARKING YELLOW (6")	10243	LIN. FT.
721	RAISED PAVEMENT MARKERS (TYPE II)	129	EACH

REVISIONS

DATE	REVISION	SHEET NUMBER
1/7/2026	ADDED MOBILE WORK ZONE DETAIL	7A, 12

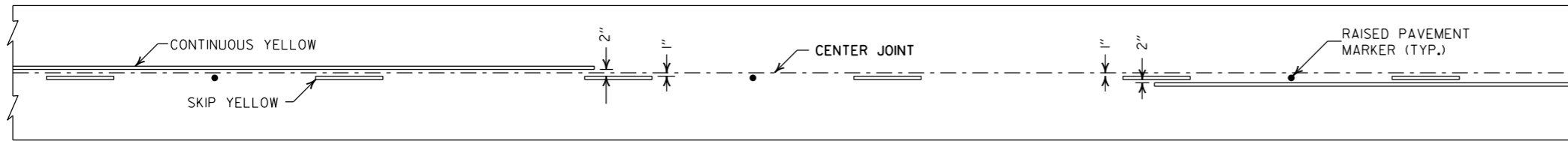


CONCRETE PAVEMENT

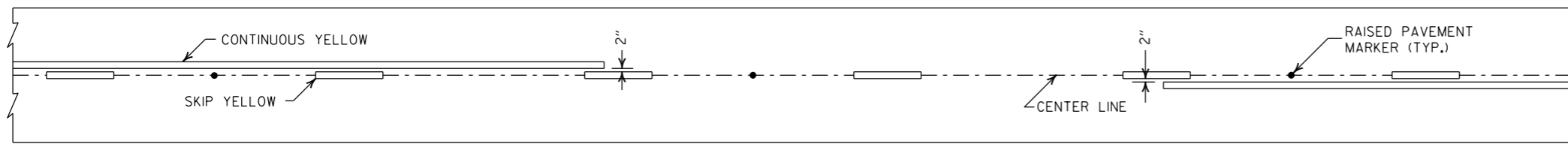


ASPHALT PAVEMENT

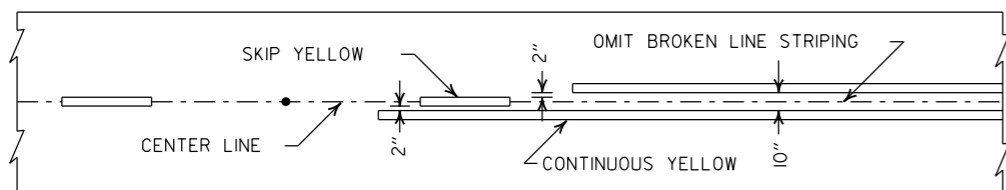
BROKEN LINE STRIPING



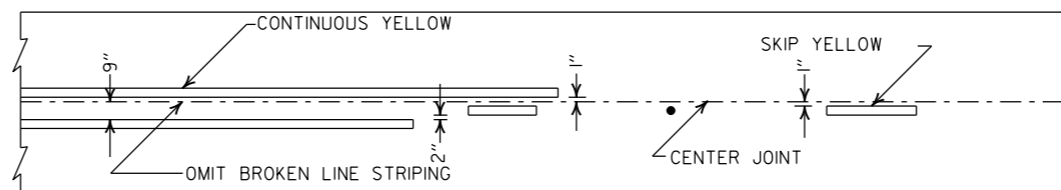
SOLID LINE STRIPING ON CONCRETE PAVEMENT



SOLID LINE STRIPING ON ASPHALT PAVEMENT

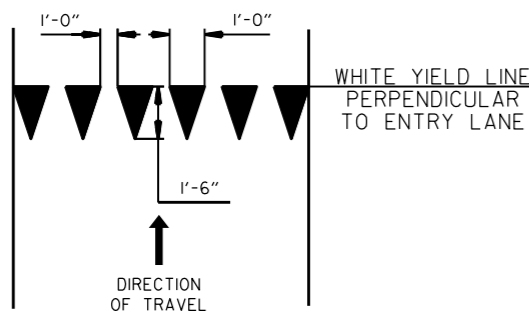


ASPHALT PAVEMENT

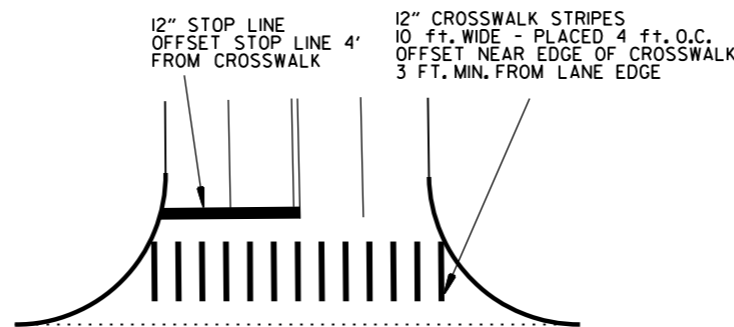


CONCRETE PAVEMENT

STRIPING AT ADJACENT NO PASSING LANES

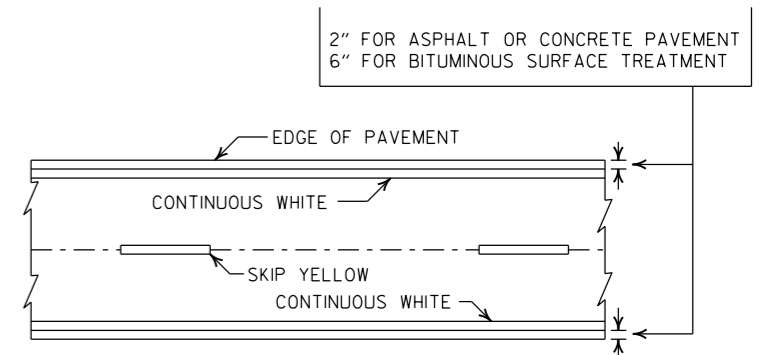


YIELD LINE DETAIL

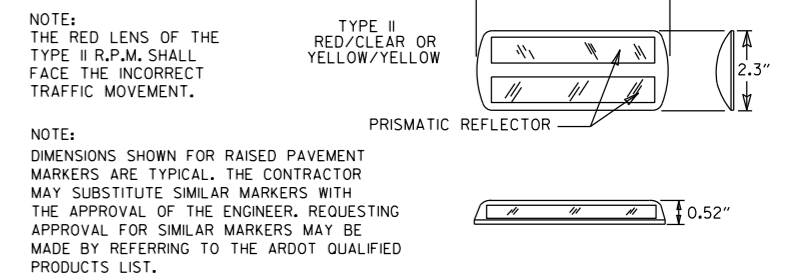


CROSSWALK AND STOP LINE DETAILS

- NOTES:
1. REFER TO THE STRIPING DETAILS FOR PAVEMENT MARKING LINE WIDTHS.
 2. THIS DRAWING SHALL BE USED IN CONJUNCTION WITH THE LATEST REVISED ADDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."
 3. RAISED PAVEMENT MARKERS SHALL BE PLACED ON AN 80 FEET SPACING UNLESS OTHERWISE SHOWN IN THE PLANS.



PAVEMENT EDGE LINE MARKING



DETAIL OF STANDARD RAISED PAVEMENT MARKERS

DATE	REVISION	FILMED
2-27-20	REVISED STOP LINE DETAILS	
6-1-17	ADDED YIELD LINE DETAIL	
5-12-16	REVISED LINE WIDTHS, SPACING, & NOTES	
9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
11-17-10	REVISED GENERAL NOTES & REMOVED PLOWABLE PVMT MRKRS	
11-18-04	REVISED NOTE 2 & GENERAL NOTES	
8-22-02	ADDED CROSSWALK & STOPBAR DTL.	
7-02-98	ADDED DETAILS OF STD. RAISED PAV'T. MARKERS	
4-26-96	REV. NOTES 3&4; ADDED R.P.M.	
9-30-80	DRAWN	1-9-30-80

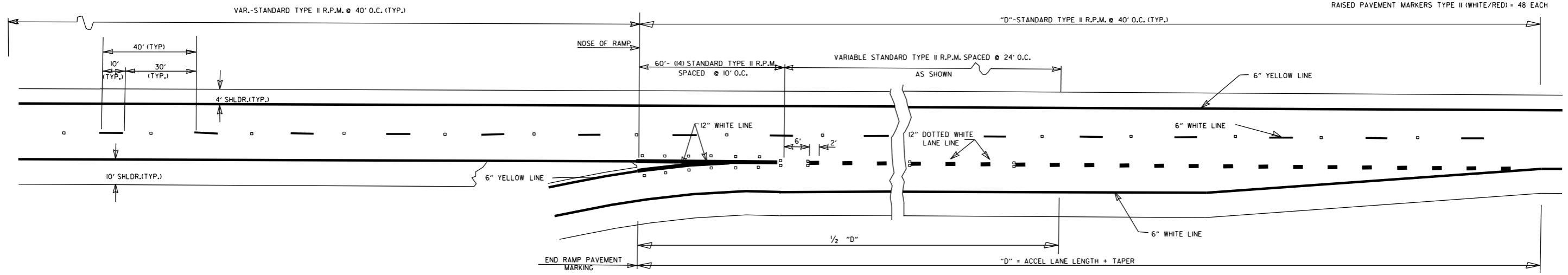
ARKANSAS STATE HIGHWAY COMMISSION

PAVEMENT MARKING DETAILS

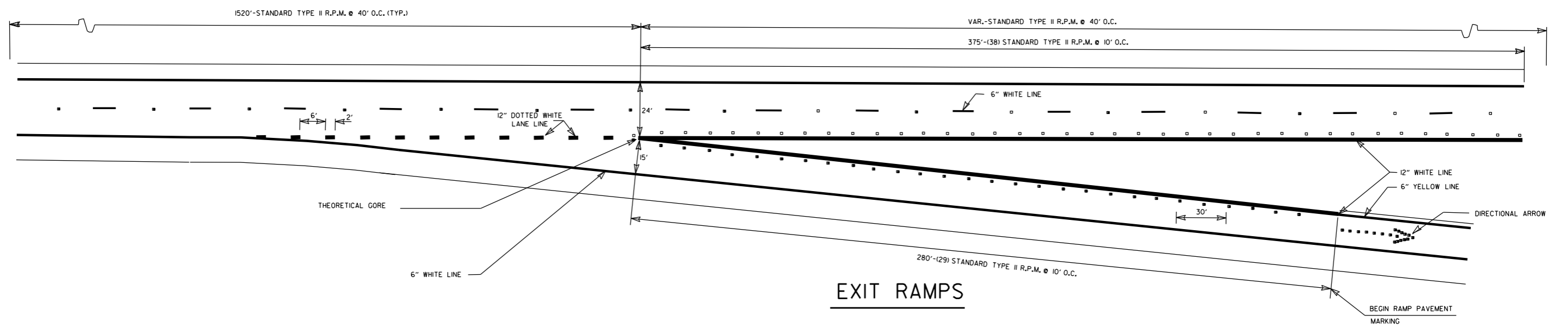
STANDARD DRAWING PM-1

ENTRANCE RAMP
12" WHITE = 370 LIN. FT.
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH

EXIT RAMP
6" WHITE = 280 LIN. FT.
12" WHITE = 815 LIN. FT.
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 48 EACH



ENTRANCE RAMPS

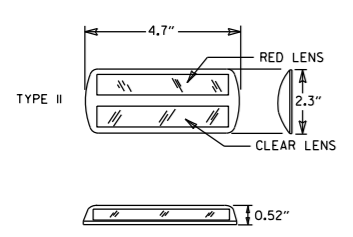


EXIT RAMPS

GENERAL NOTES:
THIS DRAWING SHOULD BE CONSIDERED AS TYPICAL ONLY AND THE FINAL LOCATION OF THE STRIPING AND PAVEMENT MARKERS SHALL BE DETERMINED BY THE ENGINEER.

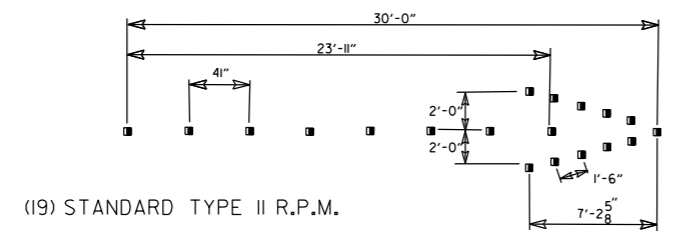
THIS DRAWING SHOULD BE USED IN CONJUNCTION WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", LATEST REVISION.

NOTE:
DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE ARDOT QUALIFIED PRODUCTS LIST.



DETAIL OF STANDARD RAISED PAVEMENT MARKERS

NOTE:
THE RED LENS OF THE TYPE II R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.




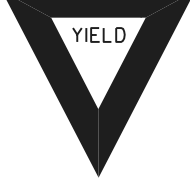





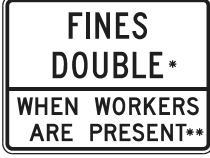


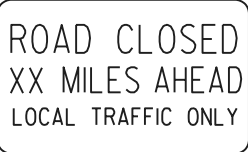


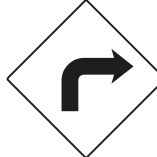








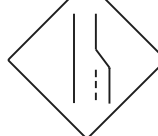




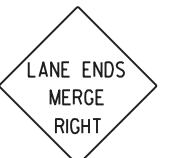














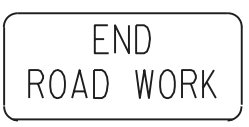
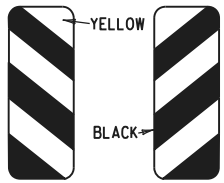


(19) STANDARD TYPE II R.P.M. DIRECTIONAL ARROWS

DATE	REVISION	FILMED
05-14-20	REMOVED CROSSHATCH MARKINGS ON EXIT RAMPS	
11-07-19	REVISED DOTTED PAV'T MARKINGS; ADDED CROSSHATCH MARKINGS ON EXIT RAMPS	
12-8-16	REVISED RAISED PAV'T MARKERS FOR 80' SPACING; REVISED WIDTH OF STRIPING	
9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
7-26-12	REVISED RPM NOTATION	
12-15-11	REVISED RPMs ACCORDING TO LATEST POLICY	
11-17-10	REMOVED PLOWABLE PAVEMENT MARKERS	
6-3-10	REVISED PER 2009 MUTCD	
11-18-04	REVISED NOTES	
8-22-02	ADDED & REVISED NOTES; REV. ENTRANCE & EXIT RAMPS	
5-18-00	REMOVED HASHMARKS	
7-02-98	CHANGED TYPES TO ROMAN NUMERALS	
4-26-96	ADDED DIMENSIONS & QUANTITIES; REVISED LANE WIDTH ON EXIT RAMP	
2-2-95	PLACED IN USE	2-2-95
		FILMED

ARKANSAS STATE HIGHWAY COMMISSION

PAVEMENT MARKING DETAILS ON ACCESS CONTROLLED ROADWAYS


STANDARD DRAWING PM-2

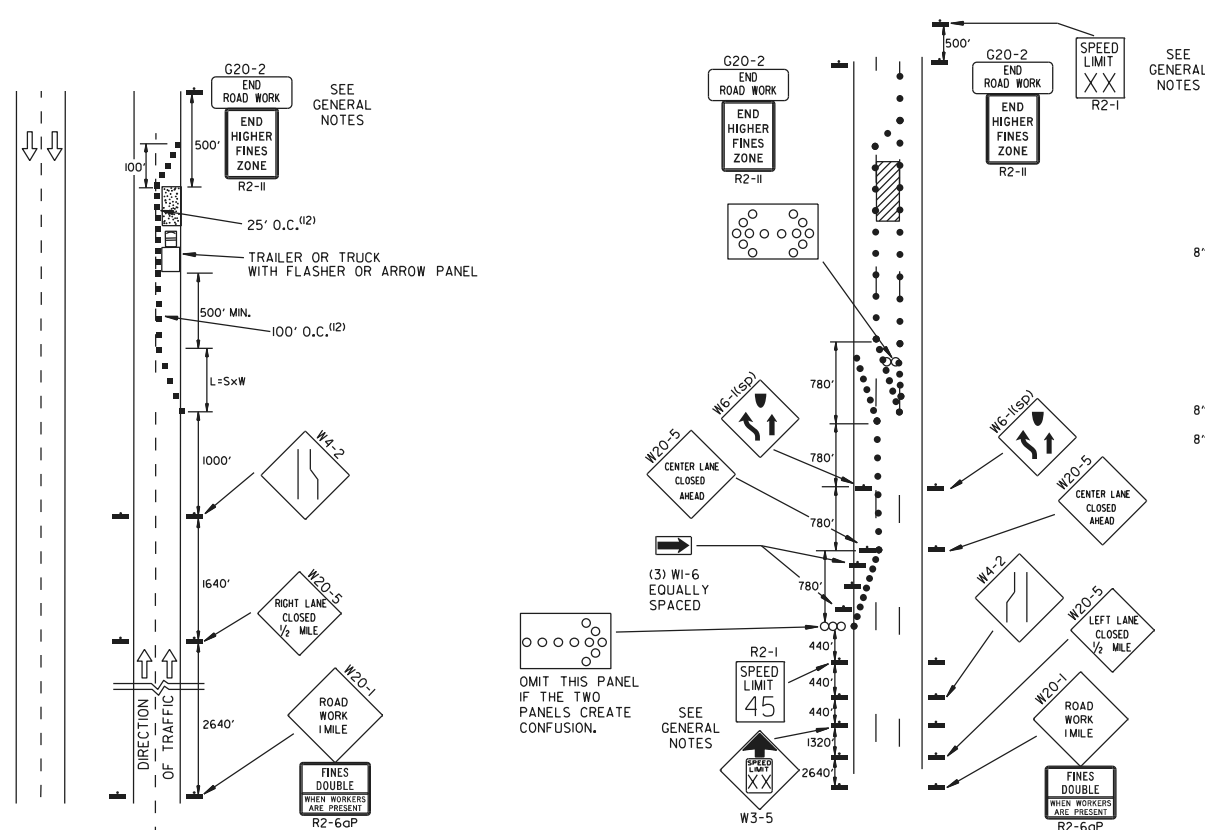
<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>W3-5</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>W3-5a</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R2-6aP</p>  <p>48"x36" *USE 6" C LETTERS **USE 4" D LETTERS</p>
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>W21-5a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>WI-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>WI-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>R2-II</p>  <p>36"x48"</p>
<p>WI-3</p>  <p>STD. 48"x48"</p>	<p>WI-4</p>  <p>STD. 48"x48"</p>	<p>WI-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>WI-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>R56-1</p>  <p>STD. 18"x18"</p>
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>	<p>W20-3</p>  <p>STD. 48"x48"</p>
<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>18" 500 FEET W6-2 24"</p> <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>	<p>WI-4b</p>  <p>STD. 48"x48"</p>	
<p>W8-II</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>	<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>	

GENERAL NOTES:

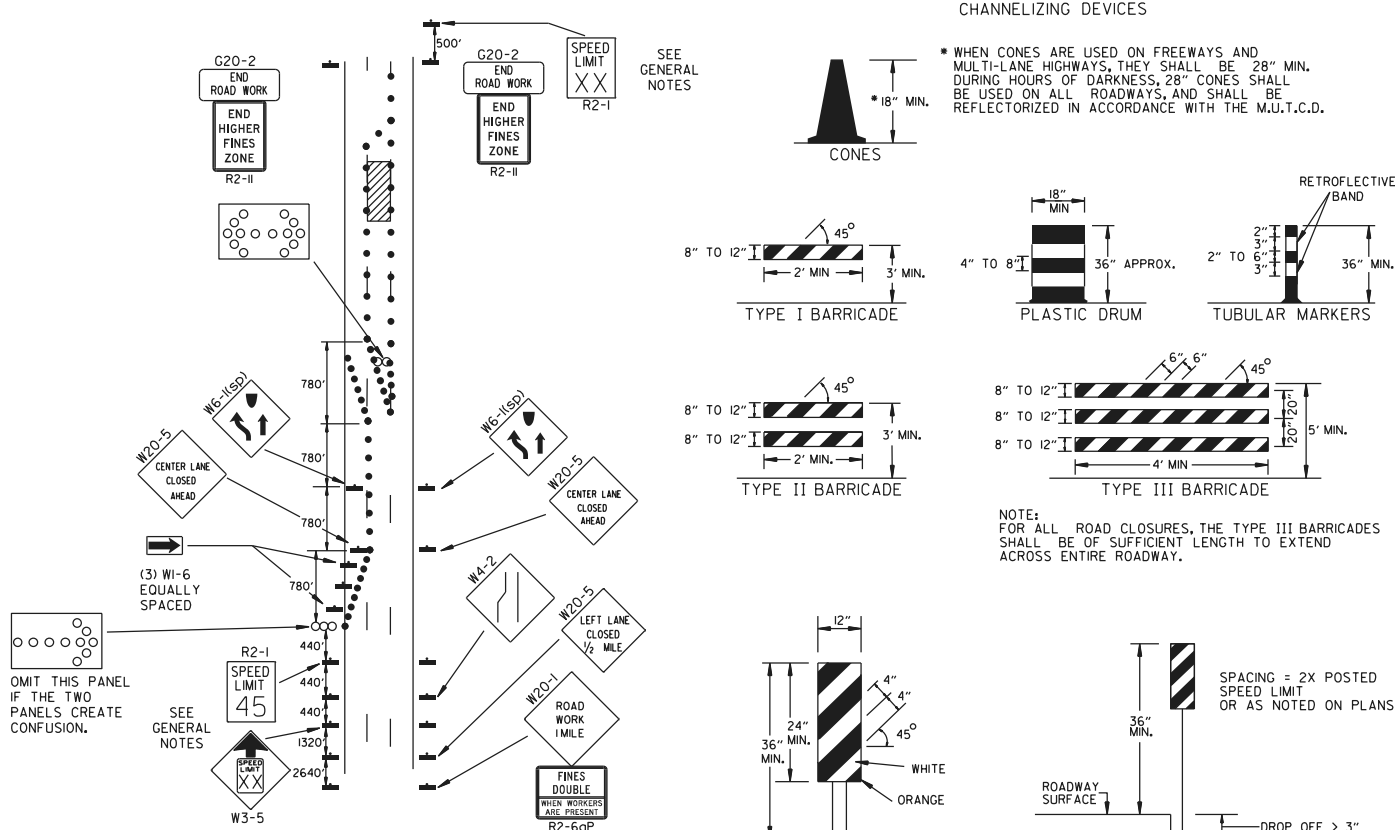
- ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.
- TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.
- EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.
- SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SO. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE.
- SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.
- POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.
- ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE, EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.
- FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
- MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.
- R2-6aP PLAQUES SHALL BE MOUNTED BELOW AN APPLICABLE ADVANCE WARNING SIGN THAT IS LOCATED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE PLAQUE SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE W3-5 "REDUCED SPEED AHEAD" SIGN. R2-II SIGNS SHALL BE INSTALLED AT OR NEAR THE DOWNSTREAM END OF THE WORK ZONE. SEE STANDARD DRAWINGS TC-2, TC-3, AND TC-6 FOR TYPICAL PLACEMENT LOCATIONS.

*NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.

		<p>ARKANSAS STATE HIGHWAY COMMISSION</p>	
<p>STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION</p>			
<p>REPLACED R55-1 WITH R2-6aP, ADDED R2-11 & REVISED NOTE 10</p>	<p>REVISED FOR MASH</p>	<p>DELETED RSP-1 & ADDED W21-5a</p>	<p>REVISED REDUCED SPEED SIGN TO SHOW NEXT SIX MILES</p>
<p>08-14-25</p>	<p>11-07-19</p>	<p>04-13-17</p>	<p>09-02-15</p>
<p>DATE</p>	<p>REV</p>	<p>DATE</p>	<p>REV</p>
<p>08-14-2025</p>	<p>STANDARD DRAWING</p>	<p>TC-1</p>	<p>DESCRIPTION</p>



(A) TYPICAL APPLICATION - DAYTIME MAINTENANCE OPERATIONS OF SHORT DURATION ON A 4-LANE DIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.



(B) TYPICAL APPLICATION - 3-LANE ONEWAY ROADWAY WHERE CENTER LANE IS CLOSED.

KEY:

○ ○ ○ ○ ○ ARROW PANEL (IF REQUIRED)

■ CHANNELIZING DEVICE

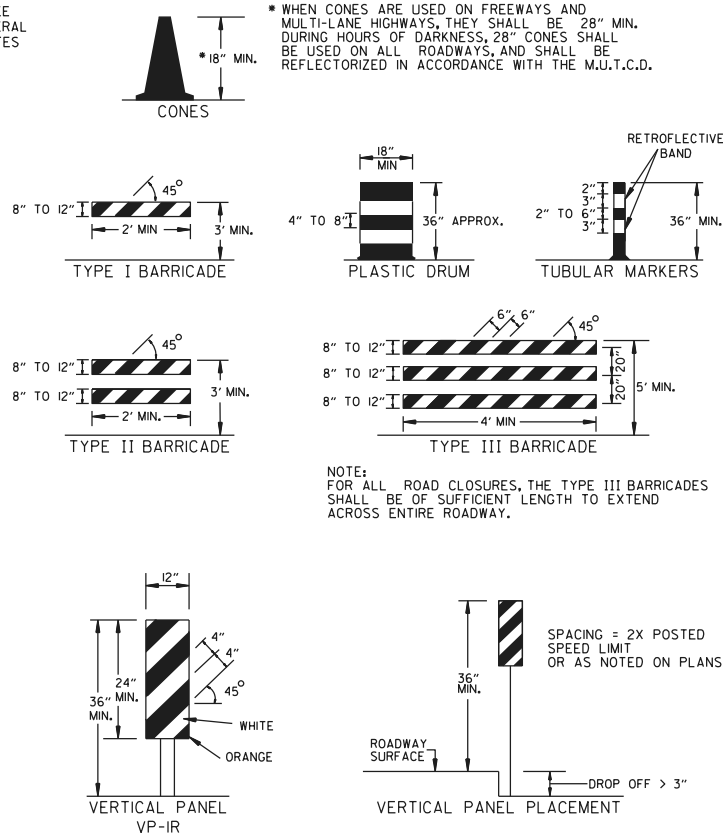
● TRAFFIC DRUM

GENERAL NOTES:

- A SPEED LIMIT REDUCTION MAY BE IMPLEMENTED ONLY WHEN DESIGNATED IN THE PLAN OR WHEN RECOMMENDED BY THE ROADWAY DESIGN DIVISION.
- WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-1(55) SHALL BE OMITTED AND THE W3-5 SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-1(45)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/2 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
- WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-1(65) SHALL BE OMITTED. ADDITIONAL R2-1(55)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/2 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
- THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT OR AS DIRECTED BY THE ENGINEER.
- WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
- PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
- THE G20-1 SIGN SHOULD BE INSTALLED ON JOBS OF OVER TWO MILES IN LENGTH. WHEN THE LANE CLOSURE IS NOT AT THE BEGINNING OF THE PROJECT, THE G20-1 SIGN SHALL BE ERECTED 125' IN ADVANCE OF THE JOB LIMIT. ADDITIONAL W20-1(1/2 MILE) SIGNS ARE NOT REQUIRED IN ADVANCE OF LANE CLOSURES THAT BEGIN INSIDE THE PROJECT LIMITS. THE DISTANCE DISPLAYED ON THE G20-1 SIGN SHALL BE STATED TO THE NEAREST WHOLE MILE.
- FLAGGERS SHALL USE STOP/SLOW PADDLES FOR CONTROLLING TRAFFIC THROUGH WORK ZONES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
- ALL PLASTIC DRUMS AND CONES SHALL MEET THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).
- TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUOUS MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE. PAYMENT FOR TRAFFIC DRUMS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR VARIOUS TRAILER MOUNTED DEVICES.
- ALL TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL MEET THE REQUIREMENTS OF THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).
- ONLY WHERE SPACE RESTRICTIONS DO NOT ALLOW FOR TRAFFIC DRUMS, TUBULAR MARKERS MAY BE USED AT 50' O.C. IN STABILIZATION ZONES AND AT 10' O.C. DIRECTLY ADJACENT TO CONSTRUCTION OPERATIONS AND AT EXIT TAPERS. TUBULAR MARKERS SHALL BE STABILIZED WITH WEIGHTED BASES IN ACCORDANCE WITH THE M.U.T.C.D.

(C) TYPICAL APPLICATION - CONSTRUCTION OPERATIONS OF INTERMEDIATE TO LONG TERM DURATION ON A 4-LANE DIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.

CHANNELIZING DEVICES



* WHEN CONES ARE USED ON FREEWAYS AND MULTI-LANE HIGHWAYS, THEY SHALL BE 28" MIN. DURING HOURS OF DARKNESS, 28" CONES SHALL BE USED ON ALL ROADWAYS, AND SHALL BE REFLECTORIZED IN ACCORDANCE WITH THE M.U.T.C.D.

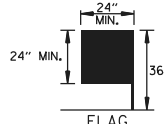
NOTE: FOR ALL ROAD CLOSURES, THE TYPE III BARRICADES SHALL BE OF SUFFICIENT LENGTH TO EXTEND ACROSS ENTIRE ROADWAY.

SPACING ≥ 2X POSTED SPEED LIMIT OR AS NOTED ON PLANS

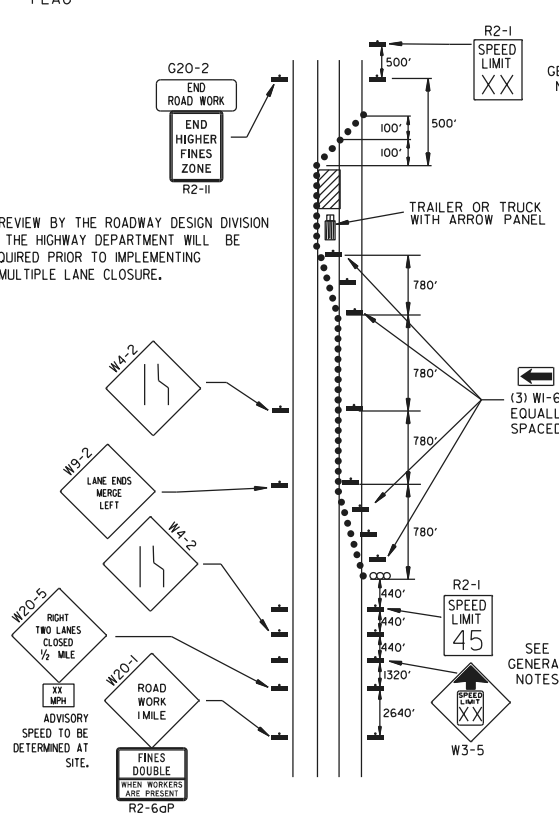
ROADWAY SURFACE

DROP OFF > 3"

FLAG SHALL BE OF GOOD GRADE RED MATERIAL



A REVIEW BY THE ROADWAY DESIGN DIVISION OF THE HIGHWAY DEPARTMENT WILL BE REQUIRED PRIOR TO IMPLEMENTING A MULTIPLE LANE CLOSURE.



(D) TYPICAL APPLICATION - CLOSING MULTIPLE LANES OF A MULTILANE HIGHWAY.

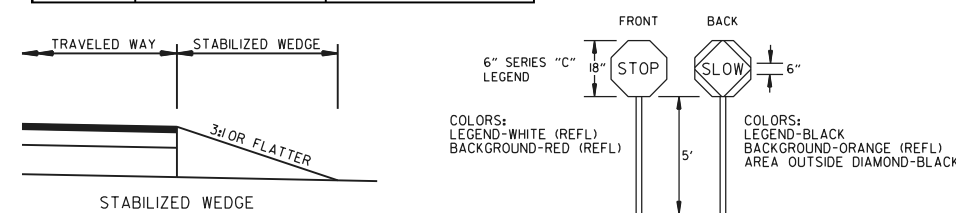
TRAFFIC CONTROL DEVICES

VERTICAL DIFFERENTIAL	LOCATION	NON-INTERSTATE	
		TRAFFIC CONTROL	
		≤ 45 MPH	> 45 MPH
≤ 1"	CENTERLINE	W8-11	W8-11
> 1"	CENTERLINE	W8-11 AND CENTERLINE LANE STRIPING	W8-11 AND CENTERLINE LANE STRIPING
> 3"	CENTERLINE	STANDARD LANE CLOSURE ⁽¹⁾	STANDARD LANE CLOSURE ⁽¹⁾
≤ 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-9 AND TRAFFIC DRUMS ⁽¹⁾	W8-9 AND TRAFFIC DRUMS ⁽¹⁾
> 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾
> 6"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾
> 18"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾	A STABILIZED WEDGE, W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾
> 24"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	PRECAST CONCRETE BARRIER ⁽¹⁾ & EDGE LINES	PRECAST CONCRETE BARRIER ⁽¹⁾ & EDGE LINES

VERTICAL DIFFERENTIAL	LOCATION	INTERSTATE	
		TRAFFIC CONTROL	
		≤ 3"	> 3"
≤ 3"	CENTERLINE	W8-11 AND LANE STRIPING	W8-11 AND LANE STRIPING
≤ 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-9, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾	W8-9, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾
> 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾
> 6"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	PRECAST CONCRETE BARRIER & EDGE LINES	PRECAST CONCRETE BARRIER & EDGE LINES

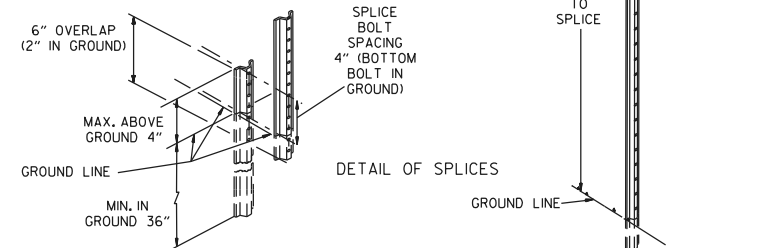
INTERSTATE AND NON-INTERSTATE		
FORESLOPE	HEIGHT	TRAFFIC CONTROL
1:1	> 2 FT	PRECAST CONCRETE BARRIER
2:1	≤ 5 FT	TRAFFIC DRUMS
2:1	> 5 FT	PRECAST CONCRETE BARRIER
Flatter than 2:1	N/A	TRAFFIC DRUMS

- GENERAL NOTES:
- WHEN THE SHOULDER AREA IS USED AS PART OF THE TRAVELED LANE AND THERE IS INSUFFICIENT WIDTH TO PLACE TRAFFIC DRUMS ON THE REMAINING SHOULDER WIDTH, THEN VERTICAL PANELS SHALL BE USED.
 - WHEN THERE IS INSUFFICIENT WIDTH TO PLACE TRAFFIC DRUMS ON THE REMAINING SHOULDER WIDTH, A STABILIZED WEDGE SHALL BE USED. PRECAST CONCRETE BARRIER WALL CAN BE USED IN LIEU OF A STABILIZED WEDGE, W8-17 SIGN, EDGE LINE STRIPING, AND TRAFFIC DRUMS. IF AND WHERE DIRECTED BY THE ENGINEER, A STABILIZED WEDGE, W8-17 SIGN, EDGE LINE STRIPING, AND TRAFFIC DRUMS CAN BE USED IN LIEU OF PRECAST CONCRETE BARRIER WALL. IF AND WHERE DIRECTED BY THE ENGINEER, W21-5, W21-5a, AND/OR W21-5b SIGNS SHALL BE USED WHERE THE ROADWAY IS UNOBSTRUCTED IF AND WHERE DIRECTED BY THE ENGINEER. TIME LIMITATIONS MUST CONFORM TO SECTION 603 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION).



NOTE: MATERIALS FOR THE STABILIZED WEDGE SHALL MEET THE REQUIREMENTS PROVIDED IN SECTION 603.02 OF THE STANDARD SPECIFICATIONS.

- NOTES:
- USE SPLICES ONLY WHEN NECESSARY FOR INSTALLATION. TYPICAL INSTALLATION SHOULD HAVE NO SPLICES (SEE STD. DRAWING NO. SHS-2)
 - NORMAL INSTALLATIONS WILL REQUIRE 1/4" DIA. BOLTS TO MOUNT SIGNS TO POST AND 5/16" DIA. BOLTS TO ASSEMBLE THE VARIOUS POST SUPPORTS. EACH OF THESE BOLTS SHALL BE CARRIAGE BOLTS.
 - SIGN POSTS SHALL BE PAINTED GREEN; SIGNS SHALL NOT BE PAINTED, AND ALL SIGN POSTS SHALL BE PLUMB.



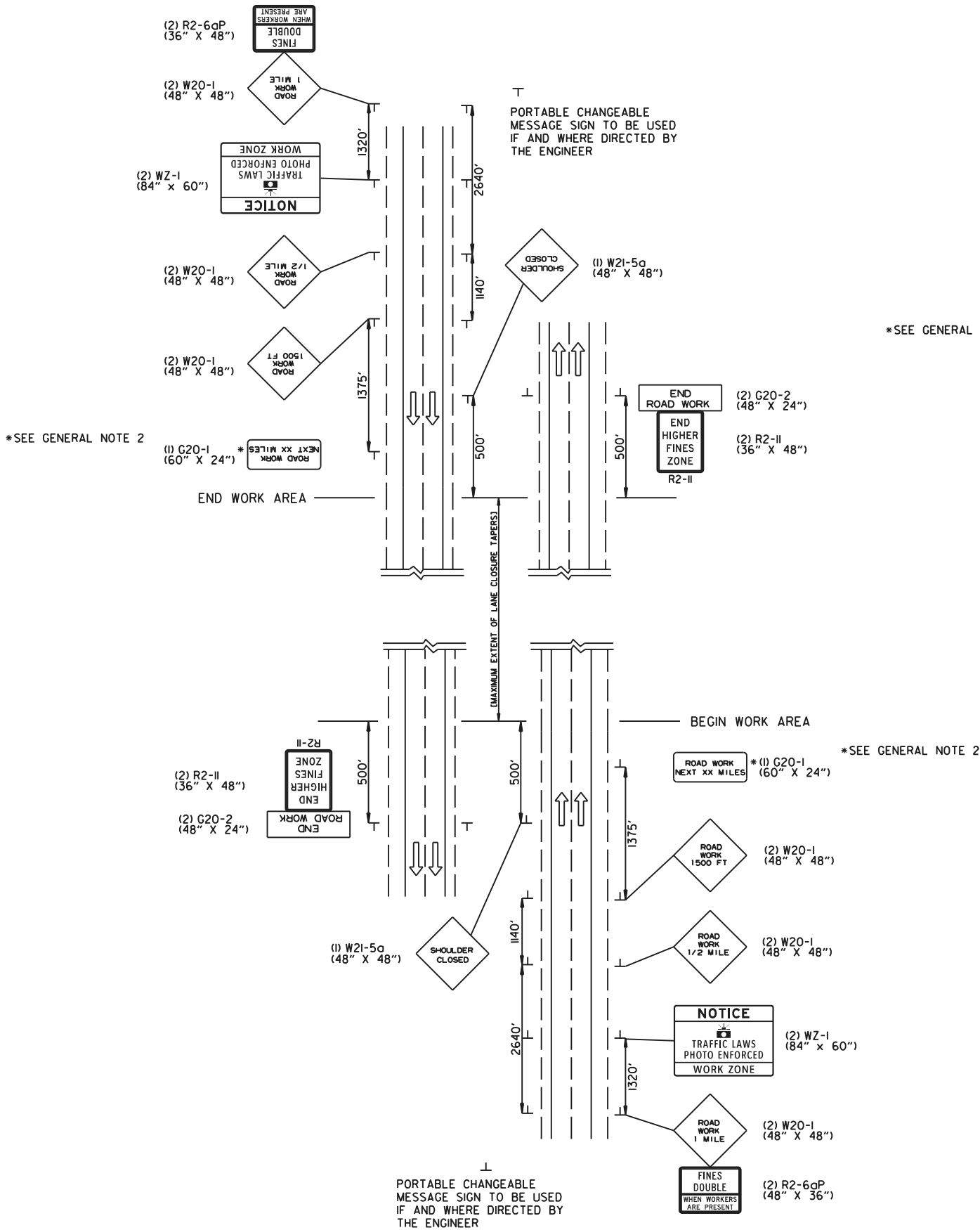
DATE	REV	DESCRIPTION
08-14-25	ADDED	R2-6aP AND R2-11
05-22-25	REVISED	TRAFFIC CONTROL DEVICES AND NOTES, ADDED NOTE 12
08-12-21	REVISED	TRAFFIC CONTROL DEVICES AND NOTES
05-20-21	REVISED	NOTE 10
02-27-20	REVISED	TRAFFIC CONTROL DEVICES DETAILS

ARKANSAS STATE HIGHWAY COMMISSION

STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION

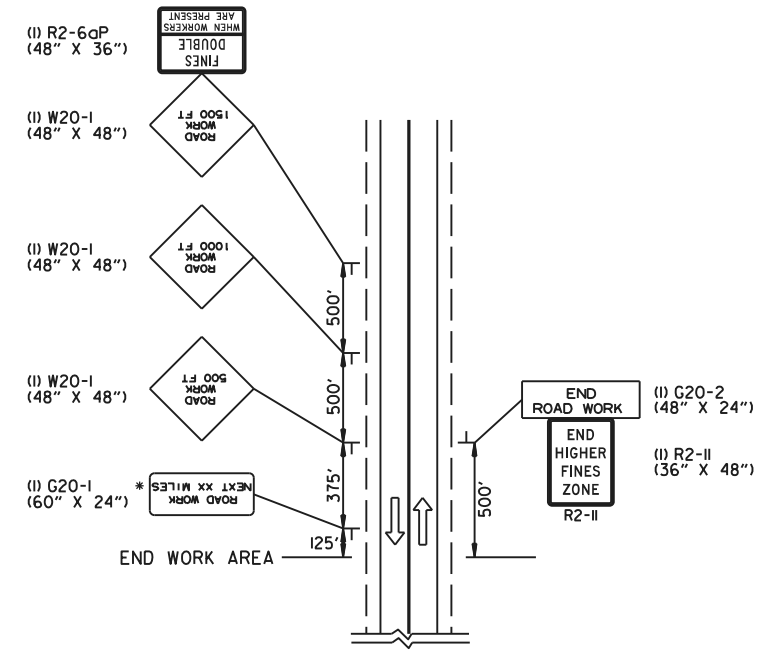
DATE EFFECTIVE	STANDARD DRAWING	TC-3
08-14-2025		

NOTE : W20-1 (VARIOUS DISTANCE) ADVANCE SIGNS TO BE REPLACED AS NEEDED BY EQUIVALENT W20-5 SIGNS AS WORKING AREA SHIFTS.



(A) TYPICAL APPLICATION - ADVANCE WARNING SIGNS AT BEGINNING AND END OF JOB (FULLY CONTROLLED ACCESS FACILITIES) (ALL STAGES)

NOTE : W20-1 (VARIOUS DISTANCE) ADVANCE SIGNS TO BE REPLACED AS NEEDED BY EQUIVALENT W20-5 SIGNS AS WORKING AREA SHIFTS.



(B) TYPICAL APPLICATION - ADVANCE WARNING SIGNS AT BEGINNING AND END OF JOB (NON-FULLY CONTROLLED ACCESS FACILITIES) (ALL STAGES)

GENERAL NOTES:

1. WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
2. THE G20-1 SIGN SHOULD BE INSTALLED ON JOBS OF OVER TWO MILES IN LENGTH, WHEN THE LANE CLOSURE IS NOT AT THE BEGINNING OF THE PROJECT, THE G20-1 SIGN SHALL BE ERECTED 125' IN ADVANCE OF THE JOB LIMIT. ADDITIONAL W20-1 (1 MILE OR 1500 FT) SIGNS ARE NOT REQUIRED IN ADVANCE OF LANE CLOSURES THAT BEGIN INSIDE THE PROJECT LIMITS. THE DISTANCE DISPLAYED ON THE G20-1 SIGN SHALL BE STATED TO THE NEAREST WHOLE MILE.
3. PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER, WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THIS DEVICE SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE. PAYMENT FOR TRAFFIC DRUMS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR PORTABLE CHANGEABLE MESSAGE SIGNS.
4. PORTABLE CHANGEABLE MESSAGE SIGNS SHALL MEET THE REQUIREMENTS OF THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).

*SEE GENERAL NOTE 2

		ARKANSAS STATE HIGHWAY COMMISSION	
STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION			
11-06-25 08-14-25 DATE REV	REPLACED * SPEED * WITH * TRAFFIC LAWS ON WZ-1 ISSUED	DATE EFFECTIVE 11-06-2025	STANDARD DRAWING TC-6